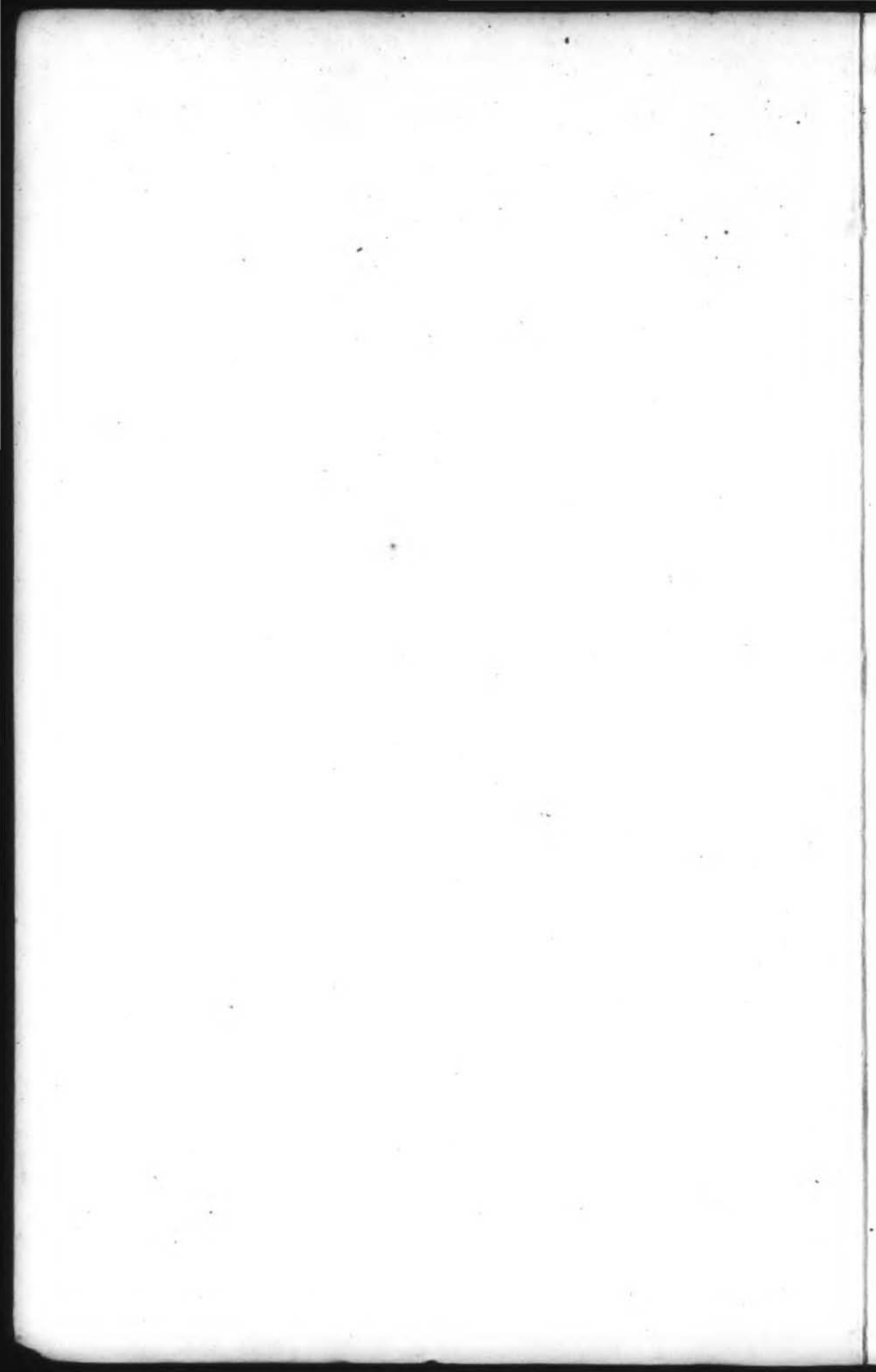


SEVENTH
BIENNIAL REPORT
of the
STATE ROAD
DEPARTMENT
of the
STATE OF FLORIDA



FOR THE PERIOD
BEGINNING JANUARY 1, 1927
AND
ENDING DECEMBER 31, 1928

F625.75
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BEGINNING JANUARY 1, 1927
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PRINTED BY
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ST. AUGUSTINE
FLORIDA
U.S.A.



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G. D. Perkins

J. Harvey Bayliss

W. P. Bevis, Secretary

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G. L. Derrick.....	Bridge Engineer
Harvey A. Hall.....	Testing Engineer
F. W. Berry, Jr.	Office Engineer
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J. H. Dowling.....	Division Engineer Second Division
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A. W. Kinney.....	Division Engineer Fifth Division
R. C. Fergus.....	Division Engineer Sixth Division
Henry Wilson	Division Engineer Seventh Division
Paul G. Kennemur.....	Acting Superintendent of Equipment

Accounting Division

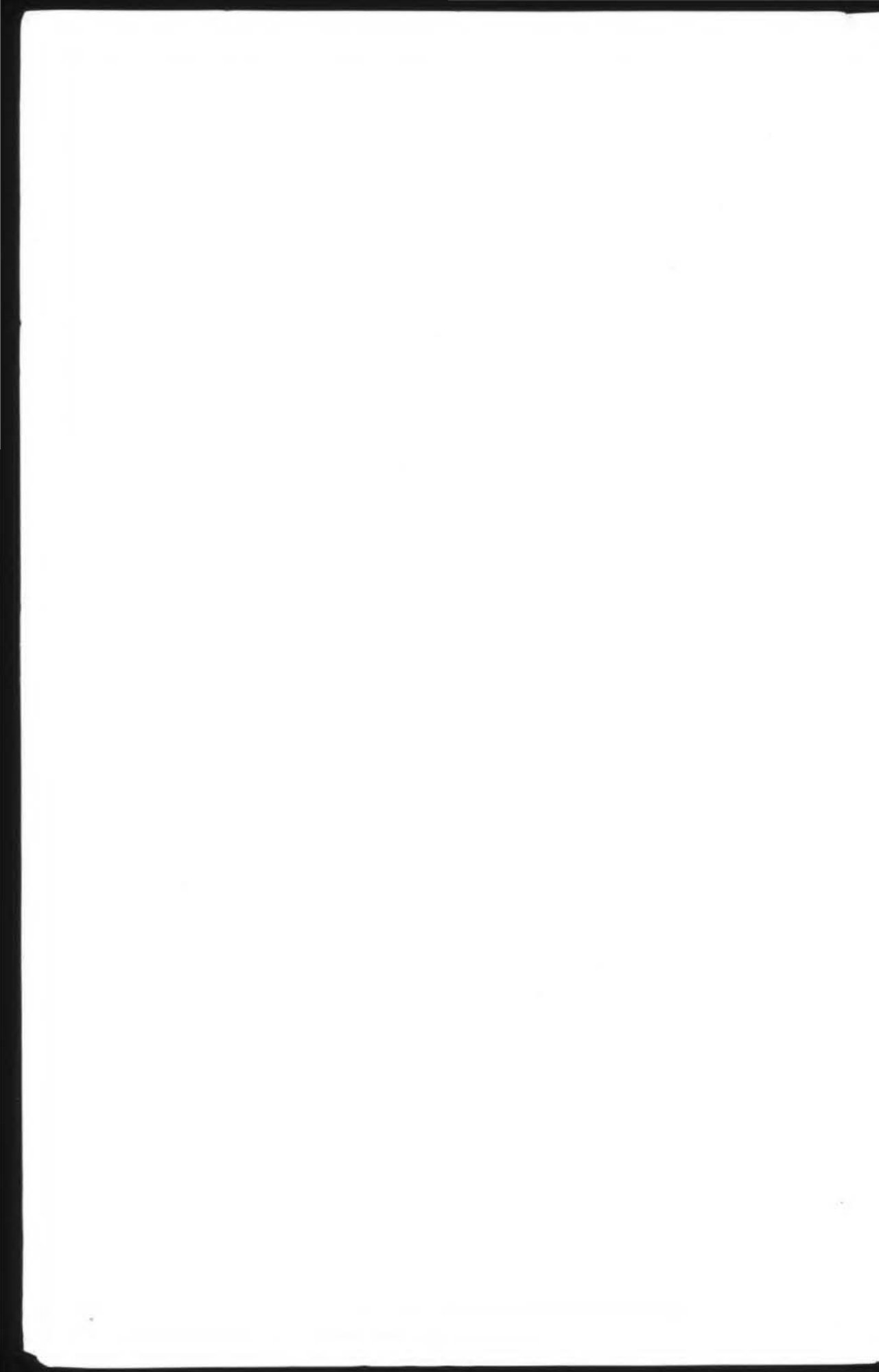
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Bettie V. Herring.....	Bookkeeper
H. B. Fryer	Bookkeeper
M. P. Relihan	Bookkeeper
A. S. Casseaux	Bookkeeper

Bureau of Public Roads

R. E. Toms	District Engineer
H. J. Morrison	U. S. Highway Engineer
E. R. Epperson.....	Associate Highway Engineer



MARTIN BUILDING
Headquarters of State Road Department of Florida





BRIDGE OVER EAST BAY, ROAD 10.
Length of Bridge, 4,000 feet; Depth of Water, 40 feet.

LETTER OF TRANSMITTAL

Hon. Doyle E. Carlton,
The Governor,
State Capitol, Tallahassee, Florida.

My dear Governor:

A requirement of the statutes is that the State Road Department shall prepare and issue a biennial report of its activities, such report to be published just prior to the regular session of the Legislature. The two-year period, 1925-26, was covered by the report issued in 1927, and the report transmitted herewith covers the years 1927-28.

This report takes up the various activities of the State Road Department, and also shows receipts and expenditures. Tables showing mileage of roads completed, other mileage under construction, and similar data are included in the presentation.

The program of road construction by the State, while it has been considerably forwarded by work done in the past two years, is far from complete. It is my earnest hope and desire that the income of this Department will not be reduced by legislative enactment, and I feel that it is the desire and will of the people generally that construction of Florida's great road system be carried on without hindrance.

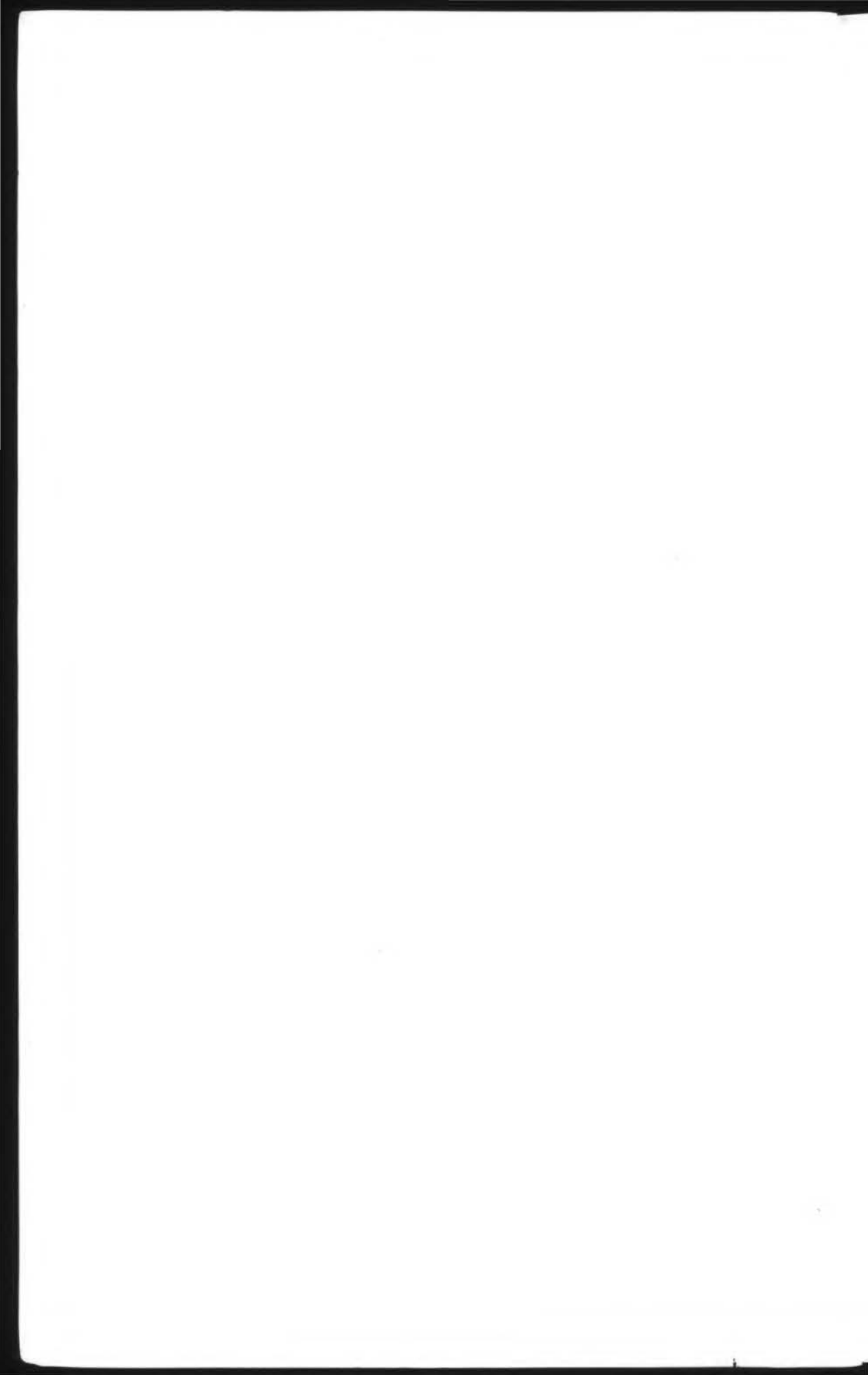
It will be noted that a number of the counties, in their desire to secure completion of main stem roads at the earliest moment possible, have contributed their own money, and thus aided the State. Such enterprise is indeed commendable.

Maintenance of roads and bridges is as important as construction, for if the structure be not maintained the original investment is impaired or completely lost.

The members of the State Road Department are of the belief that the Governor and the Legislature stand with the great mass of Florida's citizenry in their determination that the roadbuilding program shall be carried on without interruption.

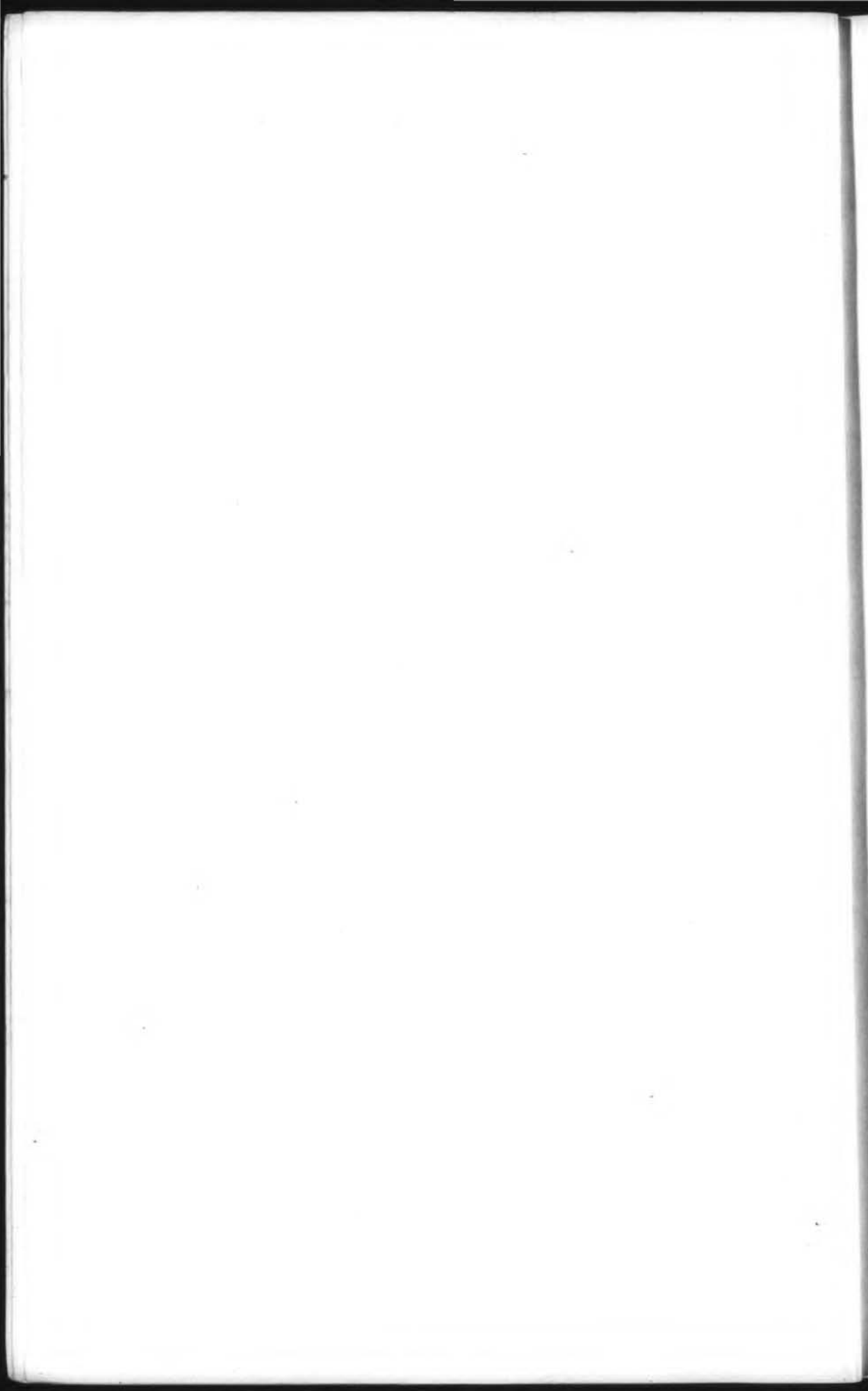
Sincerely yours,

ROBERT W. BENTLEY,
Chairman.



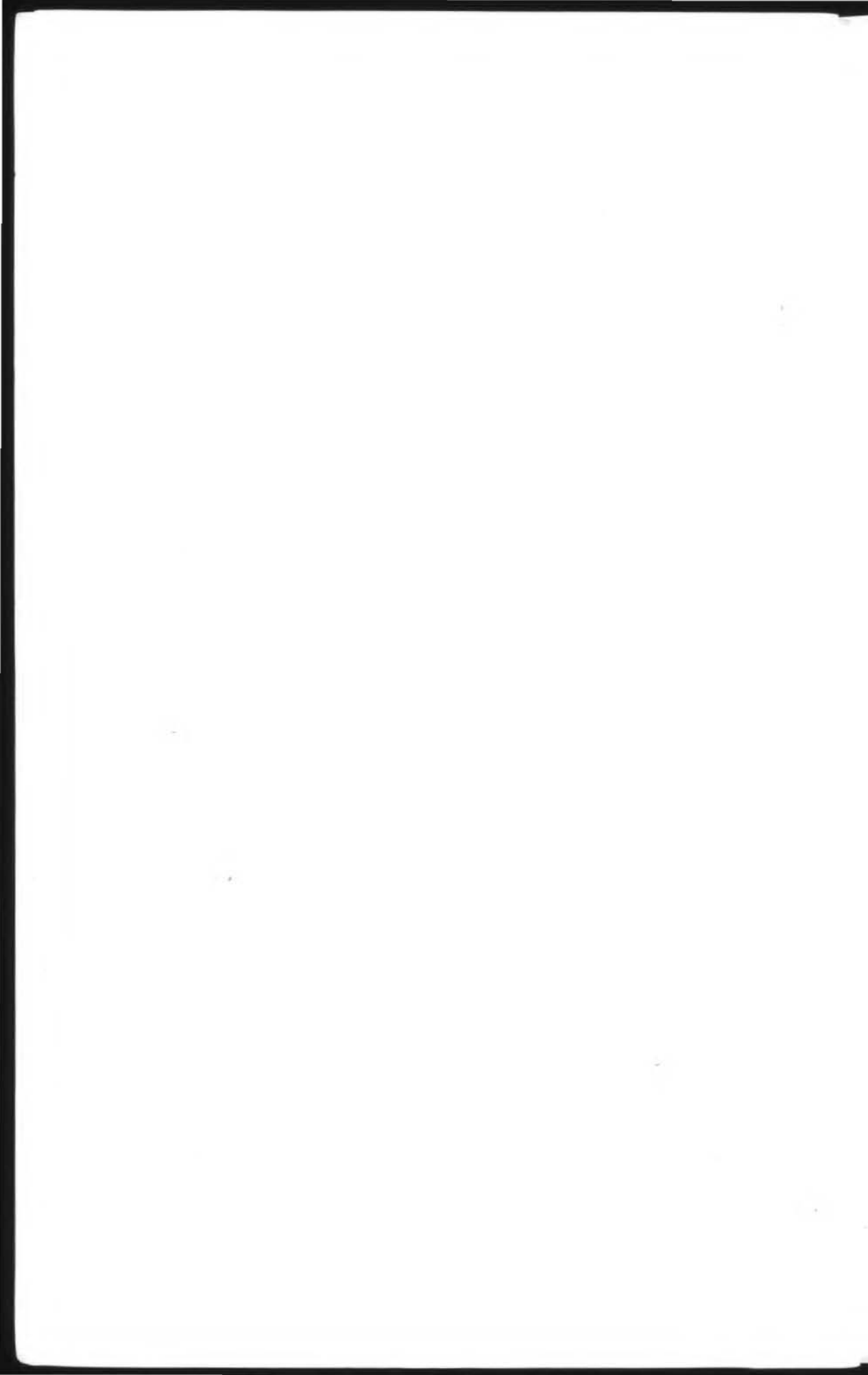


Rock Base Surface Treated, Federal Aid Road 14.





Plain Cement Concrete, Federal Aid Road 4.



The State Road System

The following is a list of the roads in the State System as designated by the Legislature of Florida, which roads have been by resolution of the Department at its October meeting, re-numbered in accordance with the direction of the Legislative Session of 1927. It is to be noted that this is simply a statement of the numerical designation of each road which has been included in the system, and does not set out the preferential status as also fixed by legislative enactment.

At the 1927 session, many additional roads were designated as a part of the State System, and in order to avoid confusion and prevent duplication the State Road Department was directed to give to such roads the proper numerical designation. This was done by resolution of the Department at its quarterly meeting in October, and the following is the list of all roads included in the State Road System:

Road No. 1.—Extending from the Alabama State Line at Nuncz Ferry to Jacksonville, passing through Pensacola, Milton, Crestview, DeFuniak Springs, Bonifay, Chipley, Marianna, Chattahoochee, Quincy, by or near Havana, Tallahassee, Monticello, Greenville, Madison, Live Oak, Lake City, Sanderson, Macclenny and Baldwin.

Road No. 2.—Extending from the Georgia State Line north of Jennings to Fort Myers, via Jasper, White Springs, Lake City, High Springs, Gainesville, Ocala, Leesburg, Lisbon, Grand Island, Eustis, Mt. Dora, Plymouth, Apopka, Orlando, Kissimmee, Haines City, Bartow, Arcadia, and Olga Bridge; also from Leesburg to Lakeland, via Mascotte, Groveland, Buck Hill, Polk City, Foxtown to Lakeland; also from Silver Lake Forks, about four miles east of Leesburg, Lake County, via Tavares, to Mount Dora, to be built with county funds and with Federal aid.

Road No. 3.—Extending from the Georgia State Line at a point on the St. Mary's River known as Wild's Landing, to Orlando, via Jacksonville, Orange Park, Green Cove Springs, Palatka, East Palatka, Crescent City, DeLand and Sanford.

Road No. 4.—Extending from the Georgia State Line south of Folkston to Miami, via Hillard, Callahan, Jacksonville, St. Augustine, Bunnell, Daytona, Titusville, Fort Pierce, West Palm Beach and Fort Lauderdale.

Road No. 4-A.—Extending from Miami to Key West, via Key Largo, provided on that portion of the road south of Dade County, Monroe County shall expend \$2,000,000, including the amount already expended on said road, and in addition to said \$2,000,000 shall provide one-third of the cost of the bridges from No-Name Key to Knight's Key and from Grassy Key to Lower Matacumbe.

Road No. 5.—Extending from High Springs to Fort Myers, via Newberry, Archer, Williston, Dunnellon, Inverness, Brooksville, Lutz, Tampa, Riverview, Bradenton, Sarasota, Venice and Punta Gorda.

Road No. 5-A.—Is the extension from High Springs to Perry, via Fort White, Branford and Mayo.

Road No. 6.—Extending from the Alabama State Line south to Dothan to Port St. Joe, via Campbellton, Marianna, Altha, Blountstown, Scotts Ferry and Wewahitchka.

Road No. 7.—From Pensacola to Alabama State Line at Flomaton.

Road No. 8.—Extending from Haines City to Fort Pierce, via Lake Wales, Frostproof, Avon Park, Sebring, DeSoto City, Lake Annie and Okeechobee City.

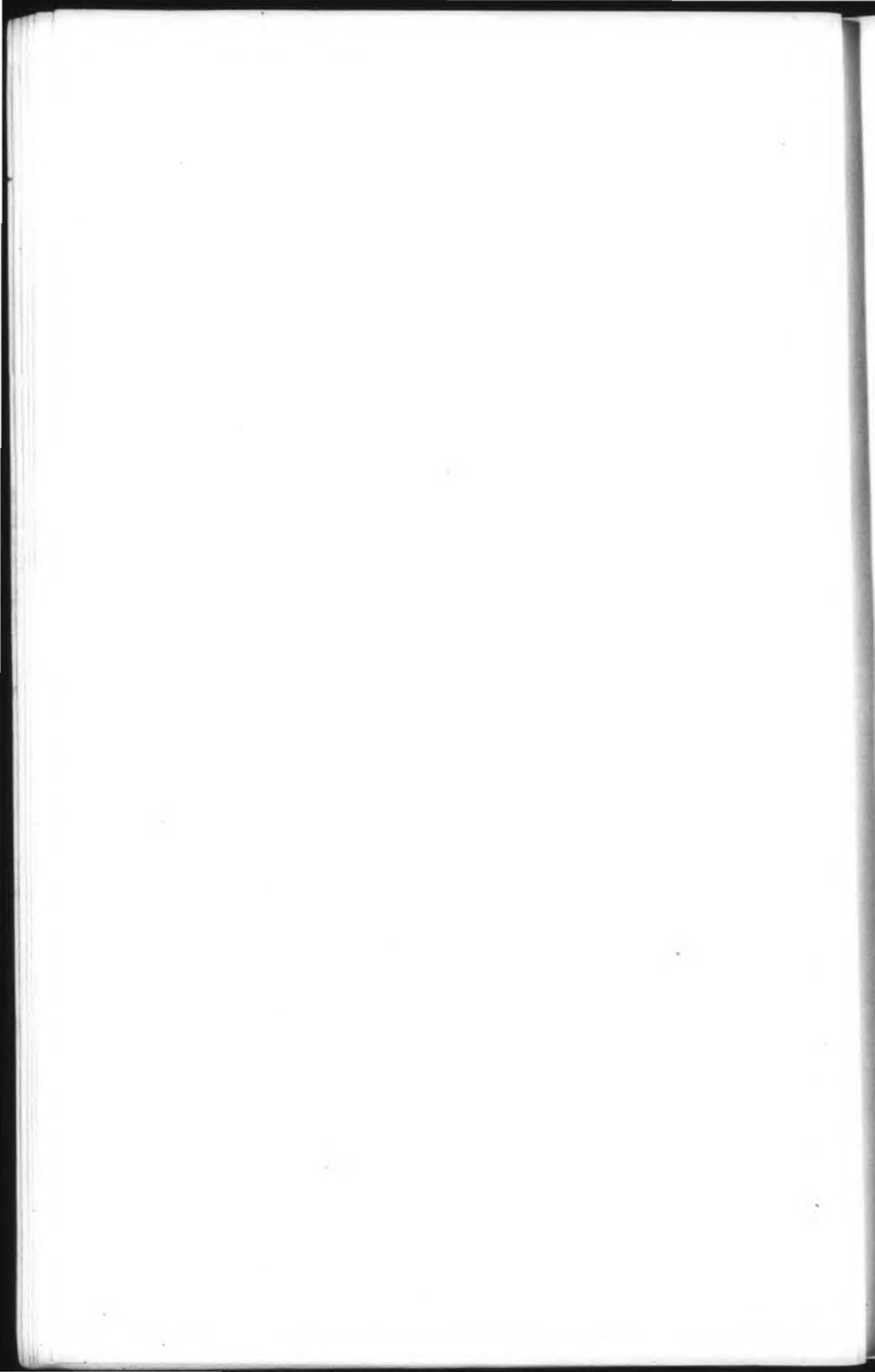
Road No. 9.—Extending from the Georgia State Line south of Quitman, via Madison, to a point on Road No. 35, near Shady Grove.

Road No. 10.—Extending from the Georgia State Line near Beachton to Pensacola, via Tallahassee, Woodville, Newport, and as near St. Marks as practicable, and thence around the coast to Panacea Springs, St. Teresa, Lanark, also via Wakulla, Crawfordville, Sopchoppy, Carrabelle, Apalachicola, Port St. Joe, Panama City, Bruce, Freeport, Portland, New Valparaiso, Camp Walton, and thence into State Road No. 1 at or near Milton, and thence over State Road No. 1, connecting with Pensacola.

Road No. 11.—Extending from a point on the Georgia State Line south of Thomasville via Monticello to Capps.



Rock Base Surface Treated, Road 2.



Road No. 12.—Extending from the Georgia State Line on the Bainbridge and Quincy Road, via Greensboro, Bristol and to East Point.

Road No. 13.—Extending from Cedar Key to Yulee, in Nassau County, via Bronson, Gainesville, Starke, Baldwin and Callahan; extending in an easterly direction from Yulee through Fernandina to the Atlantic Ocean, over and along that certain Twelve-Mile stretch of new concrete road just completed by Nassau County.

Road No. 14.—Extending from a point on Road No. 19, at the Suwannee River Bridge, near Old Town to Hastings, on Road No. 4, via Trenton, Newberry, Gainesville and Palatka.

Road No. 15.—Extending from a point on Road No. 10, at or near Newport, thence southerly along the Gulf as near as practical, to St. Petersburg, via or near as practical to the mouths of the Steinhatchee and Suwannee Rivers, Cedar Key, and as near as practical to the mouth of the Crystal River, Homosassa, Weekiwachee Rivers, Aripeka, Hudson, New Port Richey, Tarpon Springs, Clearwater and Largo. And also from Brooksville to Aripeka, via Spring Hill.

Road No. 16.—Extending from Ocala to Road No. 5, via Dunnellon, thence to Crystal River.

Road No. 17.—Extending from Haines City to Clearwater, via Lakeland, Plant City, Tampa and Oldsmar; also from Haines City eastward to a point intersecting Road No. 24 at Deer Park, in Osceola County.

Road No. 18.—Extending from Sarasota to Lake Annie, via Arcadia; and Road No. 18-A, extending from a point on Road No. 5, near Bradenton, via Oneco, intersecting Road No. 18 at a point about thirty miles east of Sarasota.

Road No. 19.—Extending from Tallahassee to Ocala, leaving out of Tallahassee on the St. Augustine Road, thence to or near Chaires via Waukeenah, Capps and Lamont, thence direct to Perry, Cross City, Old Town, Chiefland, Bronson and Williston.

Road No. 20.—Extending from Cottondale to Panama City, via Round Lake, and from Cottondale north to connect with Road No. 6.

Road No. 21.—Redesignated to extend from Daytona Beach to DeLand, thence from DeLand to Eustis, via Crow's Bluff and Cassia.

Road No. 22.—Extending from Orlando to Indian River City, via Ft. Christmas; also from Orlando to Crystal River, via Winter Garden, Clermont, Mascotte, Groveland, Bushnell and Inverness.

Road No. 23.—Extending from Ocala to Palmetto, via Bushnell, Dade City, Plant City, and also from Bushnell to Lakeland, via Webster, Tarrytown, and making connection with State Road No. 17 at or near as practical to center of the southeast quarter of Section 14, Township 28, Range 23.

Road No. 24.—Extending from Kissimmee to Melbourne, via St. Cloud.

Road No. 25.—Extending from Olga Bridge to West Palm Beach.

Road No. 26.—Extending from a point on Road No. 8, near Lake Annie, to Fort Lauderdale, via Moore Haven; and also an extension beginning at the City of Miami and proceeding northwesterly along or in proximity to the Miami Canal to a point approximately west of the city of Fort Lauderdale, and thence by the most practical route to intersection with State Road No. 26.

Road No. 27.—Extending from Ft. Myers to Miami.

Road No. 28.—Extending from Lake City to Bunnell, via Palatka, Lulu, Lake Butler, Starke and Keystone Heights.

Road No. 29.—Extending from Bithlow to Moore Haven, via Holopaw, Kenansville and Okeechobee.

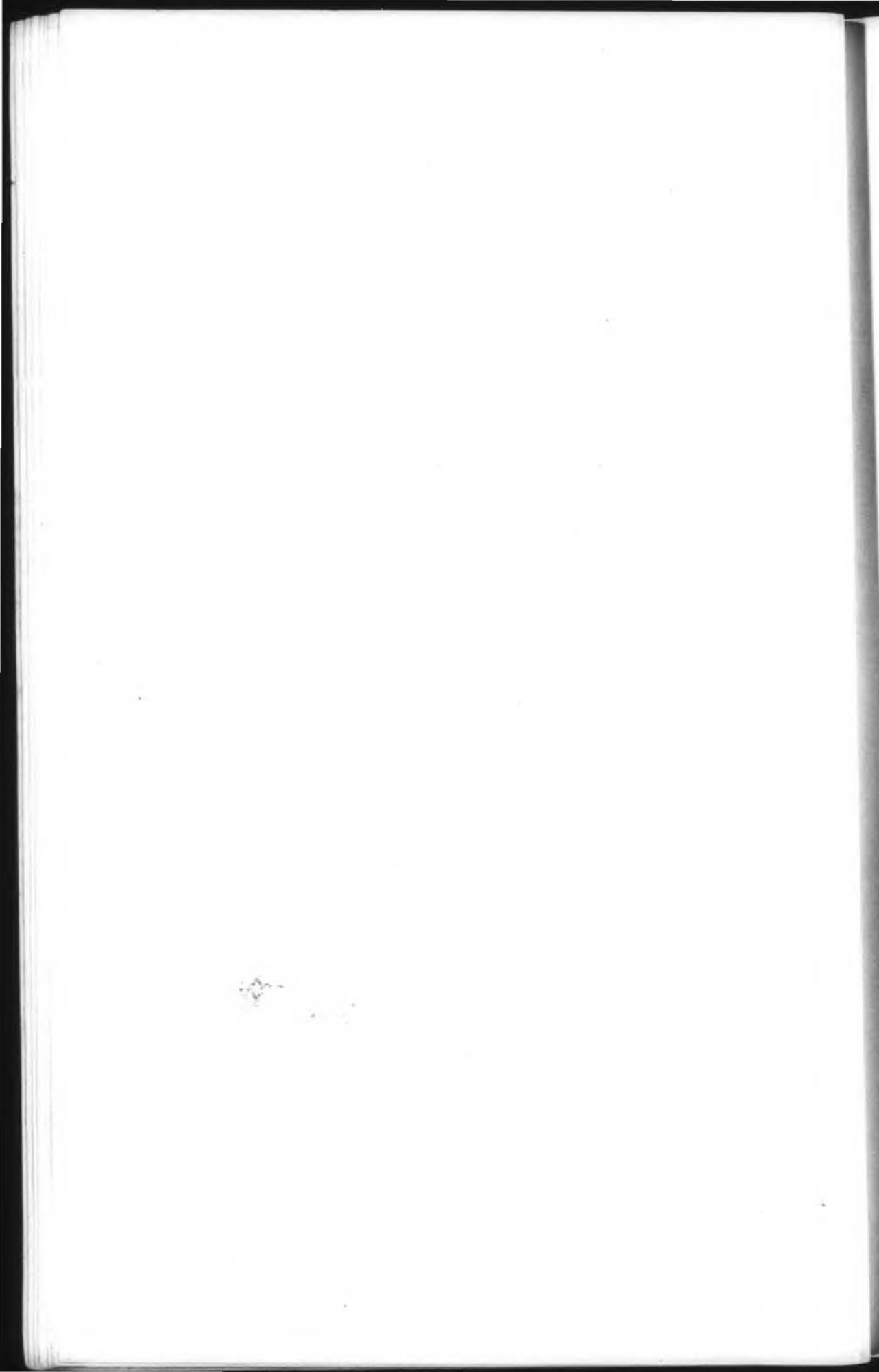
Road No. 30.—Also from Frostproof to Vero, via Dougherty Crossing and Yeehaw.

Road No. 31.—Road from Ocala to Waldo, via Citra, Island Grove and Hawthorn.

Road No. 32.—From Bradenton to Avon Park by way of Parrish, Fort Green and Wauchula; also an extension from Avon Park, eastward through Highlands County, Florida, over what is known as the Kissimmee River Road, a distance of approximately twenty miles and then in a northerly direction through the most practical route to a point where same will intersect State Road No. 30.



Sheet Asphalt, Road 5.



Road No. 33.—Extending from the Alabama State Line south of Florala via Laurel Hill to a point on Road No. 1 at or near Crestview.

Road No. 34.—Extending from Brooksville to Dade City via Spring Lake and Blanton.

Road No. 35.—Extending from the Georgia Line through Ashville and Greenville to a point on Road No. 19.

Road No. 36.—Extending from Leesburg to Inverness via Wildwood and Rutland.

Road No. 37.—Extending from Alabama State Line at Dixonville via Allentown to State Road No. 1 at Milton.

Road No. 38.—Extending from Road No. 2 at Weirsdale, east to Umatilla in Lake County, by way of Stark's Ferry.

Road No. 39.—Extending from the Alabama State Line through Bonifay to Vernon and thence southerly to Phillips Inlet.

Road No. 40.—Extending from the Alabama State Line south of Florala, Alabama, to DeFuniak Springs, Florida, and thence southerly to Freeport, and to Choctawhatchee Bay on the west side of LaGrange Bayou.

Road No. 41.—From Milligan via Baker and Blackman to the Alabama State Line.

Road No. 42.—Extending from a point on Road No. 1, north of Aucilla, through Aucilla to Lamont and to the Taylor County Line, at or near Walker Springs.

Road No. 43.—Extending from a point on the Georgia Line southerly through Miccosukee to a point on Road No. 1 and over same easterly to a point north of Lloyd and then southerly through Lloyd and Wacissa to a point on Road No. 15.

Road No. 44.—Beginning at Mims and extending to Mount Dora via Southmere, Geneva, Sanford and Sorrento.

Road No. 45.—Extending from Ocala to Daytona; and also to extend from State Road No. 45 ten miles east of Ocala, Marion County, Florida, to run in a northeast direction to Bruce, thence to Salt Springs, thence to Norwalk, thence to Welaka, Putnam County, Florida.

Road No. 46.—Extending from the Alabama State Line near Flomaton, Alabama, to Baker, Florida, via Jay, Berrydale and Munson.

Road No. 47.—Commencing at a point on Road No. 4, south of Goodbys Lake in Duval County, running thence southerly as near as practicable along the St. Johns River to a point on Road No. 14, thence to East Palatka.

Road No. 47-A.—Palatka to Ocala via Rodman, Orange Springs and Citra.

Road No. 48.—Extending from St. Augustine to Starke, via Green Cove Springs; and also beginning at the intersection of State Road Nos. 48 and 13 in the town of Starke, Bradford County, Florida, and extending to State Road No. 49 by way of the Stockade and through the State Farm.

Road No. 49.—Extending from the Georgia State Line approximately north of Macclenny to Newberry via Macclenny, Manning, Sapp, Raiford, Lake Butler, Worthington Springs and Alachua.

Road No. 50.—Suwannee River Scenic Highway. Extending from Branford to Jasper, via Live Oak, crossing the Suwannee River at Branford and extending in a southerly direction via Old Town to intersect with State Road No. 19.

Road No. 51.—Extending from Orlando to Brooksville via Groveland, Riverland and Spring Lake.

Road No. 52.—Extending from the Alabama State Line north of Graceville, through Graceville and to the Washington County Line north of Chipley, thence through Chipley to bridge across North Bay near Southport and extending from Graceville via Jacobs to connect with Road No. 6.

Road No. 53.—Extending from Camp Walton along Santa Rosa Sound as near thereto as practicable to Town Point.

Road No. 54.—Extending from Crestview by way of Valparaiso to Camp Walton on Road No. 10.

Road No. 55.—Extending from a point at or near Pierson on Road No. 3 to Road No. 2 at Smithwick's Filling Station, in Lake County, through Astor, Umatilla, Eustis, Tavares, Minneola and Clermont.

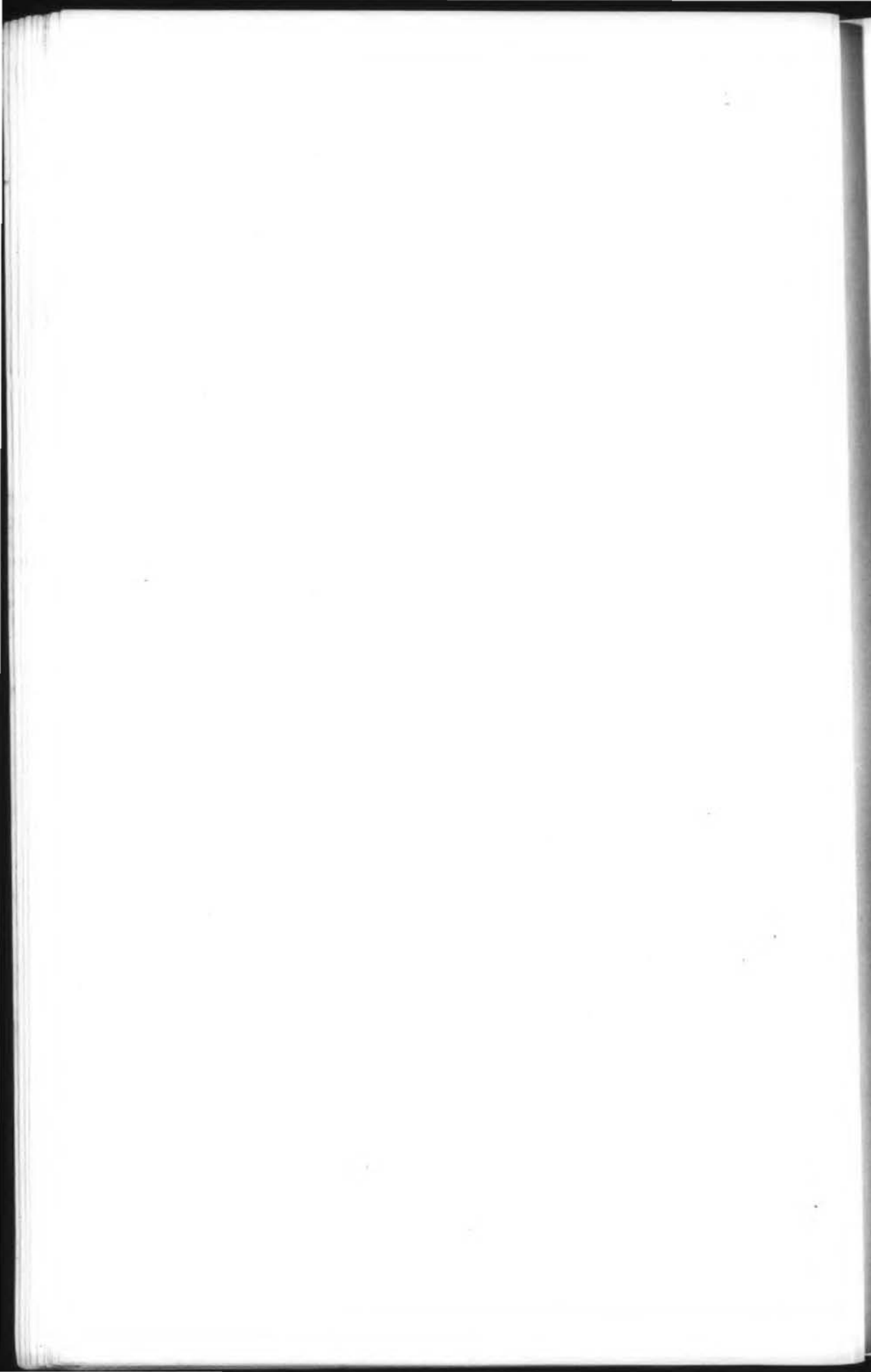
Road No. 56.—Extending from Ellisville to Lake Butler via Providence.

Road No. 57.—Extending from New Smyrna to Sanford.

Road No. 58.—Extending from the Georgia State Line near Darsey connecting with Road No. 1 at or near Havana.



Rock Base Surface Treated, Road 10.



Road No. 59.—Commencing at Zolfo Springs, in Hardee County to Crewsville, thence to DeSoto City, thence to a point at or near Ft. Bassenger, crossing steel bridge over the Kissimmee River at this point, thence in the most advisable or acceptable route to Okeechobee City.

Road No. 60.—A road leaving the Alabama State Line at a point where the Alabama State Highway System strikes the north line of Walton County, Florida, at or near Gaskin, thence to State Road No. 1, in DeFuniak Springs, Florida.

Road No. 61.—Extending from Polk City to Auburndale.

Road No. 62.—Extending from Road No. 7 on Alabama State Line via Berrydale, Munson, Baker, and connecting with Road No. 1 at Milligan.

Road No. 63.—Zolfo Springs to Bradenton via Oneco.

Road No. 64.—Extending from Largo to the Gandy Bridge via North St. Petersburg, in Pinellas County.

Road No. 65.—Extending from a point south of Gainesville on State Road No. 13, in a southwesterly direction to a point on State Road No. 5 at or near Williston.

Road No. 66.—Extending from Tallahassee to a point on Road No. 20 via Jackson Bluff, Bristol and Blountstown.

Road No. 67.—Extending from State Road No. 8 at a point known as Lake Annie, thence to Venus, Palmdale, Board Hammock, Main's Corner, Moore Haven, Liberty Point and Clewiston.

Road No. 68.—Beginning at Orange Park, Clay County, Florida, and extending through Middleburg, Keystone Heights, Hampton and Brooker to Worthington Springs.

Road No. 69.—Connecting with State Road No. 1 at Live Oak, in Suwannee County, Florida, thence extending in a southwesterly course through Suwannee County, Lafayette County and Taylor County to a point on or near the Gulf of Mexico at the mouth of the Steinhatchee River, in Taylor County, via Luraville on the Suwannee River, Mayo, Cook's Hammock and Clara.

Road No. 70.—Extending from State Road No. 23 in the City of Sumterville, at a point at the intersection of State Road No. 23 with range line dividing Ranges 22 and 23;

thence running south along said range line dividing Ranges 22 and 23 in the City of Webster.

Road No. 71.—Extending from the east side of the Withlacoochee River at and near a point east of Croom, Florida, thence running eastwardly and northeastwardly to Coleman, Florida, by way of St. Catherine, Webster, Center Hill to Coleman, making intersection with State Road No. 23.

Road No. 72.—Beginning at State Road No. 4, in the Town of Bunnell, in Flagler County, Florida, and running in a northeasterly direction along the present highway known as the Moody Boulevard to a point in the Town of Flagler Beach, where it intercepts a highway known as the Ocean Shore Boulevard, leading from St. Augustine, Florida, in a southerly direction to Daytona Beach, Florida.

Road No. 73.—Extending from Moncrief Corner to Dellwood via Bayview, in Pinellas County.

Road No. 74.—Beginning at State Road No. 19 at Chiefland, Florida, and extending to Road No. 13, at or near Sumner.

Road No. 75.—Extending from DeLand to New Smyrna, in Volusia County.

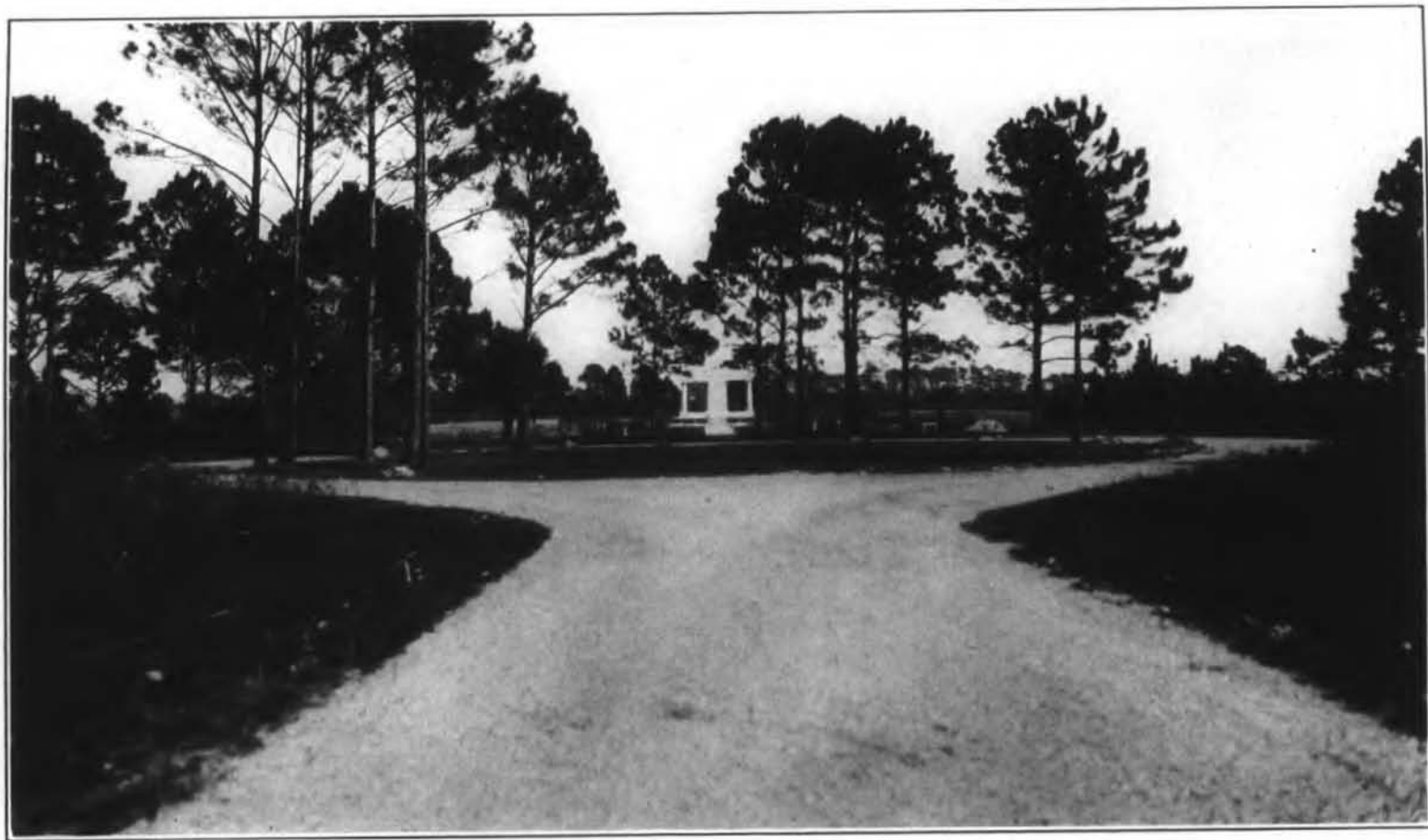
Road No. 76.—Extending from Quincy, Gadsden County, by or near Midway to Tallahassee, Leon County.

Road No. 77.—Commencing at a point on State Road No. 5-A between Branford and the Ichucknee River, and proceeding southward across the Santa Fe River, through the towns of Bell and Trenton to a point on State Road No. 19 near Bronson, in Levy County.

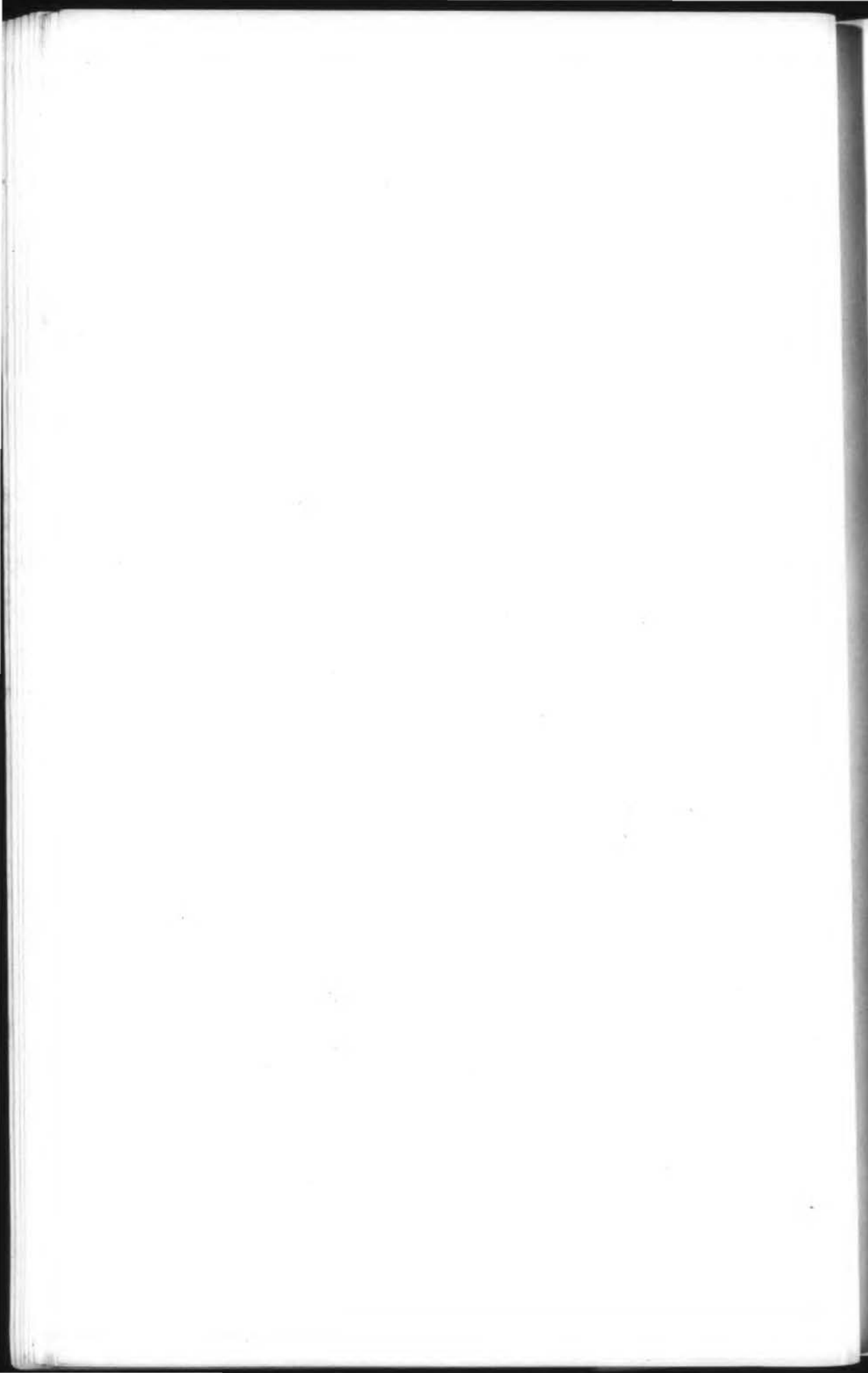
Road No. 78.—That concrete road thirty-two (32) feet wide, constructed by Duval County, from the city limits of South Jacksonville to the Atlantic Ocean at Atlantic Beach, and the road continuing thence through Jacksonville Beach and to the northern limits of the City of St. Augustine, in St. Johns County, Florida.

Road No. 79.—Extending from Wewahitchka over the route now designated and followed by present Wewahitchka-Panama County Road.

Road No. 80.—To extend from State Highway No. 28 at Keystone Heights to run thence southerly to Melrose, thence



Site of Florida's First Capitol, St. Josephs, Fla. Near Road 10.



in a southwesterly direction between the lake and prairie to a point on State Highway No. 14 at or near McMeekin, Putnam County.

Road No. 81.—Beginning at Chiefland, and extending to Dunnellon, via Otter Creek, Gulf Hammock and Lebanon and Tidewater in the most direct and practicable route.

Road No. 82.—Extending from a point about one mile north of Lake City on State Road No. 2 running through Columbia County to the Georgia line. Said road to follow as near as practical the present highway by way of Milton's store and Benton to Georgia Line.

Road No. 83.—Extending from Trenton to Lake City via at or near Williford, Knight Bridge, Fort White and Columbia City.

Road No. 84.—Extending from Marianna, on west side of Chipola River, by way of Carr and Clarksville, to where the same intersects State Road No. 6 at Chipola Park, in Calhoun County. That the route to be followed by the said Road No. 84 shall be as near as practicable the same route of the present Marianna and Wewahitchka Road to where the same intersects State Road No. 6.

Road No. 85.—Extending from Stuart to Indiantown, and thence to Okeechobee, said road to be known as the "Warfield Highway."

Road No. 86.—That the paved public highway from Arcadia, via Fort Ogden, DeSoto County, Florida, to Punta Gorda, Charlotte County, Florida, be and the same is hereby declared to be a State Highway, and is hereby designated, "The DeSoto Trail."

Road No. 87.—Beginning with the main highway leading south of Atmore, Alabama, and continuing in a southerly and southeasterly direction to Pensacola-Flomaton Highway at some point between Pine Barren Creek and Cottage Hill, said point to be determined by the State Road Department, all of said road in Escambia County.

Road No. 88.—Commencing on the State Line dividing the State of Alabama and the State of Florida, due south of Stephen's Ferry on Pea River, in the State of Alabama, at the end of State Highway in the State of Alabama, crossing

said river at said Ferry, thence in a southeasterly direction, the nearest and most practicable route to an intersection of the Section line dividing Sections 4 and 5, 8 and 9, 16 and 17, 20 and 21, 28 and 29, 32 and 33, Township 5 North, Range 17 West; and Sections 4 and 5, 8 and 9, 16 and 17, 20 and 21, 28 and 29, 32 and 33, Township 4 North, Range 17 West; and Sections 4 and 5, 8 and 9, 16 and 17, Township 3 North, Range 17 West; point of intersection of said Highway with said described Section line or lines, to be determined by the State Road Department, which from said determined point on said Section line, or lines, thence south along said described Section line, due south as nearly as practicable to the Town of Ponce de Leon, thence south to the County line dividing Holmes County and Walton County, thence in a southerly direction to the Lower Bridge on Bruce Creek, thence in a southerly direction to State Road No. 10, at or near Bruce, following the present public road as nearly as practicable; which road shall be located by the State Road Department as other State roads are located.

Road No. 89.—Extending from State Road No. 44, as now designated and established at the point where said State Road No. 44 crosses the St. Johns River, running thence due westerly as nearly as practicable to a point on State Road No. 4.

Road No. 90.—Beginning at a point on State Road No. 1, in Jackson County, not more than one-half mile east of new bridge over Chipola River, and extending to Greenwood and to within one-fourth mile of Bascom and to Dudley and to Neal's Landing on the Chattahoochee River in the most direct and practical route.

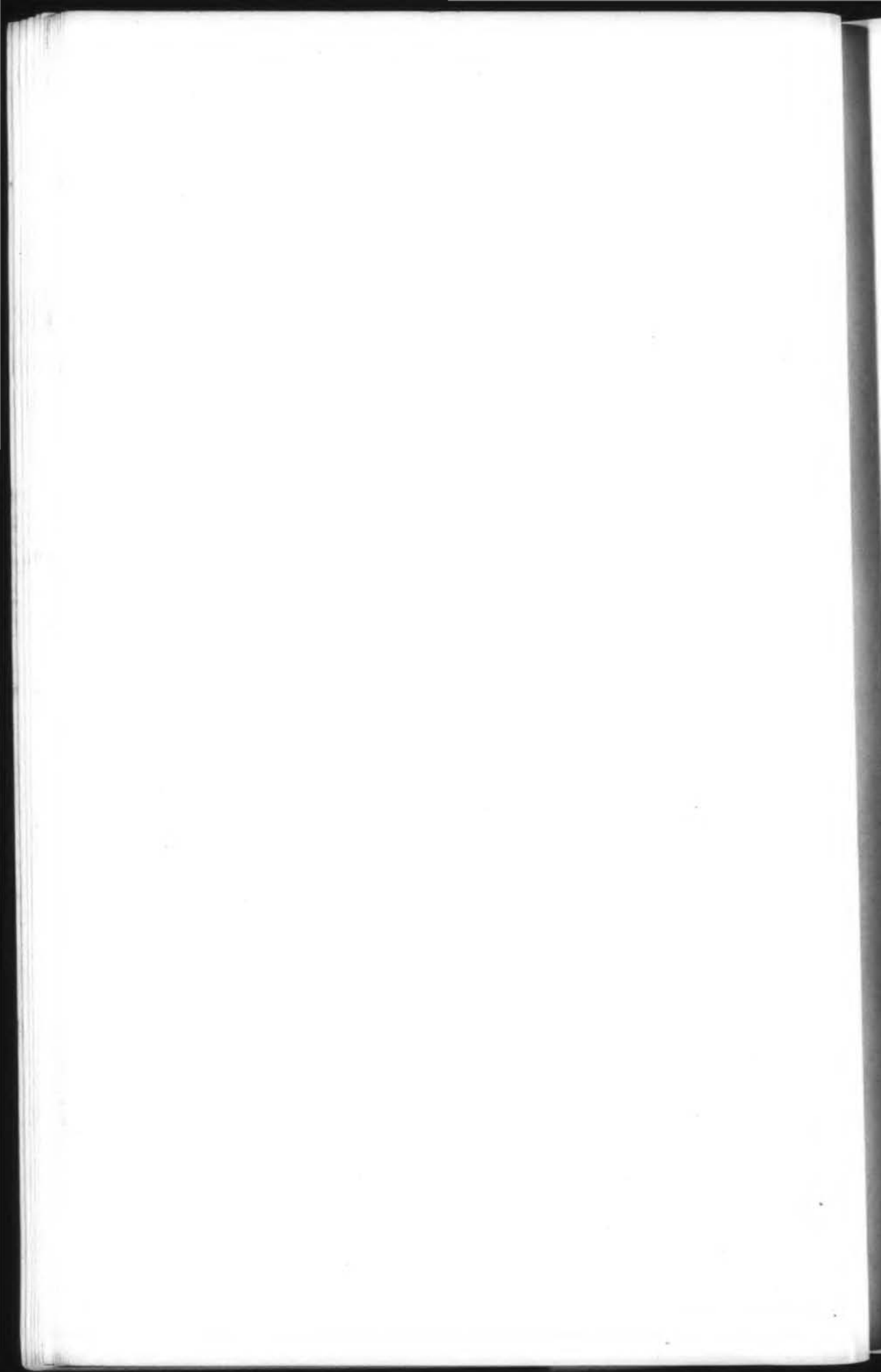
Road No. 91.—Extending from Ocala to Lake Weir, connecting with State Road No. 2, via Candler and Ocklawaha.

Road No. 92.—Connecting with State Road No. 1 at Live Oak, in Suwannee County, thence extending west to Dowling Park, on Suwannee River, thence from Suwannee River south to intersect with State Road No. 5-A, in Lafayette County.

Road No. 93.—Extending from Pensacola along Gulf Beach Highway, in Escambia County, to the public road



Rock Base Before Surface Treatment, Road 10.



upon and into Inerarity Point, and thence along said Inerarity Point road to the shore line of Perdido Bay at the proposed bridge site of the bridge over said Perdido Bay.

Road No. 94.—Extending from State Road No. 1 at Lake Jackson in a northerly direction, crossing the Ocklocknee River over the Whidden Bridge and extending through the town of Concord to the Georgia line.

Road No. 95.—Beginning at a point on State Road No. 47 where said road crosses the Township line dividing Townships Seven and Eight, run thence due west on said Township line to the Gulf of Mexico. Said road shall be known as "St. Augustine State Road No. 95."

Road No. 96.—Beginning at a point on State Road No. 43 at or near the head of the Wacissa River, thence through Thomas City and Waukeenah to a point on State Road No. 11 at or near the intersection of the present Pinhook Road with said State Road No. 11.

Road No. 97.—Beginning at Road No. 1 at Cypress and running in a southerly direction to Alliance and to Altha to connect with Road No. 6.

Road No. 98.—Beginning at Cottdendale on Road No. 20 and running from there to Graceville.

Road No. 99.—Leading from Worthington Springs, in Union County, in a southeasterly direction through LaCrosse, to a point on State Road No. 2 within Alachua County, about one mile northwest of Paradise, Florida, which point is located in Section 18, Township 9 South, of Range 20 East.

Road No. 100.—Extending from State Road No. 21 at or near Crow's Bluff on the west side of the St. Johns River and extending westward via Altoona, intersecting State Road No. 38.

Road No. 101.—Extending from the Town of Wakulla to the Town of St. Marks, in Wakulla County.

Road No. 102.—Leading from the intersection of State Roads Forty-eight (48) and Thirteen (13), in Starke, Bradford County, Florida, to the Stockade at the State Farm, be, and the same is hereby declared and designated a State Highway, same to be known as the State Farm Highway.

Road No. 103.—Leading from a point on Road No. 5 at or near Williston, in Levy County, through Old Wacahoota, Micanopy, Rochell, Windsor to State Road No. 36 at Campville, in Alachua County, thence over State Road No. 36 to Waldo, thence to Montecocha, thence to LaCrosse, thence to Alachua, thence west to Belle, in Gilchrist County, thence west to Wannee, in Gilchrist County, Florida.

Road No. 104.—Beginning at a point on the Pensacola-Flomaton Highway near the Molino cross roads, in Escambia County, Florida, thence to the City of Molino, in Escambia County, thence in a southeasterly direction to Milton, in Santa Rosa County, and there connect with State Road No. 1.

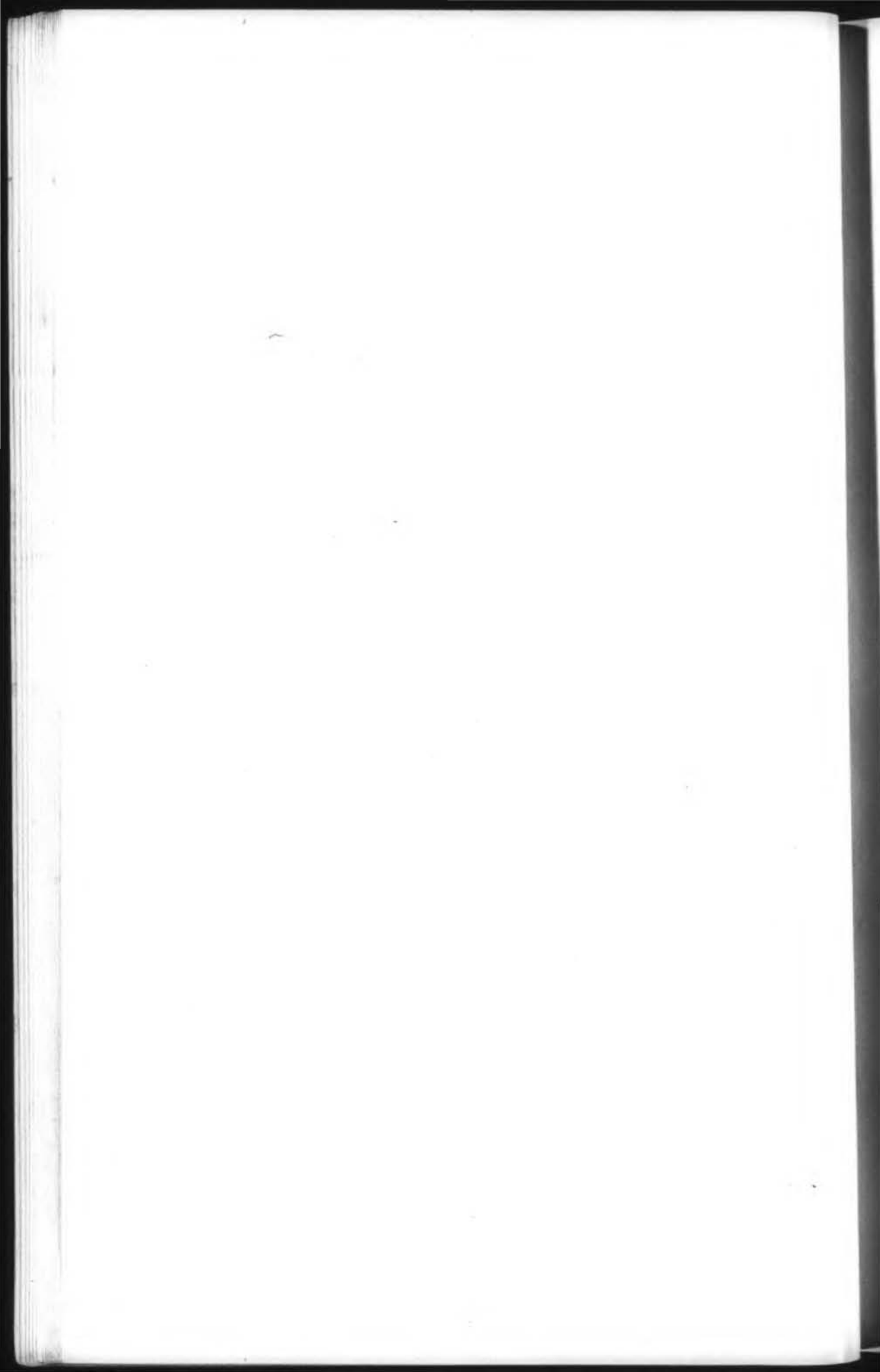
Road No. 105.—Extending from the Town of Baker, in Okaloosa County, to Galliver on Road No. 1, thence from Galliver to Holt, thence from Holt to intersection of State Road No. 10 at or near Camp Walton via Log Lake Bridge, in the most practicable route.

Road No. 106.—Begin on State Road No. 5-A, about nine miles northwest of Mayo, Lafayette County, at or near where what is known as Madison-Mayo public road intersects said Road No. 5-A, thence run northerly to or near where said Madison-Mayo road intersects the Madison-Lafayette County boundary line, thence run to Madison by the most feasible and practicable route, thence by way of Hanson and Pinetta to the Georgia State line where the Madison-Valdosta public road intersects the Georgia-Florida State line in Madison County near Horn's Bridge.

Road No. 107.—Extending from a point on State Road No. 1, about ten miles east of Tallahassee to Chaires, in the County of Leon, and thence to Capitola, in said county, following as near as practical from Chaires to Capitola the present roadway, and extending from Capitola, in the County of Leon, down through what is known as the El Destino Avenue of Oaks, in Jefferson County, to a point on Road No. 19, in Jefferson County, thence by way of Wacissa to the Taylor County line at or near Walker Springs, thence direct through Taylor County by way of Hampton Springs to Perry, thence south to a point on the Gulf of Mexico at or near the beach known as Adams Beach.



West Bay Bridge, Road 10.



Road No. 108.—Extending from Poinciana, Monroe County, in a northeasterly direction to the Tamiami Trail.

Road No. 109.—Extending from Stuart to Indiantown and thence to Port Myacca, and to be known as "Gaines Highway."

Road No. 110.—Beginning about two miles south of Tallahassee on Road No. 10, at or near that branch of the Seaboard Air Line Railway between Tallahassee and St. Marks, running thence via Ivan to Crawfordville, running thence along and as a part of State Road No. 10 to Medart, in Wakulla County, and from Medart running thence in a southerly direction to Panacea Springs on the Gulf Coast in said County of Wakulla.

Road No. 111.—Extending from Stuart to Palm City and thence to Okeechobee, said road shall be known as the "Martin Highway."

Road No. 112.—Extending from Branford to Lake City.

Road No. 113.—Extending from State Road No. 13, about three miles south of Starke, to Gainesville, via Graham, Brooker and LaCrosse.

Road No. 114.—Extending from the Town of Lawtey, in Bradford County, in a western direction to the State Farm Road.

Road No. 115.—From a point on State Road No. 10 at or near Camp Walton, Florida, running in an easterly direction across the mouth of the Choctawhatchee Peninsular, passing not more than two and one-quarter ($2\frac{1}{4}$) miles south of the Town of Santa Rosa, and not more than one and one-quarter ($1\frac{1}{4}$) miles south of the Town of Point Washington, Florida, thence in an easterly direction, joining with State Road No. 10 at some point between Panama City, Florida, and Choctawhatchee River.

Road No. 116.—Beginning on State Road No. 2 just west of Alapaha River, and running in a westerly direction via Blue Springs, and connecting with State Road No. 1 just out of Madison, and to be known as "Blue Springs Highway."

Road No. 117.—Beginning at Indiantown and extending to West Palm Beach in the most direct and practicable route, substantially along the route of the Seaboard Air Line Railway.

Road No. 118.—Extending from Astatula, on State Road No. 55, through Howey-in-the-Hills to Groveland, Lake County.

Road No. 119.—Extending from a point on State Road No. 22 two and a half miles west of Indian River City, northeasterly to the City of Titusville, thence to the Atlantic Ocean by the most direct route.

Road No. 120.—Connecting with the Suwannee River Scenic Highway at Branford, thence northeast via Hickory Sink to Wellborn, connecting with State Road No. 1, in Suwannee County, thence north through Suwannee and Columbia counties to White Springs, in Hamilton County, connecting with State Road No. 2.

Road No. 121.—Extending from a point on State Road No. 48 near north shore of Lake Kingsley, in Clay County, thence in a northwesterly direction to the Town of Lawtey, in Bradford County, thence in a westerly direction to the Stockade of the State Farm in Union County, thence in a westerly direction to a point on State Road No. 49 at or near the Town of Raiford, in Union County.

Road No. 122.—Connecting with State Road No. 1, in Live Oak, running northeast via Pine Grove, in Suwannee County, intersecting with State Road No. 120 at a point near White Springs, in Hamilton County.

Road No. 123.—Extending from Campbellton, in Jackson County, to Graceville, in Jackson County.

Road No. 124.—Road extending from the City of Lakeland, Polk County, to the City of Bartow, Polk County, via Town of Highland City.

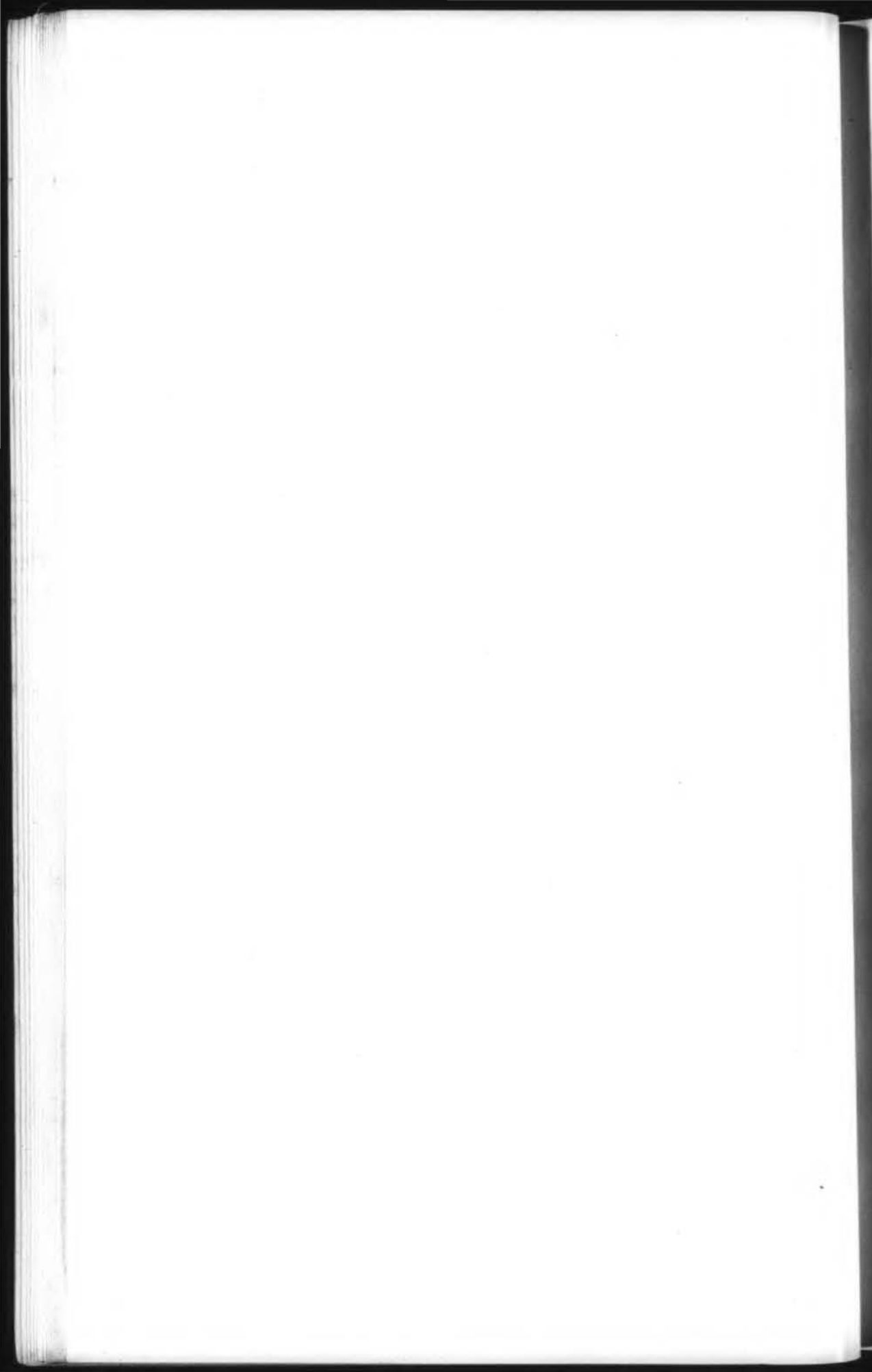
- Road No. 125.—Extending from the intersection of the Lillian Road with Nunez Ferry Road at West Pensacola, and thence in a westerly and southwesterly direction to the Lillian Bridge over Perdido Bay via Millview.

Road No. 126.—Beginning at Neal's Landing, Florida, thence in a southerly direction to Sneads, Florida, and from thence to Shady Grove, Florida.

Road No. 127.—Beginning at State Road No. 10, at Sopchoppy, thence via Greenough, Sanborn and Smith Creek Postoffice, in Wakulla County, to the intersection with State Road No. 66 at Ward, in Leon County.



Rock Base Surface Treated, Road 5.



Road No. 128.—Extending from Wakulla Beach to the intersection of State Road No. 10 at or near the upper bridge across the Wakulla River.

Road No. 129.—Beginning at St. Marks at the southern terminus of the Seaboard Air Line Railway, thence via Crawfordville to Arran.

CHAPTER 10186—(No. 164).

AN ACT to Regulate the Operation of Motor-Driven and Other Vehicles on the Public Highways of the State of Florida and to Provide for the Enforcement, and Punishment for the Violation of this Act.

Be It Enacted by the Legislature of the State of Florida:

Section 1. No person shall operate a motor vehicle upon the public highway of this State recklessly, or at a rate of speed greater than is reasonable and proper, having regard to the width, traffic, and use of the highway, or so as to endanger the property or the life or limb of any person; Provided, that a rate of speed in excess of twenty-five miles per hour in the residence portion of any city, town or village, and a rate of speed in excess of fifteen miles per hour in the business portion of any city, town or village, and a rate of speed in excess of forty-five miles per hour on any public highway outside of the corporate limits of any incorporated city or town of motor-driven vehicles weighing less than five thousand pounds shall be deemed *prima facie* evidence of reckless driving. That a rate of speed in excess of fifteen miles per hour in the residence portion of a city, town or village, and a rate of speed in excess of ten miles per hour in the business portion of any city, town or village, and a rate of speed in excess of thirty miles per hour on any public highway outside of the corporate limits of any city or town, of motor-driven vehicles weighing more than five thousand pounds and less than sixteen thousand pounds shall be deemed a violation of this Section; that a rate of speed in excess of ten miles per hour in the residence portion of any city, town or village, and a rate of speed in excess of eight miles per hour in the business portion of any city, town or village, and a rate of speed in excess of twenty miles per hour

on any public highway outside of the corporate limits of any incorporated city or town, of motor-driven vehicles weighing more than sixteen thousand pounds shall be deemed a violation of this Section; that the speed of all motor-driven vehicles shall be reduced on curves in such manner as to keep the vehicle under the entire control of the driver, provided, further, that no person shall operate upon the public highways inside the incorporate limits of any incorporated city or town of this State a motor vehicle with muffler cut-out open.

Sec. 2. All motor vehicles shall be provided with lights in front and light in the rear. The front or driving lights shall be of the tilting variety or some other device which will kill the glare of the driving lights.

Sec. 3. It shall be unlawful to stop any motor vehicle on the public roads, for either convenience or repair, but in all cases where possible to do so shall turn off the road to the right and the left wheel nearest the center of the paving shall not be more than one foot on the side of the paving. All vehicles shall drive on the right side of the road except when passing a slower vehicle.

Sec. 4. It shall be unlawful to operate upon any hard surfaced road in Florida any log cart, tractor, well machine or any steel tired vehicle other than the ordinary farm wagon or buggy, or any other vehicle or machine that is likely to damage a hard-surfaced road except ordinary wear and tear on the same.

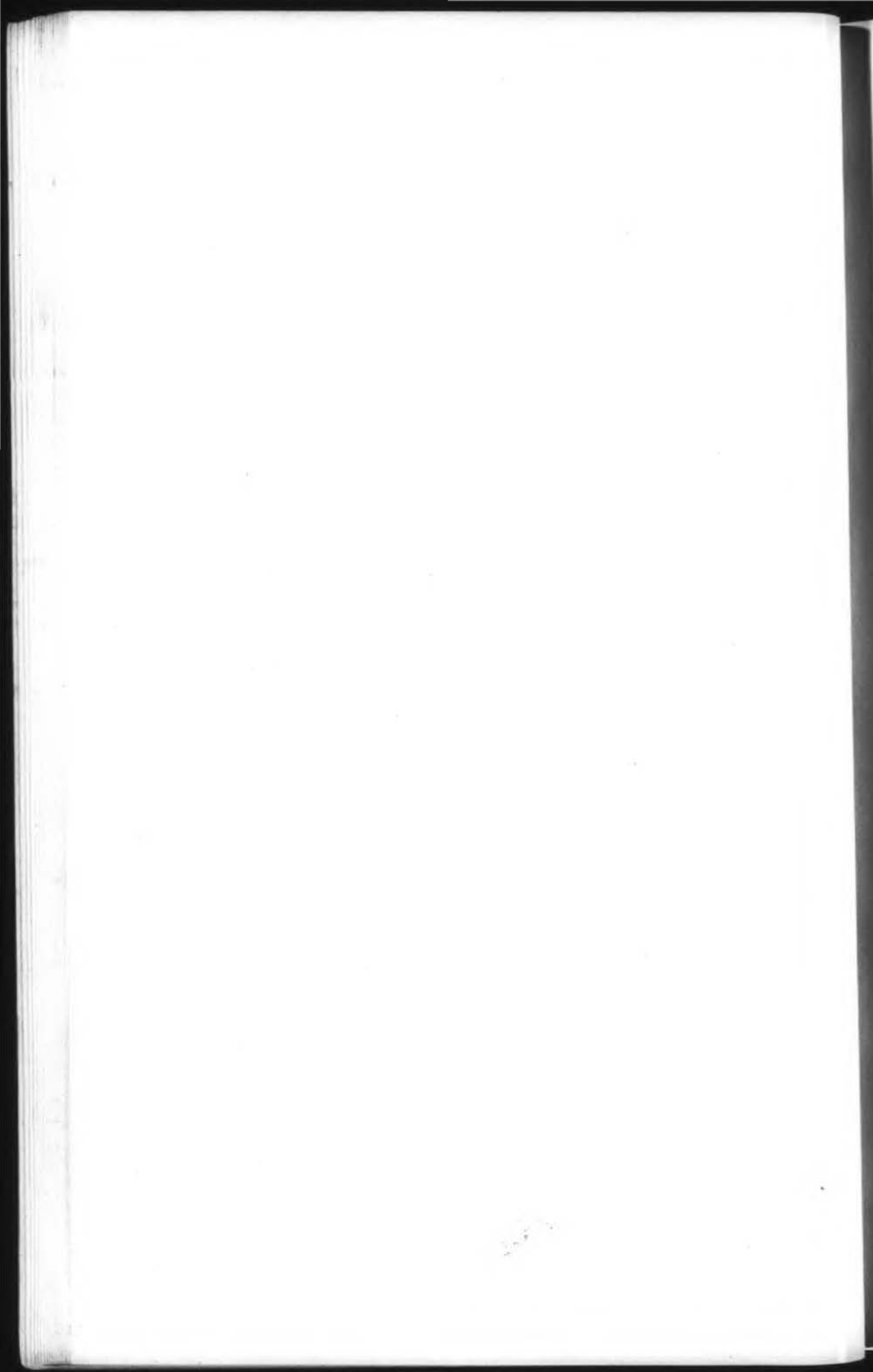
Sec. 5. It shall be unlawful for any officer in the discharge of his duties, as provided for in this Act, to demand an excessive appearance bond and in all such cases the gravity of the offense committed shall be considered in the requirement of such appearance bonds.

Sec. 6. It shall be unlawful for any city or town to pass or attempt to enforce any ordinance in conflict with the provisions of this Act. Provided, however, that this Act shall not apply to school zones.

Sec. 7. Any person convicted of the violation of any of the provisions of this Act shall be guilty of a misdemeanor and upon conviction shall be punished by a fine of not more than one hundred (\$100.00) dollars or by imprisonment for



Plain Cement Concrete, Road 4.



not more than ninety (90) days, or by both such fine and imprisonment.

Sec. 8. All laws and parts of laws in conflict herewith are hereby repealed.

Sec. 9. This Act shall take effect upon its becoming a law.

Approved June 8, 1925.

RULES AND REGULATIONS OF THE SECRETARY OF AGRICULTURE FOR CARRYING OUT THE FEDERAL HIGHWAY ACT (EXCEPT THE PROVISIONS THEREOF RELATIVE TO FOREST ROADS).

Regulation 1.—Definitions

Section 1. For the purposes of these regulations, the following terms shall be construed, respectively, to mean:

Act.—The act of Congress approved July 11, 1916, entitled "An act to provide that the United States shall aid the States in the construction of rural post roads, and for other purposes" (30 Stat. 355), as amended by the act of Congress approved February 28, 1919, entitled "An act making appropriations for the service of the Post Office Department for the fiscal year ending June 30, 1920, and for other purposes" (40 Stat. 1200, 1201), and as amended by the act of Congress approved November 9, 1921, entitled "An act to amend the act entitled 'An act to provide that the United States shall aid the States in the construction of rural post roads, and for other purposes,' approved July 11, 1916, as amended and supplemented, and for other purposes" (Public, No. 87, 67th Cong.)

Secretary.—The Secretary of Agriculture of the United States.

Bureau of Public Roads.—The Bureau of Public Roads of the United States Department of Agriculture.

Authorized representatives of the Secretary.—The Chief of the Bureau of Public Roads and such other officials and employees thereof as he may designate from time to time.

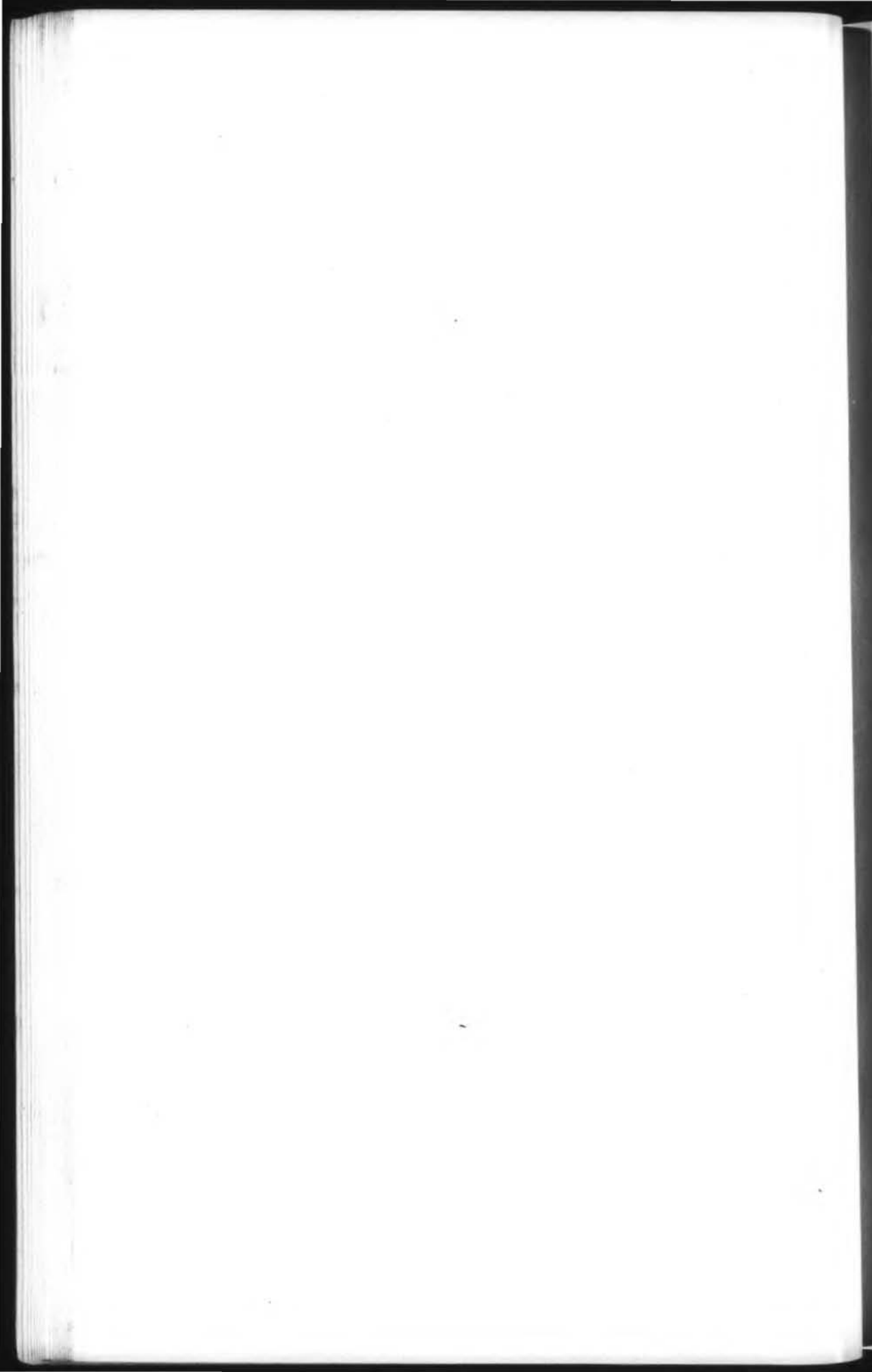
Federal aid highway system.—The system of Federal aid highways, established by actual agreement and joint action of the States and the Secretary of Agriculture, and upon which all Federal aid funds shall be spent.

Primary highways.—The primary or interstate portion of the Federal aid highway system composed of not to exceed three-sevenths thereof.

Secondary highways.—The secondary or intercounty portion of the Federal aid highway system consisting of at least four-sevenths thereof.



Rock Base Surface Treated, Road 5.



Ten per cent fund.—Items for engineering, inspection, and unforeseen contingencies not exceeding 10 per cent of the total estimated cost of the construction.

Regulation 2.—Application of Regulations

Section 1. These regulations apply to all provisions of the act, except the provisions thereof relative to forest roads and trails, unless hereafter so made applicable by order of the Secretary.

Sec. 2. These regulations shall apply as fully where the extent to which the State may engage in road construction and maintenance work, or raise State revenues therefor, is limited by its existing constitution and laws as in any other case.

Regulation 3.—Information for the Secretary

Section 1. Before any agreement is made upon any road or roads to be constructed in a State, or as to the character and method of construction, there shall be furnished the Secretary upon his request, by or on behalf of the State, general information as to its laws affecting roads and the authority of the State and local officials in reference to the construction and maintenance of roads; as to the State highway department, how equipped and organized; as to the existing provisions of its constitution or laws relative to State revenues for the construction, reconstruction, or maintenance of roads; as to funds that will be available to meet the State's share of the cost of the construction work to be performed and the general source of such funds; and as to provisions made, or to be made, for maintaining roads upon which Federal aid funds will be expended.

Sec. 2. Information requested by the Secretary or his authorized representatives relating to the maintenance of roads constructed under the provisions of the act shall be furnished, from time to time, by the State highway departments, on forms supplied by the Bureau of Public Roads.

Sec. 3. Data furnished by or on behalf of a State shall be supplemented by such reports of the Bureau of Public Roads as the Secretary may from time to time require.

Regulation 4.—Federal Aid Highway System Map

Section 1. Each State highway department shall file with the Secretary of Agriculture a State map showing the proposed Federal aid highway system and indicating the primary and the secondary portions thereof, in such form and with such information as he may require.

Sec. 2. The Secretary, through his authorized representatives, will make an examination of the proposed system and will from time to time notify the State highway department of the acceptability of the parts of the system examined.

Sec. 3. When agreement has been reached between the State highway department and the Secretary as to the whole (or if the State so desires, of a material portion) of the Federal aid highway system, the State shall make formal request for the approval of the Secretary of Agriculture. This request will be accompanied by a State map showing the full proposed Federal aid highway system with the primary and secondary highways upon which formal approval is requested, in such form and with such information as may be prescribed by the Secretary or his authorized representatives.

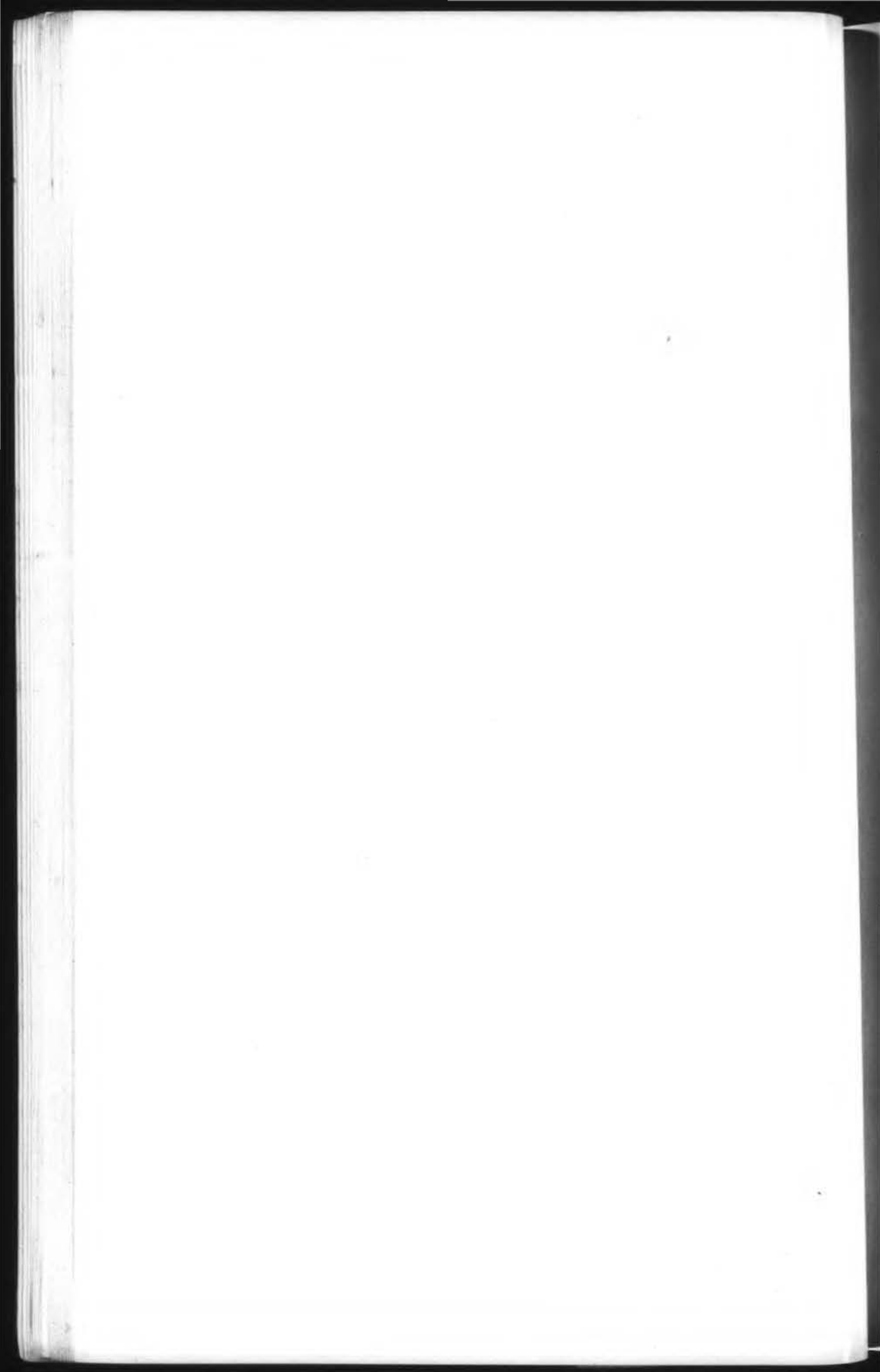
Sec. 4. Pending the formal approval of the State highway system in whole or in part by the Secretary of Agriculture, only such projects will be approved as are on routes indicated on the proposed Federal aid highway system as submitted under section 1 and tentatively accepted by the Secretary under section 2 of this regulation: Provided, That the Secretary of Agriculture may approve project statements submitted by the State highway departments prior to the selection, designation, and approval of the system of Federal aid highways herein provided for if he may reasonably anticipate that the projects will become a part of such system.

Regulation 5.—Project Statements

Section 1. A project statement may be submitted for the whole or any part of a continuous route or routes embraced in the Federal aid highway system selected or designated in accordance with the provisions of the act, preference



Rock Base Surface Treated, Road 27.



being given to such projects as will expedite the completion of a system of highways interstate in character.

Sec. 2. Prior to the selection, designation, and approval of the Federal aid highway system, project statements may be submitted for any route or part of a route if the Secretary may reasonably anticipate that such route will become a part of such system. After the Federal aid highway system shall have been selected, designated, and approved no project statements shall be submitted for any route or part of a route not embraced in the system so selected, designated and approved.

Sec. 3. A project statement shall contain such information as the Secretary may require to be submitted on a form supplied by the Bureau of Public Roads and shall be accompanied by a sketch map in sufficient detail and covering such length of road as may be necessary to determine the fitness of the location as a part of the Federal aid highway system and with the termini of the proposed improvement indicated thereon.

Regulation 6.—Surveys, Plans, Specifications, and Estimates

Section 1. The surveys, plans, specifications and estimates shall show in convenient form and detail the work to be performed, and the probable cost thereof, all in conformity with the standards, governing form, and arrangement prescribed by the Secretary.

Sec. 2. Copies of the specifications shall be submitted with the plans and estimates, except that where standard specifications have been approved by the Chief of the Bureau of Public Roads a statement to the effect that approved standard specifications govern may be submitted in lieu of the printed documents.

Sec. 3. Until plans, specifications, and estimates for a project or part thereof have been submitted and found satisfactory for recommendation, and the State has been so notified by the district engineer of the Bureau of Public Roads, no project or part thereof shall be let to contract.

Sec. 4. The estimate for each project shall show the estimated quantity and the estimated cost of each item of con-

struction in detail and, separately, the 10 per cent fund, and shall not include any expense for advertising.

Sec. 5. Unless State standard contract and bond forms have been approved there shall be submitted with each set of plans for the approval of the Secretary copies of the form of contract together with all documents referred to therein or made a part thereof, and of the contractor's bond which it is proposed to use on the project. No alteration of either of these forms, when once approved, shall be made until it is approved by the Secretary.

Sec. 6. Where any part of the cost of a project is to be furnished by a county or other local subdivision or subdivisions of a State, the plans, specifications, and estimates shall be accompanied by certified copy of each resolution or order, if any, of the appropriate local officials, or such other showing as the Secretary may require respecting the funds which are made available, or respecting the supervision of the construction of the road and of the control of the money provided for paying such cost.

Sec. 7. Right of way ample for any project shall be provided and no incidental damages to adjoining property, due to construction work paid for by or on behalf of the State, shall be included in the estimate or be paid in any part, directly or indirectly, by the Federal Government.

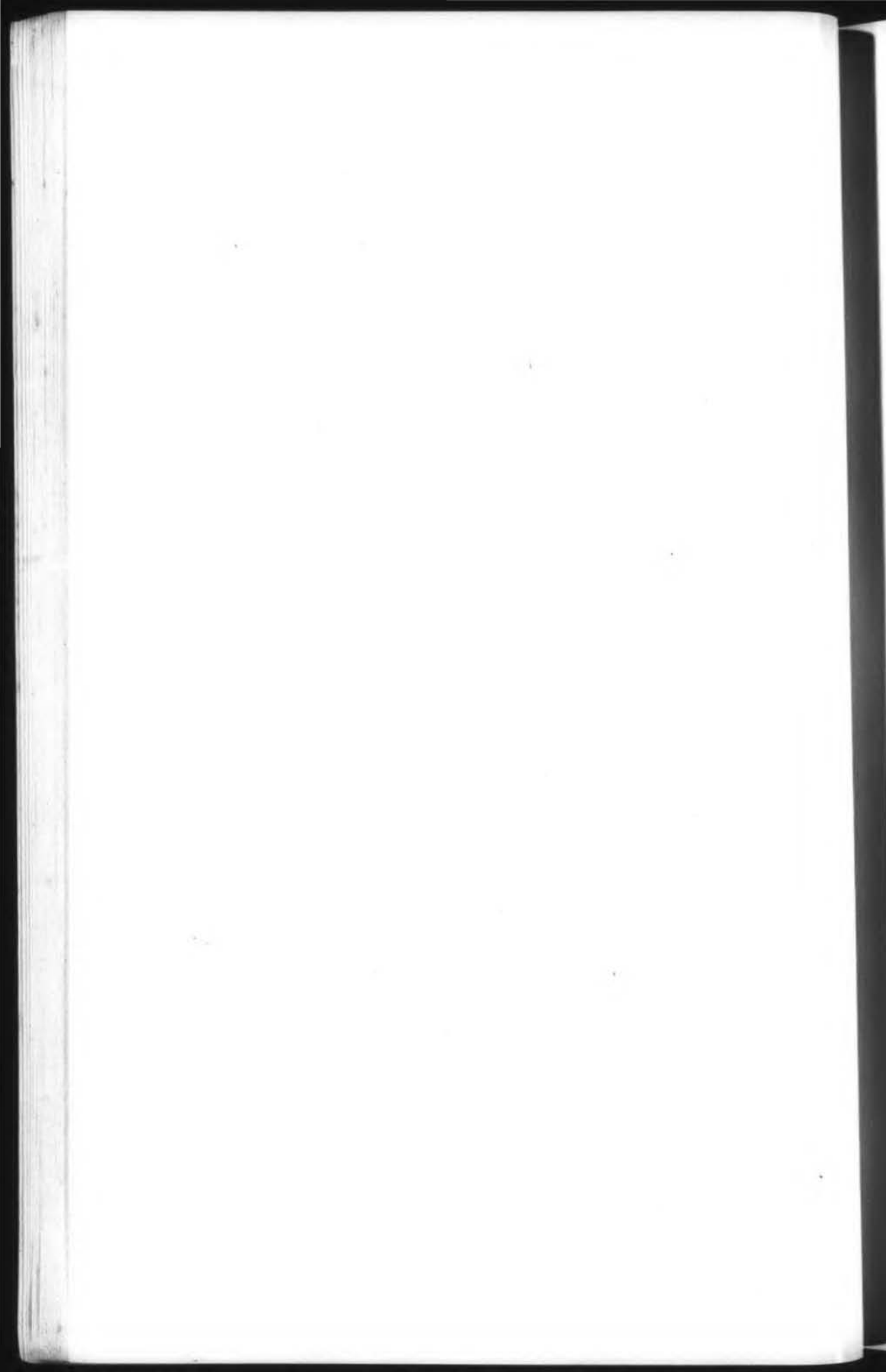
Sec. 8. Grade crossings occurring on the Federal aid highway system shall be classified for priority of improvement by agreement between the State highway departments and the Bureau of Public Roads.

Sec. 9. No part of the expense of making surveys, plans, specifications, or estimates, by or on behalf of the State prior to the beginning of construction work, shall be included in the estimate or paid by the Federal Government.

Sec. 10. Subsequent to the execution of the agreement no change which will increase the cost of a project to the Federal Government shall be made, except upon approval by the Secretary of Agriculture, and no changes shall be made in the termini or type, except upon approval of the Chief of the Bureau of Public Roads, but minor alterations which do not affect the general nature of the improvement or increase



Rock Base Surface Treated, Federal Aid Road 4.



the total cost to the Federal Government may be authorized by the Chief of the Bureau of Public Roads or his authorized representative.

Regulation 7.—Project Agreements

Section 1. A project agreement between the State highway department and the Secretary shall be executed in triplicate on a form furnished by the Secretary. No payment shall be made by the United States unless or until such agreement has been executed, nor on account of work done prior to recommendation by the district engineer of the Bureau of Public Roads that the plans, specifications, and estimates be approved.

Regulation 8.—Contracts

Section 1. No part of the Federal money set aside on account of any project shall be paid until it has been shown to the satisfaction of the Secretary that adequate methods, either advertising or other devices appropriate for the purpose, were employed, prior to the beginning of construction, to insure economy and efficiency in the expenditure of such money.

Sec. 2. Upon publication of advertisements copies thereof shall be furnished to the Bureau of Public Roads.

Sec. 3. Bids shall conform to the standard proposal form, and the items shall be the same as those contained in the estimate provided for in Regulation 6, section 4.

Sec. 4. Copy of the tabulated bid prices, showing the unit prices and the totals of each bid for every project, shall be furnished promptly to the Bureau of Public Roads.

Sec. 5. In advance of the acceptance of any bid sufficient notice of the time and place the contract is to be awarded shall be given to the Bureau of Public Roads to enable it, if it so desires, to have a representative present. When a bid has been accepted prompt notice thereof shall be given to the Bureau of Public Roads.

Sec. 6. If the contract be awarded to any other than the lowest responsible bidder the Federal Government shall not pay more than its pro rata share of the lowest responsible

bid, unless it be satisfactorily shown that it was advantageous to the work to accept the higher bid.

Sec. 7. The specifications and plans shall be made a part of the contract.

Sec. 8. A copy of each contract, as executed, shall be promptly certified by the State highway department and furnished to the Secretary and no alteration in the contract shall be subsequently made without the approval of the Secretary.

Regulation 9.—Construction

Section 1. Suitable samples of materials to be used in construction work shall be submitted, by or on behalf of the State highway department, to the Bureau of Public Roads whenever requested.

Sec. 2. Unless otherwise stipulated in writing by the Secretary or his authorized representative, materials for the construction of any project shall be tested, prior to use, for conformity with specifications, according to methods prescribed or approved by the Bureau of Public Roads.

Sec. 3. No part of the money apportioned under the act shall be used, directly or indirectly, to pay or to reimburse a State, county, or local subdivision for the payment of any premium or royalty on any patented or proprietary material, specification, process, or type of construction unless purchased or obtained on open actual competitive bidding at the same or a less cost than unpatented articles or methods, if any, equally suitable for the same purpose.

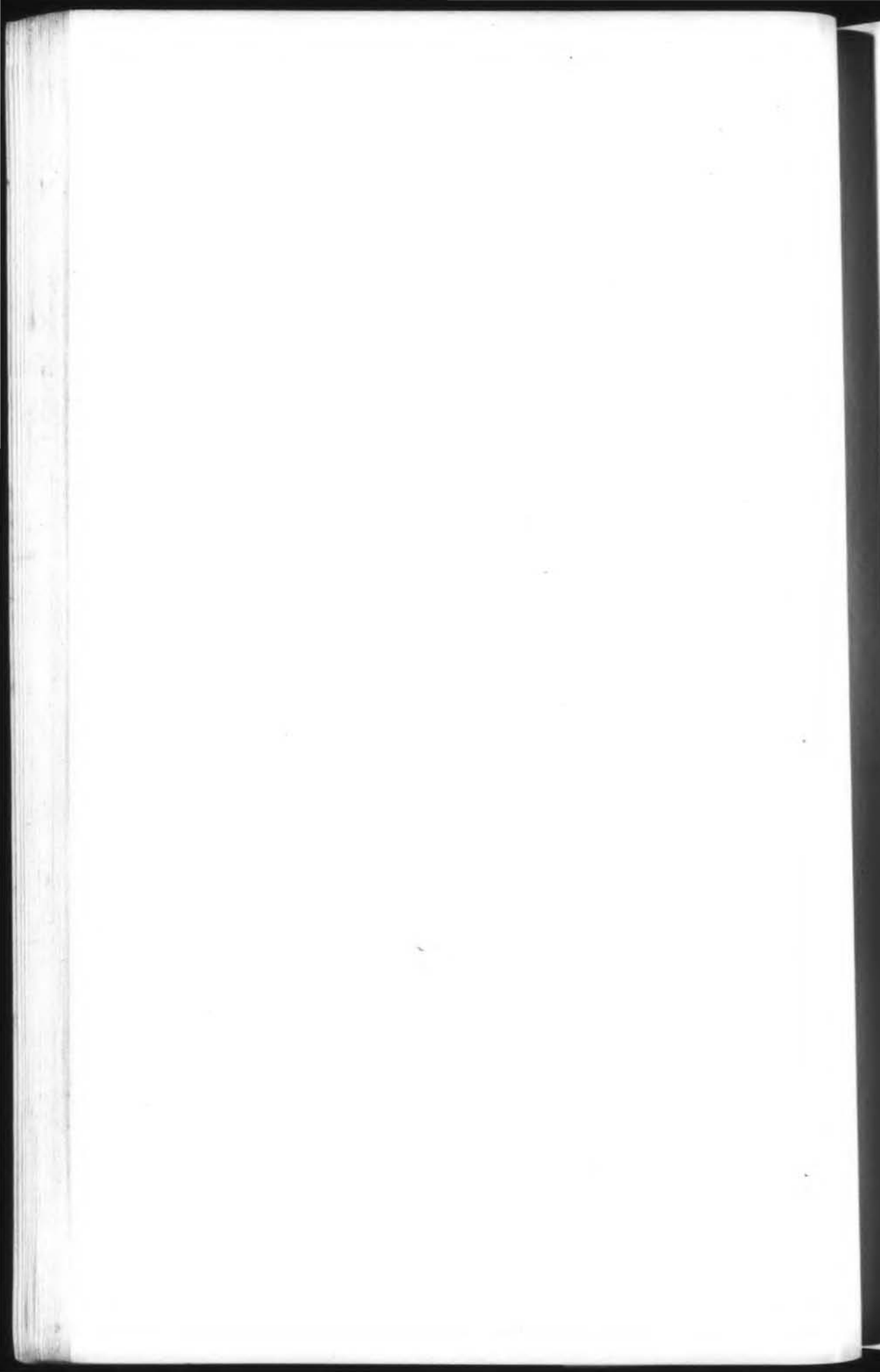
Sec. 4. The supervision of each project by the State highway department shall include adequate and continuous engineering inspection throughout the course of construction.

Sec. 5. Written notice of commencement and completion of work on any project shall be given promptly by the State highway department to the Bureau of Public Roads.

Sec. 6. Reports of the progress of construction, showing force employed and work done, shall be furnished as requested by the Secretary or his authorized representatives.



Rock Base Surface Treated, Road 3.



Regulation 10.—Records and Cost Keeping

Section 1. Such records of the cost of construction, of inspection, of tests, and of maintenance, done by or on behalf of the State, shall be kept, by or under the direction of the State highway department, as will enable the State to report, upon the request of the Secretary or his authorized representatives, the amount and nature of the expenditure for these purposes.

Sec. 2. The accounts and records, together with all supporting documents, shall be open at all times to the inspection of the Secretary or his authorized representatives, and copies thereof shall be furnished when requested.

Regulation 11.—Payments

Section 1. Vouchers, in the form provided by the Secretary and certified as therein prescribed, showing amounts expended on any project and the amount claimed to be due from the Federal Government on account thereof, shall be submitted by the State highway department to the Bureau of Public Roads, either after completion of construction of the project or, if the Secretary has determined to make payments as the construction progresses, at intervals of not less than one month.

Regulation 12.—Submission of Documents

Section 1. Papers and documents required by the act or these regulations to be submitted to the Secretary may be delivered to the Chief of the Bureau of Public Roads or his authorized representatives and, from the date of such delivery, shall be deemed submitted.

RAILROAD AGREEMENT.

This Agreement, made and entered into by and between the State Road Department of Florida, party of the first part, and Atlantic Coast Line Railroad Company, Seaboard Air Line Railway Company, Florida East Coast Railway Company and Louisville & Nashville Railroad Company, parties of the second part, witnesseth:

Whereas, said party of the first part is carrying on throughout the State of Florida a large program of highway improvement and construction; and

Whereas, the parties of the second part desire to co-operate with the party of the first part in the elimination as far as possible of grade crossing accidents; and

Whereas, the parties hereto recognized that these accidents usually occur on the main traveled highways; therefore,

It is agreed by and between the parties hereto as follows:

The parties hereto will co-operate with each other in the establishment, construction, relocation and repair of highway crossings under the jurisdiction of said State Road Department upon the following terms and conditions:

(1)

Whenever a State road crosses an existing railroad right-of-way, the State Road Department shall have the right to determine whether such State road and railroad shall cross each other at grade, or by over-pass or under-pass, under the terms and conditions of this agreement.

The State Road Department may under this agreement provide for the elimination of existing grade crossings of State roads over existing railroad tracks.

(2)

When it is provided that an over-pass crossing of a highway over an existing railroad shall be established, either as a new project or as the improvement of an existing crossing, the railroad involved shall be responsible for one-half of the entire cost of the bridge which shall span the opening over the tracks of the railroad from abutment to abutment



Rock Base Surface Treated, Road 9.



and including such abutments; and the State Road Department shall be responsible for the balance of the cost. The railroad company involved shall maintain the supporting abutments and bents of the bridge which shall span the opening over the tracks of the railroad from abutment to abutment; and the State Road Department shall maintain the balance of the structure. When an over-pass is constructed, it shall be so constructed as to be sufficient to accommodate at least two railway tracks; this requirement may be waived and construction for one railway track provided for at the option of the railway company.

(3)

When it is provided that an under-pass crossing of a highway under an existing railroad shall be established, either as a new project or as the improvement of an existing crossing, the railroad involved shall be responsible for one-half of the expense of all excavations through the existing railroad fill of the railroad right-of-way and one-half of the complete cost of the structure necessary to carry the railroad tracks on the railroad right-of-way, including the foundation, sub-structure and super-structure; and the State Road Department shall be responsible for the balance of the cost. The railroad company involved shall maintain the abutments and super-structure of such under-pass and the State Road Department shall maintain the roadway and drainage thereof. When an under-pass is constructed, it shall be so constructed as to be sufficient to accommodate at least two railway tracks; this requirement may be waived and construction sufficient for one railway track provided for at the option of the railway company.

(4)

When it is provided that a crossing at grade shall be established over a railroad, either as a new project or as a re-location of an existing crossing, the railroad company involved shall be responsible for one-half of the cost of such crossing between points ten (10) feet beyond the end of the cross ties on each side of its tracks; and the State Road Department shall be responsible for the balance of the cost.

The railroad company involved shall maintain the crossing between points ten (10) feet beyond the end of the crossings on each side of its tracks and the State Road Department shall maintain the balance of such crossing.

(5)

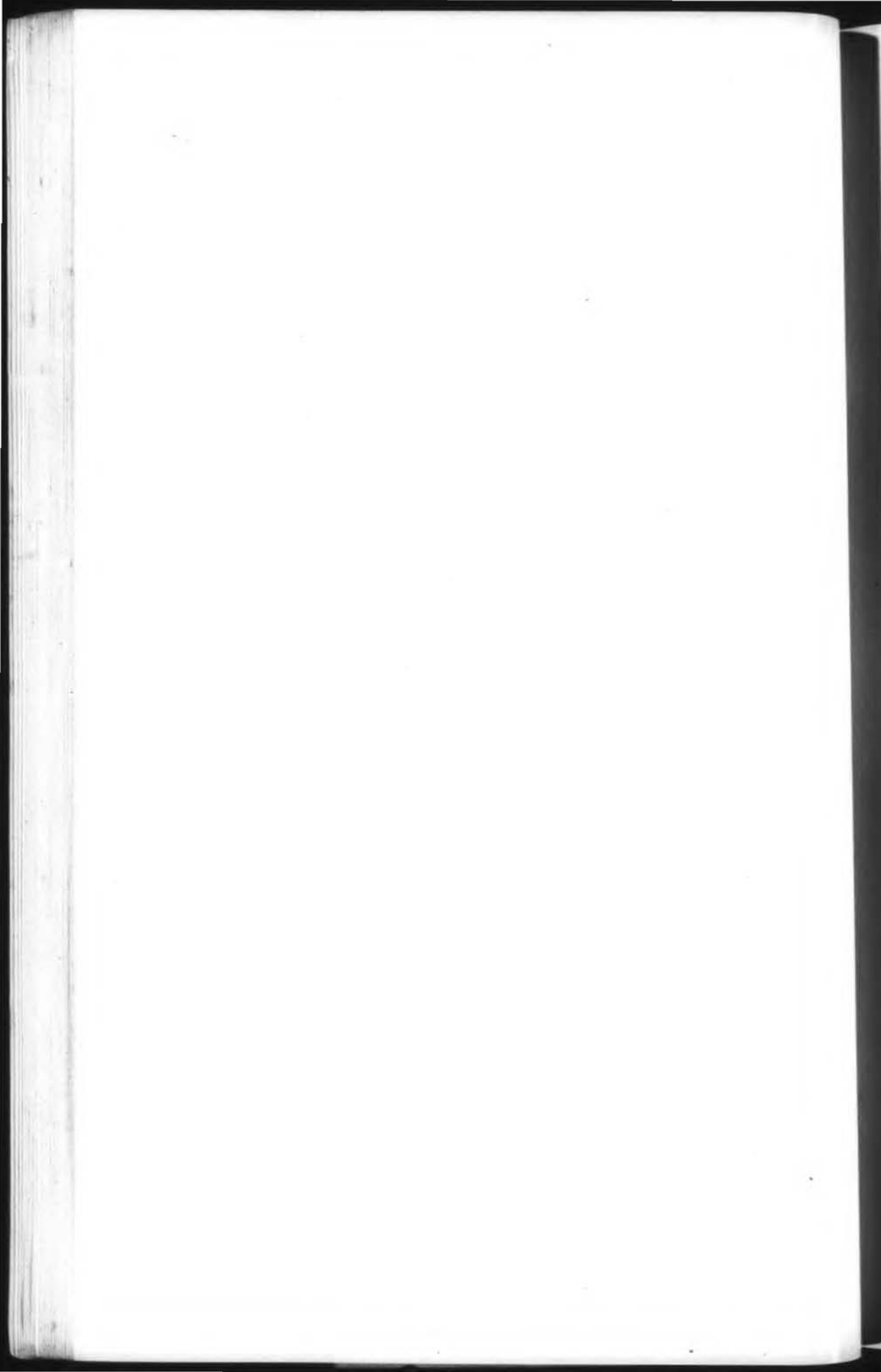
Where a new line of railroad owned or controlled by the parties hereto crosses an existing highway under the jurisdiction of the State Road Department, unless said State Road Department and the railroad shall agree for a crossing at grade (in which event section four shall apply), the railroad shall have the right to elect whether said highway shall be carried over or under the proposed railroad track. In case it elects that the highway shall be carried over the railroad, then the State Road Department shall bear one-half of the cost of the bridge spanning the railroad tracks, from abutment to abutment, including abutments, and the railroad shall provide the necessary fills to restore the existing width of the highway road-bed for the highway approaching said bridge, which shall be on a grade of five per cent unless otherwise agreed upon, and shall co-operate with the State Road Department in the reconstruction of the pavement disturbed to an amount equal to the appraised value of the existing pavement so disturbed. In case the railroad shall elect that the railroad shall be carried over the highway, then, and in that event, the State Road Department shall pay one-half of the cost of the railroad bridge spanning the highway, including the abutments thereto, and the railroad shall provide such fills for its tracks approaching said bridge as may be necessary.

The same policy is to prevail as to elimination of grade crossings over new railroad construction as is applied in the case of old railroad construction; that is, the elimination of a grade crossing over new railroad construction will be required only under the same circumstances and conditions as would cause the elimination of a grade crossing over old construction, and there shall be no discrimination as between the two types of construction.

The construction of additional track or tracks on an existing right-of-way of an existing railroad shall not be con-



Bituminous Concrete, Road 5.



sidered new construction, but crossing of highways under the jurisdiction of the State Road Department over such multiple track lines shall be governed by the provisions of Sections one, two, three and four hereof.

(6)

No railroad shall be obligated under this proposal and its acceptance to expend in any one calendar year a sum in excess of one-fifth of one per cent of its total gross operating revenue derived from business in the State of Florida, both interstate and intrastate, for the calendar year next preceding; provided, that this clause shall not preclude consideration of any crossing improvement by any railroad company or companies under this proposal, and the participation of such company or companies in such improvement provided the exigencies of the case and the circumstances of the company or companies, in the judgment of its officers, warrant such improvement.

(7)

The agreement shall be effective from and after this, the 3rd day of July, A. D. 1924.

(8)

Whenever an overhead bridge, under-pass, or grade crossing is substituted for an existing grade crossing, the existing grade crossing shall be eliminated, closed and vacated.

(9)

This agreement shall remain of force until terminated by one year's written notice from either party unto the other.

STATE ROAD DEPARTMENT OF FLORIDA.

By H. B. Phillips, Chairman.

Attest: Ella Cleary Thompson, Secretary.

ATLANTIC COAST LINE RAILROAD COMPANY.

By Lyman DeLano, Executive Vice-President.

SEABOARD AIR LINE RAILWAY COMPANY.

By W. L. Stanley, Vice-President.

FLORIDA EAST COAST RAILWAY COMPANY.

By H. N. Rodenbaugh, General Manager.

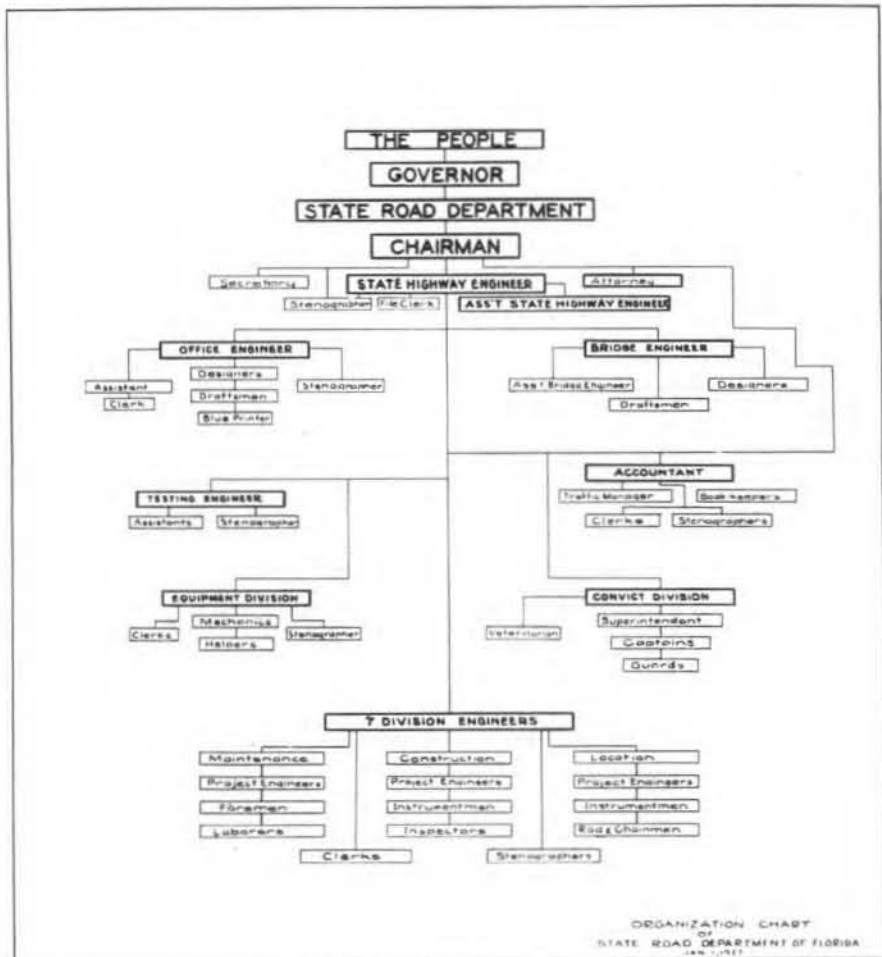
LOUISVILLE & NASHVILLE RAILROAD CO.

By W. L. Maporther, President.

ORGANIZATION CHART

OF

STATE ROAD DEPARTMENT OF FLORIDA



State Highway Engineer's Report

Tallahassee, Florida.

Hon. Robert W. Bentley, Chairman,
State Road Department,
Tallahassee, Florida.

Dear Sir:

I have the honor to submit to you the report of the State Highway Engineer for the period from January 1st, 1927, to December 31, 1928.

Respectfully submitted,
J. L. Cresap,
State Highway Engineer.

Organization

The Legislature of Florida at its 1923 session passed a law defining the organization of the State Road Department as well as that of the employees and fixed the salaries of these. The Department is composed of five members, one of whom is appointed from each of the four Congressional districts, and one from the State at large. The members select one of their number as chairman, who is the executive head of the Department. The Department is empowered to employ assistants for the purpose of prosecuting the work. These employees consist of a Secretary, State Highway Engineer, Assistant State Highway Engineer, Division Engineers, Testing Engineer, Office Engineer, Bridge Engineer, Auditor, Superintendent of Equipment, and other assistants necessary for the proper prosecution of the work.

Surveys and Plans

The Legislature has laid out a very definite system of roads on which State funds may be spent on construction. The Legislature also authorized or directed this Department to make survey of any roads, or section of roads, not on the Preferential Systems wherever and whenever counties made available monies for the construction of same. The Department has made necessary survey for all work done on preferential and secondary systems in accordance with the

law, and has always made surveys for the counties, but during the past two years it has made numerous surveys at the request of various counties that had the funds to spend on the roads, and, in addition to the survey, prepared all detail plans, furnished estimates and specifications and supervised the work. This is shown in the accompanying table of Surveys and Plans.

In connection with surveys, the first thing is the proper location of a proposed new road, and in making locations the Department has attempted to make them as safe as possible by good alignment, as well as selecting the most economical line on which the highway may be constructed. The matter of safety is the elimination of sharp curves, railroad crossings and dangerous intersections with cross roads on the system. On Road No. 1, by using care and judgment in location, between Jacksonville and Pensacola, twenty-two railroad crossings were eliminated—eight by constructing overhead crossings and three by building under-passes. On State Road No. 4, between St. Augustine and Miami, we eliminated by relocation twenty-four grade crossings and two by constructing overhead crossings. On these two roads alone the Department has eliminated the hazards attached to fifty-nine grade crossings. These are only a few of the grade crossings that have been eliminated in the location and construction of trunk line roads to date. After a location is determined and approved, a careful study is made by the field engineers of all conditions entering into the construction of the road. Accurate surveys to determine drainage are made, soil conditions that would affect grade and foundation are studied to determine type of structure. The data is then assembled and detail plans are prepared in the main office. Care and study is given in the preparation of the plans by experienced men, thereby securing a saving in the ultimate cost of the work. The accompanying tables show the number of sets of plans prepared in the past two years, together with the number of miles of road that they cover. The same standard is used throughout in preparing plans and making estimates on county projects as on State; the Department having only one standard.



Dora Canal Bridge, Federal Aid Road 2.



STATE ROAD DEPARTMENT OF FLORIDA
PLANS PREPARED FOR ROADS—1927

Project	Road	County	Length
55	14	Alachua.....	16.77
60-A	4	Taylor-Volusia.....	11.64
61-A	1	Gadsden.....	10.00
61-C	1	Gadsden.....	9.77
62-D	24	Osceola.....	12.66
62-C	24	Osceola.....	11.83
502	6	Gulf.....	6.94
518	5-A	LaFayette.....	17.75
...	49	Union.....	9.17
522	3	Nassau.....	4.06
535	5-A	LaFayette.....	13.19
585	1	Santa Rosa.....	5.82
585*	1	Santa Rosa.....	5.18
609	2	Hardee.....	7.90
615	5	Sarasota.....	11.92
616	2	DeSoto.....	5.85
624	50	Hamilton.....	6.23
669-D	27	Dade.....	12.30
669-Z	27	Collier.....	4.44
669-W	27	Collier.....	20.00
676-C	19	Levy.....	1.02
683-A	4	Palm Beach.....	8.04
700	19	Jefferson.....	9.26
704	3	Orange.....	3.88
706-A	28	Putnam.....	12.09
706-B	28	Putnam.....	14.91
709	39	Holmes.....	14.27
709*	39	Holmes.....	9.08
710-A	17	Hillsborough.....	12.04
710-C	17	Hillsborough.....	10.73
713	28	Columbia.....	10.00
714	28	Union.....	10.20
715	28	Union.....	3.70
716	28	Bradford.....	11.12
717	28	Bradford.....	12.00
719	5-A	Suwannee.....	8.57
720	11	Jefferson.....	9.64
722	43	Jefferson.....	8.83
723	66	Leon.....	11.76
724	66	Leon.....	10.67
724*	66	Leon.....	11.10
725	10	Wakulla.....	12.57
732	17	Polk.....	9.47
733	33	Walton.....	4.23
734	40	Walton.....	7.89
735	40	Walton.....	13.71
736	39	Holmes.....	3.41
736*	39	Holmes.....	8.97
738	42	Jefferson.....	10.17

STATE ROAD DEPARTMENT OF FLORIDA
PLANS PREPARED FOR ROADS—1927 (Continued)

Project	Road	County	Length
740	10	Gulf.....	9.62
742	13	Alachua.....	7.65
743	10	Bay.....	18.25
744	19	Madison.....	5.78
745	19	Taylor.....	15.95
747	35	Jefferson.....	6.50
748	35	Madison.....	6.21
749	14	Gilchrist.....	6.75
749*	14	Gilchrist.....	7.81
750*	14	Gilchrist.....	12.97
751	40	Walton.....	7.28
752	40	Walton.....	8.73
755	17	Polk.....	11.22
757	2	Polk.....	10.26
758	2	Polk.....	9.89
762	2	Osceola.....	8.96
763	50	Suwannee.....	12.23
764	50	Suwannee.....	12.00
765	50	Suwannee.....	7.00
766	10	Bay.....	8.74
767	10	Bay.....	5.27
768	40	Walton.....	2.97
776	18	Manatee.....	6.57
780	29	Okeechobee.....	11.00
781	29	Okeechobee.....	11.00
782	29	Okeechobee.....	6.62
786	6	Jackson.....	6.76
787	10	Walton.....	16.30
807-A	25	Palm Beach.....	10.81
807-C	25	Palm Beach.....	6.14
827	0	Escambia.....	8.12
Total miles.....			762.11

Note—*Made over account of revision.



Rock Base Surface Treated, Road 8.



STATE ROAD DEPARTMENT OF FLORIDA
PLANS PREPARED FOR ROADS—1928

Project	Road	County	Length
6	1	Madison	5.48
52	1	Escambia	10.08
60-C	4	Volusia	9.64
62-A	24	Osceola	12.52
500-B	20	Bay	12.75
500-C	20	Bay	12.18
...	49	Union	8.60
...	49	Union	7.68
...	49	Union	13.90
587	5-A	Columbia	4.34
601	0	St. Johns	16.94
603	14	Putnam-St. Johns	7.09
608-B	4	Brevard	2.00
644-C	10	Wakulla	5.05
669-Y	27	Collier	13.55
669-Y*	27	Collier	13.55
672	1	Leon	9.92
673	1	Gadsden	14.95
678	10	Bay	8.72
688	10	Bay	9.31
690	12	Gadsden	12.22
696	8	St. Lucie	14.23
696*	8	St. Lucie	12.99
702	10	Franklin	8.83
703	10	Wakulla	4.35
711	58	Gadsden	5.62
712	10	Wakulla	8.53
718	5-A	Columbia	8.22
725	10	Wakulla	12.66
737-A	39	Washington	8.27
739-A	39	Washington	9.84
739-C	39	Washington	10.45
762	2	Osceola	8.96
788	10	Walton	17.54
789	15	Walton	7.78
792-A	15	Levy	10.92
793-A	15	Citrus	10.90
793-C	15	Citrus	7.04
793-D	15	Citrus	7.99
798	13	Nassau	15.30
799	28	Putnam	6.18
800	28	Flagler	17.55
802-A	10	Okaloosa	7.76
802-A*	10	Okaloosa	6.00
802-C	10	Okaloosa	10.24
803	10	Okaloosa	11.13
805	25	Hendry	9.42
806-A	25	Hendry	11.00
806-C	25	Hendry	11.00

STATE ROAD DEPARTMENT OF FLORIDA
PLANS PREPARED FOR ROADS—1928 (Continued)

Project	Road	County	Length
806-D	25	Hendry.....	12.69
808	44	Brevard.....	7.38
809	44	Volusia.....	5.03
812-A	12	Franklin.....	10.35
812-C	12	Franklin.....	11.34
816-A	90	Jackson.....	10.70
816-C	90	Jackson.....	10.90
817	14	Alachua.....	9.69
818	26	Glades.....	17.79
819	54	Okaloosa.....	4.69
820	96	Jefferson.....	9.45
821	96	Jefferson.....	5.18
823	41	Okaloosa.....	9.18
824	41	Okaloosa.....	9.81
825	88	Holmes.....	10.37
826	88	Holmes.....	10.83
828	15	Wakulla.....	7.19
829	76	Leon.....	7.99
830	66	Liberty.....	12.48
831	115	Bay.....	7.16
832	10	Santa Rosa.....	16.01
833	10	Santa Rosa.....	8.01
834	52	Jackson.....	16.04
836	110	Wakulla.....	3.21
840	115	Walton.....	10.45
840*	115	Walton.....	10.20
841	115	Walton.....	6.44
842	115	Walton.....	10.15
843	12	Gadsden.....	8.90
844-C	115	Okaloosa.....	5.63
845	19	Taylor.....	8.57
854	60	Walton.....	9.81
855	60	Walton.....	9.32
Total miles.....			804.11



Rock Base Surface Treated, Road 10.



Federal Aid

The Federal Government aids the several states in the construction of certain trunk line highways in accordance with certain acts passed by the Congress of the United States and under the rules and regulations adopted by the Bureau of Public Roads and approved by the Secretary of Agriculture. The roads on which Federal Aid may be applied comprise what is designated as the Federal Seven Per-cent System, that is, this system may comprise not more than seven per cent. of the total public road mileage of the State. According to the present regulations, no Federal funds may be applied to any roads other than those on this seven per cent. system until this system has been completed. This does not mean that all the roads on this seven per cent. system shall necessarily be built with the use of Federal aid, but may be built with state or county funds, but must be built of a type and in a manner acceptable to the Bureau of Public Roads. The maximum participation of the Federal Government in the cost of any road is fifteen thousand dollars per mile, not including bridges having a clear span of more than twenty feet. The Government will participate in the cost of bridges having a clear span of more than twenty feet to fifty per cent. of the cost of such bridge. There seems to be an erroneous idea as to the amount of Federal aid a State may receive during a certain year. Regardless of the amount of money which may be made available by the State authorities, only a certain fixed amount may be received from the Government. Florida has been receiving something less than nine hundred thousand dollars per year for the last four years and may expect, under existing Federal legislation, to receive a similar amount for two years to come. The Federal aid is apportioned to the several States in the following manner: One-third in the ratio which the area of each State bears to the total area of all the States; one-third in the ratio which the population of each State bears to the total population of all the States as shown by the latest available Federal census, and one-third in the ratio which the mileage of rural delivery routes and star routes in each State bears to the total mileage of rural de-

livery and star routes in all the States at the close of the next preceding fiscal year as shown by certificate of the Postmaster-General, which he is directed to make and furnish annually to the Secretary of Agriculture.

It will be noted that during the past four years, although operating under laws which made available the same amount annually as Federal aid, Florida's proportion has not been exactly the same during any two years. This is due to the increased mileage of rural delivery and star routes as well as to certain refunds in the way of contingencies which were not used up and were turned back to the general fund and reapportioned to the several States.

The Federal Government does not participate in the cost of the surveys necessary in the location of the project, the making of the plans and specifications, nor in the maintenance of the road after it has been built. The Government does, however, exercise certain jurisdiction over Federal Aid projects after completion and, according to law, requires that they be maintained in an acceptable manner.

Certain tables found in this report will show the amount of Federal Aid appropriated annually since the enactment of the Federal Aid Road Act, as well as the amount which has been received by the State of Florida.



Plain Cement Concrete, Federal Aid Road 3.



STATE ROAD DEPARTMENT OF FLORIDA
United States Bureau of Public Roads
Summary of Appropriation to All the States

Year	Law of 1916	Amendment 1919	Amendment 1921	Amendment 1922	Entire United States
1917.....	\$ 5,000,000.00	\$	\$	\$	\$ 5,000,000.00
1918.....	10,000,000.00	.....	.....	.....	10,000,000.00
1919.....	15,000,000.00	50,000,000.00	.....	.....	65,000,000.00
1920.....	20,000,000.00	75,000,000.00	.....	.....	95,000,000.00
1921.....	25,000,000.00	75,000,000.00	25,000,000.00	.....	125,000,000.00
1922.....	.....	.....	50,000,000.00	.....	50,000,000.00
1923.....	.....	.....	.....	50,000,000.00	50,000,000.00
1924.....	.....	.....	.....	65,000,000.00	65,000,000.00
1925.....	.....	.....	.....	75,000,000.00	75,000,000.00
1926.....	.....	.....	.....	75,000,000.00	75,000,000.00
1927.....	.....	.....	.....	75,000,000.00	75,000,000.00
1928.....	.....	.....	.....	75,000,000.00	75,000,000.00
1929.....	.....	.....	.....	75,000,000.00	75,000,000.00
Total.....	\$ 75,000,000.00	\$ 200,000,000.00	\$ 75,000,000.00	\$ 490,000,000.00	\$ 840,000,000.00

PRO RATA ALLOTTED TO FLORIDA

Year	Law of 1916	Amendment 1919	Amendment 1921	Amendment 1922	Total to Florida
1917.....	\$ 55,976.27	\$	\$	\$	\$ 55,976.27
1918.....	111,952.54	.....	.....	.....	111,952.54
1919.....	170,723.88	573,797.20	.....	.....	744,521.08
1920.....	229,518.88	860,695.79	.....	.....	1,090,214.67
1921.....	286,861.98	860,585.94	.....	.....	1,147,447.92
1922.....	.....	.....	886,825.69	.....	886,825.69
1923.....	.....	.....	.....	599,221.13*	599,221.13
1924.....	.....	.....	.....	771,395.18	771,395.18
1925.....	.....	.....	.....	887,336.52	887,336.52
1926.....	.....	.....	.....	892,878.00	892,878.00
1927.....	.....	.....	.....	897,185.00	897,185.00
1928.....	.....	.....	.....	899,451.00	899,451.00
1929.....	.....	.....	.....	901,311.00	901,311.00
Total.....	\$ 855,033.55	\$ 2,295,078.93	\$ 886,825.69	\$ 5,848,777.83	\$ 9,885,716.00

Note—*Includes \$8,004.00 Reapportionment from Montana.

STATE ROAD DEPARTMENT OF FLORIDA
RECORD OF FEDERAL AID PROJECTS TO DECEMBER 31, 1928

Road No.	Project No.	County	Length Miles	Type	Total Cost Estimated	Amount Requested	Amount Granted	Amount Secured	Final Payment Made
1	1	Oklaosa	10.45	Sand-Clay	\$ 34,083.28	\$ 17,041.64	\$ 17,041.64	\$ 16,938.10	May, 1919.
2	2	Osceola	4.12	Brick	69,678.53	20,000.00	20,000.00	20,000.00	April 15, 1925.
1	3	Gadsden-Jackson	0.19	Concrete Arch.	417,031.12	208,515.56	208,515.56	208,515.56	Nov. 25 1924.
2	4	DeSoto	3.03	Concrete	66,005.98	30,000.00	30,000.00	30,000.00	March 14, 1925.
18	5	DeSoto	2.50	Concrete	46,879.17	20,000.00	20,000.00	20,000.00	May 15, 1926.
1	6-A	Madison	5.449	Earth Graded	357,145.38	112,103.86	112,103.86	46,996.08	.....
1	6-B	Jefferson-Madison	0.078	Concrete Bridges					
1	7-A-B	Hamilton	13.047	Rock Base, Concrete Arch					
2	8	Alachua	11.73	Bituminous Macadam	467,299.91	227,826.56	227,826.56	217,421.13	Aug. 8, 1927.
1	9	Holmes	5.11	Sand Clay	217,155.38	108,577.69	108,577.69	108,577.69	Sept. 29, 1923.
1	11	Baker-Nassau-Duval	15.40	Concrete	25,525.06	12,762.53	12,762.53	12,762.53	March 1, 1921.
2	13	Orange	8.59	Sheet Asphalt	560,421.41	281,710.70	281,710.70	248,400.01	Feb. 18, 1927.
1	14-A-B	Santa Rosa	6.40	Brick, Concrete & Steel	305,686.15	152,843.07	152,843.07	134,518.71	Oct. 25, 1926.
2	15	DeSoto	3.26	Concrete	284,174.26	142,087.13	142,087.13	139,691.18	Sept. 15, 1923.
18	16	DeSoto	0.67	Concrete	54,570.67	20,000.00	20,000.00	20,000.00	May 28, 1926.
5	17	Manatee	7.87	Bituminous Macadam	11,798.80	5,899.40	5,899.40	5,245.57	May 28, 1926.
3	18	Putnam	12.79	Bituminous Macadam	264,953.11	132,476.55	132,476.55	103,867.12	July 29, 1926.
2	19-A	Hamilton	8.162	R. B. Surface Treated	454,730.74	227,365.37	227,365.37	209,245.28	Feb. 18, 1927.
2	19-B	Hamilton	0.032	Concrete Bridges	370,976.46	185,488.23	185,488.23	154,128.95	Aug. 20, 1926.
1	20	Holmes & Washington	0.38	Concrete					
1	21-A	Baker	5.70	Concrete					
1	21-B	Baker	0.064	Concrete Bridges	234,656.07	117,328.03	117,328.03	102,240.83	May 28, 1926.
1	22	Baker	8.90	Concrete	284,984.50	142,492.25	142,492.25	126,671.93	Aug. 16, 1926.
1	23	Duval	12.80	Concrete	467,017.29	233,508.64	233,508.64	220,570.86	Sept. 11, 1924.
1	24	Columbia	8.98	Bituminous Macadam	348,171.92	174,085.96	174,085.96	143,595.12	June 5, 1924.
2	26	Columbia	11.01	Concrete	370,697.14	178,358.52	178,358.52	167,785.81	July 30, 1928.
2	27-A-B	Columbia	12.36	Concrete	541,991.98	252,245.22	252,245.22	252,245.22	Nov. 28, 1924.

5	29-A-B	Hillsborough	0.06	Concrete	148,680.79	74,340.39	74,340.39	74,340.39	June 26, 1926.
2	30	Columbia-Hamilton	0.061	Concrete & Steel	53,983.38	26,991.69	26,991.69	26,991.69	March 31, 1924.
2	31	Hamilton	11.81	R. B. Surface Treated	294,287.60	144,982.64	144,982.64	138,448.65	Aug. 9, 1927.
4	32	Nassau	10.00	Bituminous Macadam	369,717.21	184,858.60	184,858.60	179,131.29	Oct. 13, 1926.
2	33	Alachua	12.60	Bituminous Macadam	358,844.39	179,422.19	179,422.19	171,863.17	May 28, 1926.
7	34	Escambia	10.00	Concrete	297,291.77	148,645.88	148,645.88	139,795.19	Feb. 28, 1925.
1	35	Escambia	5.00	Concrete	152,411.86	76,205.93	76,205.93	72,890.22	Feb. 27, 1925.
4	36-A	St. Lucie	7.76	Bituminous Macadam	276,924.40	138,462.20	138,462.20	138,462.20	March 15, 1927.
4	36-B	St. Lucie	7.12	Concrete	288,228.60	142,420.00	142,420.00	138,738.49	Aug. 25, 1927.
2	37-A-C	Alachua	3.96	Sheet Asphalt	180,779.29	90,300.93	90,300.93	87,880.36	Oct. 26, 1926.
2	37-B	Alachua	0.189	Concrete	39,256.28	19,628.14	19,628.14	9,540.36	Dec. 26, 1928.
2	37-D	Alachua	2.14	R. B. Surface Treated	172,898.02	48,347.07	48,347.07	48,184.57	Dec. 20, 1927.
2	37-E	Alachua	7.96	Sheet Asphalt	332,870.41	159,240.00	159,240.00	159,240.00	Oct. 21, 1926.
1	38-A	Escambia	3.20	Timber Bridges, Approach Embankment	1,061,100.73	530,550.36	530,550.36	491,681.26	Dec. 13, 1927.
1	38-B	Escambia	0.29	Steel Bridge					
4	39-A-B	Brevard & St. Lucie	0.426	Concrete & Dredged Embankment	232,913.55	116,456.77	116,456.77	113,419.87	Dec. 13, 1927.
4	40-A	Brevard	16.169	R. B. Surface Treated	446,546.93	223,273.46	223,273.46	223,273.46	Jan. 21, 1928.
4	40-B	Brevard	0.025	Concrete	58,631.83	29,315.91	29,315.91	0.00	.....
4	40-D	Brevard	6.727	R. B. Surface Treated	158,783.06	79,391.53	79,391.53	78,601.17	Feb. 13, 1928.
4	40-E	Brevard	13.596	R. B. Surface Treated	315,218.29	157,609.14	157,609.14	157,609.14	March 21, 1927.
4	41-A	Dade	12.00	Sheet Asphalt	692,319.40	214,906.05	214,906.05	214,006.05	Oct. 25, 1928.
4	41-B	Dade	0.043	Concrete & Steel	108,036.83	54,018.41	54,018.41	45,669.80	.....
2	43	Marion	10.534	Sheet Asphalt	381,991.44	173,581.29	173,581.29	171,195.12	Jan. 21, 1928.
2	44	Lake	10.529	Bituminous Concrete	395,611.38	165,565.84	165,565.84	165,557.71	Jan. 21, 1928.
1	45	Suwannee & Madison	0.173	Concrete & Steel	124,902.27	62,451.13	62,451.13	55,526.90	.....
3	46	Nassau	11.526	Concrete	495,071.06	197,195.15	197,195.15	196,348.25	April 16, 1928.

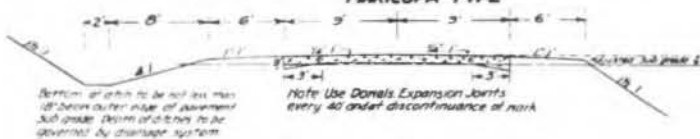
STATE ROAD DEPARTMENT OF FLORIDA
RECORD OF FEDERAL AID PROJECTS TO DECEMBER 31, 1928 (Continued)

Road No.	Project No.	County	Length Miles	Type	Total Cost Estimated	Amount Requested	Amount Granted	Amount Secured	Final Payment Made
4	47	St. Johns	14.88	Concrete	\$ 575,344.40	\$ 241,071.37	\$ 241,071.37	\$ 241,068.81	July 2, 1928.
4	48-A	St. Johns	15.855	R. B. Surface Treated	474,454.83	234,675.00	234,675.00	225,937.75	Aug. 8, 1928.
4	48-B	St. Johns	0.086	Concrete	82,700.91	41,350.45	41,350.45	40,221.04	July 7, 1928.
4	49-A	Flagler	13.774	R. B. Surface Treated	435,829.97	217,914.96	217,914.96	216,883.12	Aug. 8, 1928.
4	49-B	Flagler	0.039	Concrete Bridges					
14	50-A	Putnam	6.556	R. B. Surface Treated	249,606.55	117,371.26	117,371.26	30,626.83	
14	50-B	Putnam	9.778	R. B. Surface Treated	310,949.01	155,474.50	155,474.50	139,465.90	
14	50-C	Putnam	10.030	R. B. Surface Treated	268,014.78	134,007.39	134,007.39	133,495.12	Dec. 12, 1928.
7	51	Escambia	6.287	Concrete	206,575.47	94,302.00	94,302.00	94,260.00	July 11, 1927.
1	52	Escambia	10.089	Earth Graded	240,716.49	120,358.24	120,358.24	105,162.30	
2	53-A	Lake	7.108	Bituminous Concrete	309,286.93	106,620.00	106,620.00	84,956.20	
2	53-B	Lake	0.127	Concrete Deck Girder	112,903.37	56,451.68	56,451.68	41,642.25	
2	53-C	Lake	1.870	Bituminous Concrete	113,087.04	28,095.00	28,095.00	24,286.53	
1	54-A	Leon	6.236	R. B. Surface Treated	131,688.98	60,844.49	60,844.49	60,844.49	July 2, 1928.
14	55-A	Alachua	16.120	Earth Graded	193,098.47	96,549.23	96,549.23	77,721.75	
14	55-B	Alachua	0.315	Concrete Bridges					
10	56	Leon	17.928	Concrete	415,722.75	207,861.37	207,861.37	50,858.46	
1	58	Jefferson	6.309	R. B. Surface Treated	132,835.49	66,417.74	66,417.74	61,375.50	June 18, 1928.
1	61-A	Gadsden	10.000	Concrete	230,381.36	115,190.68	115,190.68	99,033.00	
1	61-C	Gadsden	9.779	Concrete	236,044.21	118,022.10	118,022.10	94,220.29	
24	62-A	Osceola	12.247	Earth Graded	136,719.47	68,359.73	68,359.73	0.00	
24	62-C	Osceola	12.091	Earth Graded	120,874.82	60,437.40	60,437.40	42,244.15	
24	62-D	Osceola	12.665	Earth Graded	127,550.89	63,775.44	63,775.44	28,584.90	
3	421	Nassau	0.106	Concrete & Steel	258,417.08	129,208.54	129,208.54	87,248.00	
54	Forest 2-B	Okaloosa	13.580	Earth Graded	87,840.23	26,387.37	26,387.37	9,309.74	
Totals			556.825		\$19,383,345.35	\$ 9,116,037.82	\$ 9,116,037.82	\$8,104,391.51	

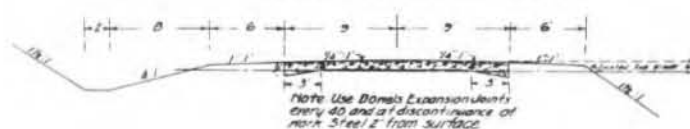


Rock Base Surface Treated. Federal Aid Road 2.

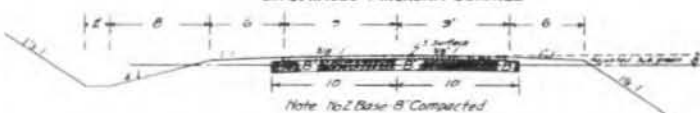
TYPICAL CROSS SECTIONS

PLAIN CONCRETE SURFACE
MARICOPA TYPE

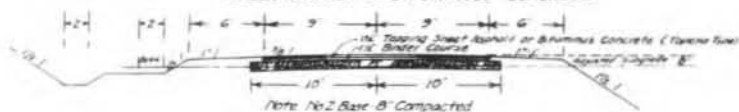
REINFORCED CONCRETE SURFACE



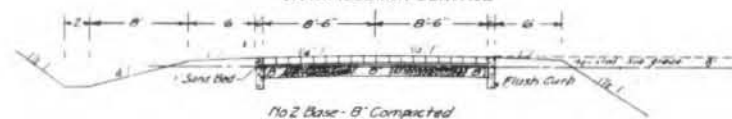
BITUMINOUS MACADAM SURFACE



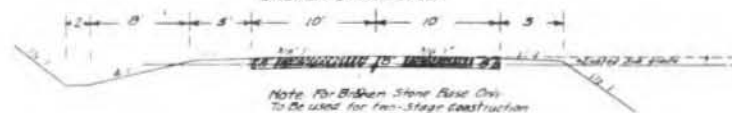
SHEET ASPHALT OR BITUMINOUS CONCRETE



VITRIFIED BRICK SURFACE



BROKEN STONE BASE





Rock Base Surface Treated, Road 1.



Road Construction

During the 1923 session of the Legislature of Florida an Act was passed designating a definite system of roads and at the same time specified the roads upon which the Department might expend State funds. In 1925 this Act was amended, but the general policy was adhered to. A copy of this latter Act may be found in this report.

At the beginning of this biennium conditions for road construction were very much improved over conditions in the beginning of the past biennium, and the Department had under construction a considerable mileage of roads of all classes of construction work. Under the organization of the Department, general supervision is handled from the headquarters office in Tallahassee, with the State divided into seven Districts, under the direct supervision of a Division Engineer to each. Division Officers handle all construction work in their districts and are furnished with necessary Project Engineers to supervise and inspect the work on the individual projects. In the handling of construction work it is necessary that all locations be restaked and construction stakes be set for the contractor or State forces to work by, and the field inspection of all material entering into the work as well as the method and manner in which the work is performed.

In the beginning of this biennium, owing to the amount of work that was under way, a large field personnel was essential. In the latter part of this biennium it has been necessary to release quite a number of the Project Engineers, Inspectors, Instrumentmen and Assistants.

The specifications adopted by the State Road Department, approved by the Bureau of Public Roads, who joins this Department in inspection, both of the survey and plans and the construction on all Federal Aid projects, are recognized standards, and the results obtained are evidence of the care which is used in seeing that the State gets value received for the money spent.

The building of roads in Florida presents many problems which must be overcome. These problems are in the nature

of soil conditions in certain sections that affect the stability of the road bed upon which a pavement is laid, the numerous streams and rivers necessary to cross, and the providing of adequate drainage to insure a thorough drainage of the sub-base of roads. The progress of the work is materially affected by these local conditions.

In the construction of bridges, in many cases the most difficult work and the costliest work is under the ground, or under water, and the average person does not realize the necessarily great cost of this part of construction, but in the building of a bridge the foundations are the main factors, both as to cost and permanency of the structure.

In the construction of a highway, regardless of the type of surface, the success depends upon the foundation upon which it is laid. This foundation is not necessarily the base material, but is the sub-base or sub-grade upon which it is laid, and this, in turn, is a part of the main grade. Therefore, the Department has always attempted to construct its roads from the bottom of the ditches to the top, and extreme care and close supervision have always been given to the grading of our roads, and it is believed that the result obtained has justified this. Whenever possible, and in the majority of cases it has been done, roads have been graded and drainage structures built in advance of the surfacing. It is felt that at least twelve months should elapse between the grading of the roads and the surfacing. In all cases this has not been possible, but in no case should a pavement be laid on a new grade within six months. Following out this policy, you will note from the accompanying tables the Department has over six hundred miles of roads graded, which are ready, or will be ready for surfacing in the near future.

The Department's standards, as shown by the typical sections and specifications, include many types of standard pavement, and it has built quite a number of miles of each type. In the construction of pavements of any type, a careful control of the materials entering into the work is maintained through the use of our laboratory, which is located at Gainesville, Florida, in charge of a Testing Engineer.



Bituminous Macadam, with Flush Curb, Road 4.



Preliminary tests of all materials are made to determine the quality and the conformity of the same to the specifications before being used, and field control is maintained by drawing of subsequent samples and the making of tests. On concrete pavement and hot asphalt mixtures, field laboratories are maintained that keep a daily check on the analysis of the materials, and in this manner a definite control of the mixture is maintained.

All pavements are designed to meet certain requirements as to load and traffic, and by these laboratories and field plants, in charge of competent men, this is insured.

The Department recognizes the value of road building materials found in this State and tries to make use of them at all times where conditions will warrant their use and the quality of the materials will give results within the limits of the specifications. Florida limerock has been used very extensively as a base under all asphaltic surfaces and it has proven equal to any other base yet devised for this purpose. The tables in this report show the length, type and location of the various sections of roads constructed, together with the location, length and type of bridges constructed.

Referring to laws designating a State Road System and taking contemplated roads as designated by the last Legislature, it is found that this system has a total length of approximately 8,524 miles. This system is divided into the First and Second Preferential roads and other roads known as State Roads. The First Preferential comprises 2,588 miles; the Second Preferential, 883 miles, and the remainder, 4,053 miles. The length on the Preferential Systems is the distance continuous through the cities and towns throughout the length of the various roads comprising them. Taking the information contained in the accompanying tables of work completed, the work under construction, both for grading and paving, the status of the First Preferential System is as follows: 1,588 miles paved by the State; 59 miles paving under construction; 214 miles graded by the State ready for paving; 78 miles grading under construction; 426 miles paved by the counties, which is now being used as part of the System, and 42 miles on which no work has been done;

making a total of 2,407 construction miles on the First Preferential System.

Second Preferential System: 132 miles paved by the State; 27 miles of paving under construction; 234 miles graded by the State and ready for paving; 119 miles grading by the State under construction; 96 miles paved by the counties and being used as part of the system, and 266 miles on which no work has been done; making a total of 874 construction miles on the Second Preferential System.

From this it is noted that out of 3,281 miles comprising the First and Second Preferential Systems there has been paved by the State and counties, 2,242, leaving a balance of 1,039 miles to be paved.

Of the 2,242 miles of paved roads, it is noted that 522 miles have been paved by the counties. This mileage has an economic life varying from 2 years to 15 years. There is approximately only 4% that has a life of 15 years, and the average life of the remainder is approximately 5 years.



Rock Base Surface Treated, Road 3.



STATE ROAD DEPARTMENT OF FLORIDA
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1928.

Project No.	Road No.	County	Length Miles	Type	Cost	County Participation
1	1	Okaloosa	10.40	Sand Clay	\$ 33,901.45	\$ 9,989.40
2	2	Osceola	4.13	Brick Surface Treated	125,693.90	15,783.38
4	2	DeSoto	3.03	Concrete	92,576.69	62,017.12
5	18	DeSoto	2.5	Concrete	50,431.49	45,542.38
7	2	Hamilton	13.00	Rock Base Surface Treated	324,656.41	78,926.15
8	2	Alachua	11.73	Bituminous Macadam	250,517.42	49,918.10
9	1	Holmes	5.11	Sand Clay Surface Treated	50,611.12	9,987.49
11	1	Baker-Nassau-Duval	15.39	Concrete	460,326.53	19,393.03
13	2	Orange	8.59	Sheet Asphalt	287,517.51	0.00
14	1	Santa Rosa	6.02	Brick	224,297.62	64,546.27
15	2	DeSoto	3.28	Concrete	70,356.13	46,000.00
16+ Ex.	18	DeSoto	9.17	Rock Base Surface Treated	127,133.45	18,500.00
17	5	Manatee	7.07	Bituminous Macadam	202,712.75	0.00
18	3	Putnam	12.80	Bituminous Macadam	456,110.33	143,543.42
19	2	Hamilton	8.20	Rock Base Surface Treated	242,658.59	78,926.15
21	1	Baker	5.68	Concrete	171,713.65	19,393.02
22	1	Baker	8.90	Concrete	250,774.05	16,333.03
23	1	Duval	12.80	Concrete	450,813.63	0.00
24	1	Columbia	9.89	Bituminous Macadam	295,706.67	87,000.00
26	2	Columbia	11.01	Concrete	377,386.39	20,000.00
27	2	Columbia	12.35	Concrete	519,384.53	126,452.78
31	2	Hamilton	11.86	Rock Base Surface Treated	353,704.31	6,658.89
32	4	Nassau	10.00	Bituminous Macadam	371,409.03	0.00
33	2	Alachua	12.60	Bituminous Macadam	373,977.67	177,467.14
34	7	Escambia	10.00	Concrete	285,467.45	139,901.08
35	1	Escambia	5.00	Concrete	148,508.91	84,950.54
36-A	4	Indian River	7.76	Bituminous Macadam	314,037.74	52,500.00
36-B	4	Indian River	7.12	Concrete	290,540.66	52,500.00
37-A-C	2	Alachua	3.96	Sheet Asphalt	171,145.32	52,268.99

STATE ROAD DEPARTMENT OF FLORIDA
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1928 (Continued)

Project No.	Road No.	County	Length Miles	Type	Cost	County Participation
37-D	2	Alachua	2.14	Rock Base Surface Treated	\$ 244,427.67	\$ 28,246.37
37-E	2	Alachua	7.96	Sheet Asphalt	332,865.34	105,065.97
40-A	4	Brevard	16.17	Rock Base Surface Treated	661,000.19	88,626.96
40-D	4	Brevard	6.72	Rock Base Surface Treated	209,394.59	36,831.98
40-E	4	Brevard	13.60	Rock Base Surface Treated	370,060.16	74,541.06
41	4	Dade	12.00	Sheet Asphalt	804,387.30	0.00
43	2	Marion	10.44	Sheet Asphalt	347,636.59	12,520.85
44	2	Lake	10.52	Bituminous Concrete	387,044.94	269,350.00
46	3	Nassau	11.52	Concrete	523,864.76	0.00
47	4	St. Johns	14.88	Concrete	606,752.15	0.00
48	4	St. Johns	15.39	Rock Base Surface Treated	585,655.08	0.00
49	4	Flagler	13.81	Rock Base Surface Treated	439,806.25	0.00
50-A	14	Putnam	6.18	Rock Base Surface Treated	168,540.11	114,477.45
50-B	14	Putnam	9.77	Rock Base Surface Treated	352,194.05	180,978.24
50-C	14	Putnam	10.03	Rock Base Surface Treated	273,748.26	185,794.31
51	7	Escambia	6.23	Concrete	194,139.10	47,046.96
53-A	2	Lake	7.01	Bituminous Concrete	298,452.35	177,325.00
54	1	Leon	13.00	Concrete	510,748.56	140,000.00
58	1	Jefferson-Leon	12.53	Rock Base Surface Treated	404,274.84	100,000.00
59	1	Jefferson	9.01	Concrete	369,030.98	0.00
61-A	1	Gadsden	10.00	Concrete	296,150.79	72,500.00
61-C	1	Gadsden	9.77	Concrete	361,911.95	72,500.00
500-A	20	Bay	10.00	Concrete	383,576.85	377,248.53
502	6	Gulf	6.94	Rock Base Surface Treated	203,963.71	0.00
503	2	Charlotte	20.18	Marl	141,530.13	0.00
504	1	Columbia	9.15	Rock Base Surface Treated	242,735.51	19,727.94
505	2	Columbia	11.08	Rock Base Surface Treated	217,864.19	33,379.71
507	4	Flagler-Volusia	9.15	Rock Base Surface Treated	240,123.78	34,474.89
510	5	Hillsborough	9.20	Asphalt Block	*255,976.44	0.00

511	5	Hillsborough	11.00	Asphalt Block	*140,468.85	0.00
512	5	Hillsborough	3.00	Asphalt Block	*21,492.56	0.00
514	1	Jackson	11.04	Sand Clay Surface Treated	99,263.04	0.00
515	20	Jackson	16.60	Sand Clay Surface Treated	*114,508.19	0.00
519	5	Manatee	3.50	Bituminous Macadam	105,314.66	0.00
520	2	Marion	15.53	Rock Base Surface Treated	255,532.58	18,625.35
521	4	Nassau	12.41	Rock Base Surface Treated	475,490.08	0.00
523	8	Okeechobee	18.46	Rock Base Surface Treated	444,376.95	209,605.02
529	1	Suwannee	12.14	Concrete	424,289.03	0.00
532	3	Volusia	18.90	Rock Base Surface Treated	332,255.78	133,333.27
533	1	Suwannee	13.47	Rock Base Surface Treated	376,777.52	0.00
534	24	Brevard	14.50	Rock Base Surface Treated	639,149.28	120,000.00
537	23	Marion	6.93	Rock Base Surface Treated	*15,462.00	0.00
538	2	Marion	10.92	Rock Base Surface Treated	181,227.44	13,096.55
539	5	Marion	11.30	Rock Base Surface Treated	*87,624.13	0.00
543	3	Seminole	15.50	Bituminous Macadam	592,661.32	0.00
544	5	Pasco	20.08	Rock Base Surface Treated	457,778.80	0.00
545	5	Hernando	9.51	Sheet Asphalt	303,782.81	89,899.09
546	4	Brevard	5.00	Rock Base Surface Treated	129,221.16	25,496.09
548	5	Levy	18.00	Rock Base Surface Treated	*46,462.02	0.00
553	2	Marion	9.15	Rock Base Surface Treated	135,809.52	10,973.74
554	4	Brevard	5.00	Rock Base Surface Treated	123,536.27	25,496.08
560	6	Gulf	20.00	Sand Clay	86,421.26	0.00
562-A	8	Highlands	5.37	Rock Base Surface Treated	202,028.11	0.00
562-B	8	Highlands-Polk	10.71	Sheet Asphalt	350,579.99	0.00
564-A	5	Charlotte	10.88	Rock Base Surface Treated	*202,653.11	0.00
564-B	5	Charlotte	10.31	Rock Base Surface Treated	356,002.83	50,000.00
564-C	5	Charlotte	3.94	Rock Base Surface Treated	131,985.26	0.00
565	1	Madison	15.64	Concrete	604,663.29	119,788.05
567	1	Walton	21.35	Sand Clay Surface Treated	122,767.27	0.00
570	5	Manatee	3.96	Bituminous Macadam	150,391.74	0.00
571	1	Madison	14.73	Rock Base Surface Treated	423,550.22	112,818.45
572	13	Bradford	7.50	Rock Base Surface Treated	*142,799.59	0.00
574	9	Madison	11.66	Rock Base Surface Treated	283,342.57	75,269.65
575	3	Putnam	5.46	Rock Base Surface Treated	180,574.00	0.00

STATE ROAD DEPARTMENT OF FLORIDA
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1928 (Continued)

Project No.	Road No.	County	Length Miles	Type	Cost	County Participation
576	5	Sarasota.....	6.68	Rock Base Surface Treated ...	\$ 176,086.37	\$ 0.00
579	1	Holmes.....	8.62	Sand Clay Surface Treated ...	88,091.04	5,012.00
580	19	Dixie.....	10.00	Rock Base Surface Treated ...	347,205.64	0.00
581	5	Hillsborough.....	12.10	Rock Base Surface Treated ...	367,951.13	0.00
584	24	Osceola.....	2.00	Brick.....	45,608.33	0.00
585	1	Santa Rosa.....	5.81	Sand Clay Surface Treated ...	85,784.82	0.00
586	1	Jackson-Washington.....	17.37	Sand Clay Surface Treated ...	196,927.40	0.00
588	3	Putnam.....	2.34	Sheet Asphalt.....	61,579.18	0.00
589	5	Lee.....	8.27	Rock Base Surface Treated ...	246,792.75	0.00
590	1	Okaloosa-Santa Rosa.....	9.00	Sand Clay Surface Treated ...	49,560.84	0.00
592	10	Franklin.....	7.89	Rock Base Surface Treated ...	216,976.63	146,768.65
594	13	Bradford.....	9.10	Rock Base Surface Treated ...	*130,101.95	0.00
595	3	Volusia.....	7.43	Rock Base Surface Treated ...	165,806.12	0.00
597	4	Volusia.....	16.24	Rock Base Surface Treated ...	511,835.46	313,977.50
599	2	DeSoto.....	7.40	Marl.....	49,271.01	19,362.64
604	4	Volusia.....	7.72	Rock Base Surface Treated ...	290,590.19	31,560.00
607	13	Bradford.....	5.10	Rock Base Surface Treated ...	*88,624.25	0.00
607-B	13	Clay.....	7.00	Rock Base Surface Treated ...	146,589.24	0.00
608	4	Brevard.....	9.29	Concrete & Rock Base, Sur- face Treated.....	376,375.81	0.00
609	2	Hardee.....	1.00	Concrete.....	48,099.04	0.00
613	5	Sarasota.....	4.62	Sheet Asphalt.....	290,493.05	100,000.00
617	5	Alachua.....	5.17	Rock Base Surface Treated ...	113,145.25	108,045.00
618	5	Alachua.....	10.89	Rock Base Surface Treated ...	232,440.23	58,045.05
619	5	Alachua.....	9.28	Rock Base Surface Treated ...	183,972.90	86,575.88
621	1	Okaloosa.....	17.35	Sand Clay Surface Treated ...	229,143.56	0.00
622	2	Lake.....	.21	Bituminous Concrete.....	7,283.76	0.00
625	5	Citrus.....	10.86	Rock Base Surface Treated ...	*209,050.72	0.00
626	5	Citrus.....	6.61	Rock Base Surface Treated ...	*111,887.45	0.00

627	3	Putnam	6.27	Rock Base Surface Treated	173,124.91	0.00
628	3	Volusia	12.00	Rock Base Surface Treated	242,176.54	10,000.00
629	8	Highlands	6.00	Rock Base Surface Treated	170,372.31	0.00
630	8	Highlands	11.00	Rock Base Surface Treated	295,383.99	0.00
631	1	Washington-Holmes	8.53	Sand Clay Surface Treated	163,399.20	0.00
634	1	Jackson	11.15	Sand Clay Surface Treated	143,274.66	0.00
635	5	Manatee	1.28	Bituminous Concrete	54,152.89	0.00
636	8	St. Lucie	12.80	Rock Base Surface Treated	296,519.65	76,000.00
638	1	Santa Rosa	.30	Concrete	32,149.79	0.00
640-A	4	Martin	9.00	Sheet Asphalt	349,155.42	0.00
640-B	4	Martin	11.80	Sheet Asphalt	455,598.72	0.00
641	4	Palm Beach	13.36	Rock Base Surface Treated	408,511.77	0.00
642	3	Putnam	10.32	Rock Base Surface Treated	294,416.25	0.00
644-A	10	Wakulla	8.49	Rock Base Surface Treated	231,856.75	0.00
645	10	Wakulla	17.00	Rock Base Surface Treated	473,830.51	0.00
646	10	Franklin	18.50	Sand Clay Surface Treated	189,903.90	0.00
647	8	Highlands	7.00	Rock Base Surface Treated	189,468.76	0.00
648	2	Hardee	14.10	Rock Base Surface Treated	373,575.81	0.00
649	33	Okaloosa	10.43	Sand Clay Surface Treated	*37,393.99	0.00
651	10	Gulf	14.72	Rock Base Surface Treated	442,383.84	0.00
652	33	Okaloosa	9.04	Sand Clay Surface Treated	*59,070.88	0.00
654	4	Broward	6.30	Bituminous Macadam	171,323.39	0.00
655	18	Highlands	15.00	Rock Base Surface Treated	345,118.39	0.00
657	6	Jackson	11.00	Sand Clay Surface Treated	84,666.81	0.00
658	1	Holmes	8.21	Sand Clay Surface Treated	95,469.86	0.00
659	3	Clay	13.27	Rock Base Surface Treated	441,630.69	50,000.00
660	3	Clay	10.57	Rock Base Surface Treated	292,185.23	50,000.00
661	2	Lake	3.52	Sheet Asphalt	177,832.51	88,050.00
663	5	Citrus	8.03	Rock Base Surface Treated	240,166.93	0.00
666	6	Jackson	6.52	Sand Clay Surface Treated	69,092.91	0.00
667	18	DeSoto	8.63	Rock Base Surface Treated	243,864.70	0.00
668	4	Brevard	12.00	Rock Base Surface Treated	432,171.16	14,068.19
669-B	27	Dade	10.00	Rock Base Surface Treated	497,873.63	0.00
669-E	27	Dade	4.55	Rock Base Surface Treated	351,961.81	0.00
669-V	27	Collier	31.63	Rock Base Surface Treated	1,071,409.44	0.00
669-W	27	Collier	16.00	Rock Base Surface Treated	378,442.31	0.00

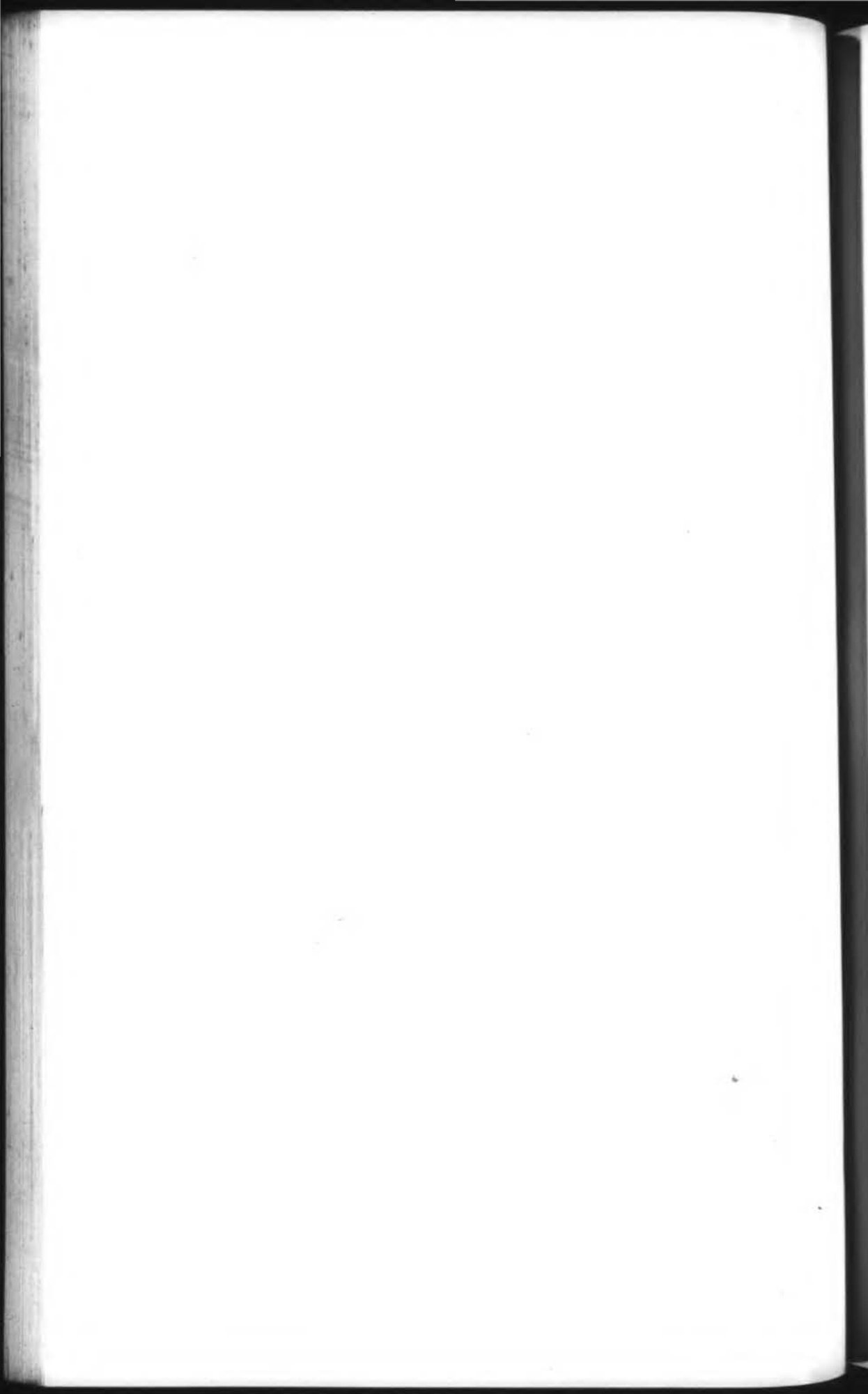
STATE ROAD DEPARTMENT OF FLORIDA
ROAD PROJECTS COMPLETED TO DECEMBER 31st, 1928. (Continued)

Project No.	Road No.	County	Length Miles	Type	Cost	County Participation
669-X	27	Collier.....	9.04	Rock Base Surface Treated ...	\$ 304,895.52	\$ 0.00
669-Z	27	Collier.....	6.48	Rock Base Surface Treated ...	194,610.68	0.00
670	6	Jackson.....	12.08	Sand Clay Surface Treated ...	120,263.31	0.00
671	20	Jackson.....	4.07	Sand Clay Surface Treated ...	43,921.46	0.00
674	1	Duval.....	3.50	Concrete.....	144,256.19	0.00
675	17	Polk.....	5.16	Sheet Asphalt.....	261,205.97	0.00
676-A	19	Levy.....	9.95	Rock Base Surface Treated ...	223,928.31	140,904.34
676-B	19	Levy.....	13.40	Rock Base Surface Treated ...	391,397.29	189,760.61
676-C	19	Levy.....	11.72	Rock Base Surface Treated ...	305,441.05	165,969.73
677-A	13	Levy.....	6.96	Rock Base Surface Treated ...	131,968.04	57,830.36
679	5	Hernando.....	7.11	Rock Base Surface Treated ...	230,718.00	0.00
682	5	Citrus.....	6.45	Rock Base Surface Treated ...	209,662.96	0.00
683-A	4	Palm Beach.....	8.04	Concrete.....	283,893.71	27,000.00
687-A	2	Lake.....	15.00	Sheet Asphalt.....	564,416.38	300,000.00
689	5-A	Alachua.....	1.89	Rock Base Surface Treated ...	35,100.13	0.00
691	4	Indian River.....	5.52	Concrete.....	270,210.08	0.00
692	4	St. Lucie.....	7.38	Concrete.....	399,803.47	0.00
693	4	St. Lucie.....	8.48	Concrete.....	410,265.77	200,000.00
694	4	Martin.....	8.93	Concrete.....	390,754.60	0.00
721	3	Putnam.....	4.50	Rock Base Surface Treated ...	64,221.70	0.00
747	35	Jefferson.....	6.50	Sand Clay.....	47,364.24	42,525.00
748	35	Madison.....	6.21	Sand Clay.....	75,604.05	0.00
786	123	Jackson.....	6.76	Sand Clay.....	71,449.40	0.00
Total.....			1,736.07		\$46,763,722.99	\$ 6,934,990.87

Note—*County participated in construction, handling its own Funds.



Rock Base Surface Treated, Road 18.



STATE ROAD DEPARTMENT OF FLORIDA
ROAD PROJECTS UNDER CONSTRUCTION DECEMBER 31, 1928

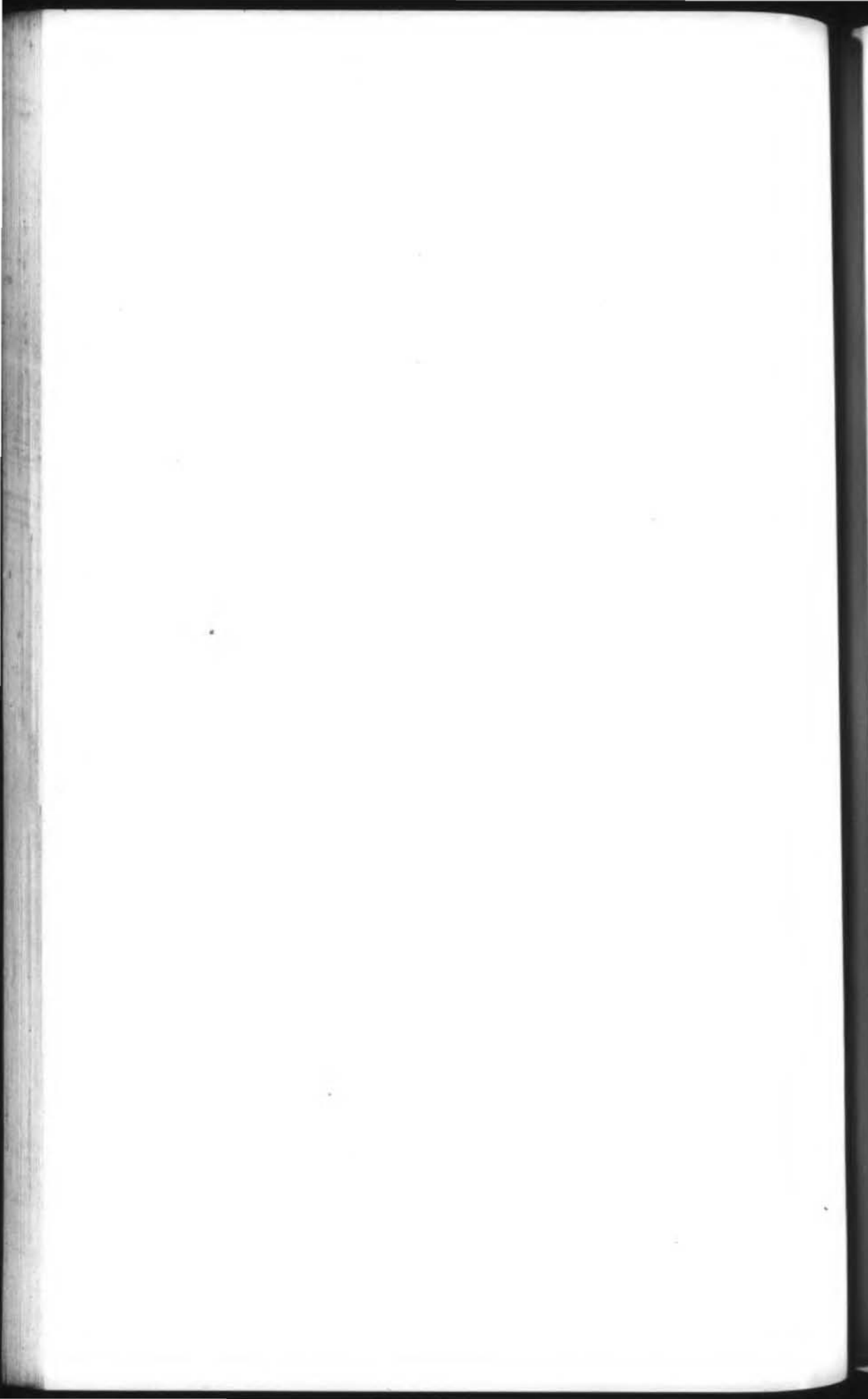
Project No.	Road No.	County	Length Miles	Type	Estimated Cost	Spent to Date	County Participation
52	1	Escambia.....	10.00	Rock Base Surface Treated ...	\$ 484,037.67	\$ 280,054.87	\$ 150,000.00
53-C	2	Lake.....	1.85	Bituminous Concrete.....	240,637.61	172,859.04	43,010.00
55	14	Alachua.....	16.77	Rock Base Surface Treated ...	374,050.42	159,638.10	203,393.84
56	10	Leon.....	17.98	Concrete.....	654,255.32	272,568.70	247,610.00
62-A	24	Osceola.....	12.52	Graded and Drained.....	84,778.51	80,028.91	93,028.85
62-C	24	Osceola.....	11.83	Graded and Drained.....	133,302.26	112,867.82	87,901.87
62-D	24	Osceola.....	12.66	Graded and Drained.....	110,461.79	89,379.83	94,069.28
500-B	20	Bay.....	12.75	Graded and Drained.....	119,377.34	117,377.34	0.00
500-C	20	Bay.....	12.18	Graded and Drained.....	162,071.89	89,071.89	0.00
501	6	Calhoun.....	12.00	Graded and Drained.....	202,662.96	192,662.96	50,739.96
501-C	6	Calhoun.....	15.00	Graded and Drained.....	129,642.41	30,142.41	0.00
501-D	6	Calhoun-Gulf....	14.20	Graded and Drained.....	87,501.38	12,501.38	0.00
592-C	10	Franklin.....	5.32	Graded and Drained.....	33,540.00	1,725.16	0.00
614	5	Sarasota.....	17.34	Bituminous Concrete.....	820,675.17	781,915.01	0.00
624	50	Hamilton.....	6.23	Rock Base Surface Treated ...	162,592.51	77,704.33	0.00
669-C	27	Dade.....	12.00	Rock Base Surface Treated ...	1,027,594.13	991,219.19	50,000.00
669-D	27	Dade.....	12.30	Rock Base Surface Treated ...	1,257,529.99	1,256,700.00	75,000.00
669-Y	27	Collier.....	13.55	Graded and Drained.....	117,000.00	5,444.17	0.00
672	1	Leon.....	9.92	Concrete.....	351,554.97	142,913.90	86,603.80
673	1	Gadsden.....	14.97	Concrete.....	522,382.95	223,678.83	108,210.00
677-D	13	Levy.....	8.91	Rock Base Surface Treated ...	287,437.69	282,897.03	74,032.86
685	10	Franklin.....	18.46	Rock Base Surface Treated ...	536,534.42	347,872.42	0.00
687-B	2	Lake.....	15.22	Rock Base Surface Treated ...	381,284.14	274,406.95	140,000.00
719	5-A	Suwannee.....	8.57	Rock Base Surface Treated ...	140,615.85	118,339.60	100,000.00
720	11	Jefferson.....	9.64	Graded and Drained.....	97,511.04	68,210.54	65,064.18
722	43	Jefferson.....	8.83	Graded and Drained.....	71,395.05	52,821.72	61,254.10
723	66	Leon.....	11.76	Sand Clay.....	175,142.25	131,689.55	91,031.05
726	19	Dixie.....	12.59	Rock Base Surface Treated ...	109,669.36	65,843.97	0.00
732	17	Polk.....	9.47	Graded and Drained.....	89,560.00	79,271.92	0.00
733	33	Walton.....	4.23	Sand Clay.....	47,373.22	6,202.95	47,373.22
734	40	Walton.....	7.89	Sand Clay.....	69,581.67	6,346.62	69,581.67
735	40	Walton.....	13.71	Sand Clay.....	130,342.33	16,022.15	130,342.33

STATE ROAD DEPARTMENT OF FLORIDA
ROAD PROJECTS UNDER CONSTRUCTION DECEMBER 31, 1928 (Continued)

Project No.	Road No.	County	Length Miles	Type	Estimated Cost	Spent to Date	County Participation
740	10	Gulf.....	9.62	Rock Base Surface Treated ...	\$ 255,219.65	\$ 243,219.65	\$ 0.00
743	10	Bay.....	18.25	Rock Base Surface Treated ...	432,292.03	187,292.03	0.00
744	19	Madison.....	5.78	Graded and Drained.....	80,600.00	72,802.03	0.00
745	19	Taylor.....	15.95	Graded and Drained.....	226,500.00	220,001.29	0.00
749	14	Gilchrist.....	7.81	Graded and Drained.....	51,599.99	33,363.44	0.00
750	14	Gilchrist.....	12.97	Graded and Drained.....	66,057.13	53,080.20	0.00
751	40	Walton.....	7.28	Sand Clay.....	53,739.49	2,323.66	53,739.49
752	40	Walton.....	8.72	Sand Clay.....	63,037.15	5,545.89	63,037.15
755	17	Polk.....	11.22	Graded and Drained.....	108,422.59	94,422.59	0.00
763	50	Suwannee.....	12.23	Rock Base Surface Treated ...	198,255.26	68,120.84	100,000.00
764	50	Suwannee.....	12.00	Rock Base Surface Treated ...	253,231.39	182,994.16	100,000.00
765	50	Suwannee.....	7.00	Rock Base Surface Treated ...	125,038.36	40,020.17	100,000.00
766	10	Bay.....	8.74	Graded and Drained.....	45,908.15	908.15	0.00
767	10	Bay.....	5.27	Graded and Drained.....	32,982.35	16,982.35	0.00
780	29	Okeechobee.....	11.00	Rock Base Surface Treated ...	300,997.40	122,043.87	113,554.29
781	29	Okeechobee.....	11.00	Rock Base Surface Treated ...	304,995.29	112,763.82	101,251.39
782	29	Okeechobee.....	6.62	Graded and Drained.....	70,714.58	68,187.67	64,296.04
787	10	Walton.....	16.30	Graded and Drained.....	152,988.11	15,868.11	0.00
788	10	Walton.....	17.54	Graded and Drained.....	164,274.07	3,234.54	164,274.07
798	13	Nassau.....	15.30	Graded and Drained.....	148,993.11	54,625.56	0.00
802-A	10	Okaloosa.....	7.76	Graded and Drained.....	76,559.56	21,638.22	71,354.68
802-C	10	Okaloosa.....	10.24	Graded and Drained.....	45,545.92	7,046.05	90,000.00
803	10	Okaloosa.....	11.13	Graded and Drained.....	82,342.33	13,114.11	82,242.33
804	67	Glades.....	18.55	Rock Base Surface Treated ...	416,083.52	298,928.24	0.00
807-A	25	Palm Beach.....	10.81	Rock Base Surface Treated ...	202,010.22	187,796.07	0.00
807-C	25	Palm Beach.....	6.14	Rock Base Surface Treated ...	99,923.54	74,324.49	0.00
815	54	Okaloosa.....	13.58	Graded and Drained.....	60,680.23	54,002.91	34,680.23
827	....	Escambia.....	8.12	Sand Clay.....	75,461.26	67,061.26	38,340.00
842	115	Walton.....	10.15	Graded and Drained.....	76,591.51	2,013.26	76,591.51
844-A	115	Okaloosa.....	7.10	Graded and Drained.....	40,456.67	6,314.67	0.00
Totals.....			694.83		\$13,523,597.16	\$ 8,870,418.56	\$ 3,321,608.19



Rock Base Surface Treated, Road 3.



STATE ROAD DEPARTMENT OF FLORIDA

ROAD PROJECTS THAT ARE GRADED READY FOR PAVING DECEMBER 31st, 1928

Project No.	Road No.	County	Length Miles	Type	Cost	County Participation
6	1	Madison.....	5.60	Graded.....	\$ 110,073.54	\$ 2,500.00
518	5-A	LaFayette.....	17.75	Graded.....	179,446.44	57,370.00
535	5-A	LaFayette.....	13.71	Graded.....	104,547.88	42,630.00
573-D	2	Orange.....	16.81	Graded.....	143,453.30	0.00
587	5-A	Columbia.....	4.37	Graded.....	36,202.92	0.00
615	5	Sarasota.....	11.92	Graded.....	124,364.43	0.00
623	35	Madison.....	12.13	Graded.....	139,643.12	0.00
653	4	Broward.....	13.46	Graded.....	129,372.27	0.00
677-B	13	Levy.....	11.58	Graded.....	170,151.71	96,217.75
677-C	13	Levy.....	10.16	Graded.....	291,135.51	84,419.03
678	10	Bay.....	8.72	Graded.....	54,591.97	0.00
688	10	Bay.....	9.31	Graded.....	49,568.06	0.00
695	2	Lake.....	10.50	Graded.....	237,394.72	0.00
698	19	Leon.....	12.45	Graded.....	153,052.23	135,000.00
699	19	Jefferson.....	7.70	Graded.....	42,753.58	30,000.00
700	19	Jefferson.....	9.26	Graded.....	79,961.61	70,000.00
706-A	28	Putnam.....	12.09	Graded.....	125,501.42	50,000.00
706-B	28	Putnam.....	14.91	Graded.....	97,173.61	50,000.00
708	11	Jefferson.....	7.98	Graded.....	69,238.97	52,226.83
709	39	Holmes.....	9.09	Graded.....	64,027.36	42,740.75
710-A	17	Hillsborough.....	12.04	Graded.....	71,609.28	0.00
710-C	17	Hillsborough.....	12.69	Graded.....	126,906.98	0.00
713	28	Columbia.....	10.00	Graded.....	93,572.33	0.00
714	28	Union.....	10.20	Graded.....	93,507.84	125,482.00
715	28	Union.....	3.70	Graded.....	37,025.77	45,518.00
716	28	Bradford.....	11.12	Graded.....	89,446.24	62,500.00
717	28	Bradford.....	10.93	Graded.....	102,169.84	62,500.00

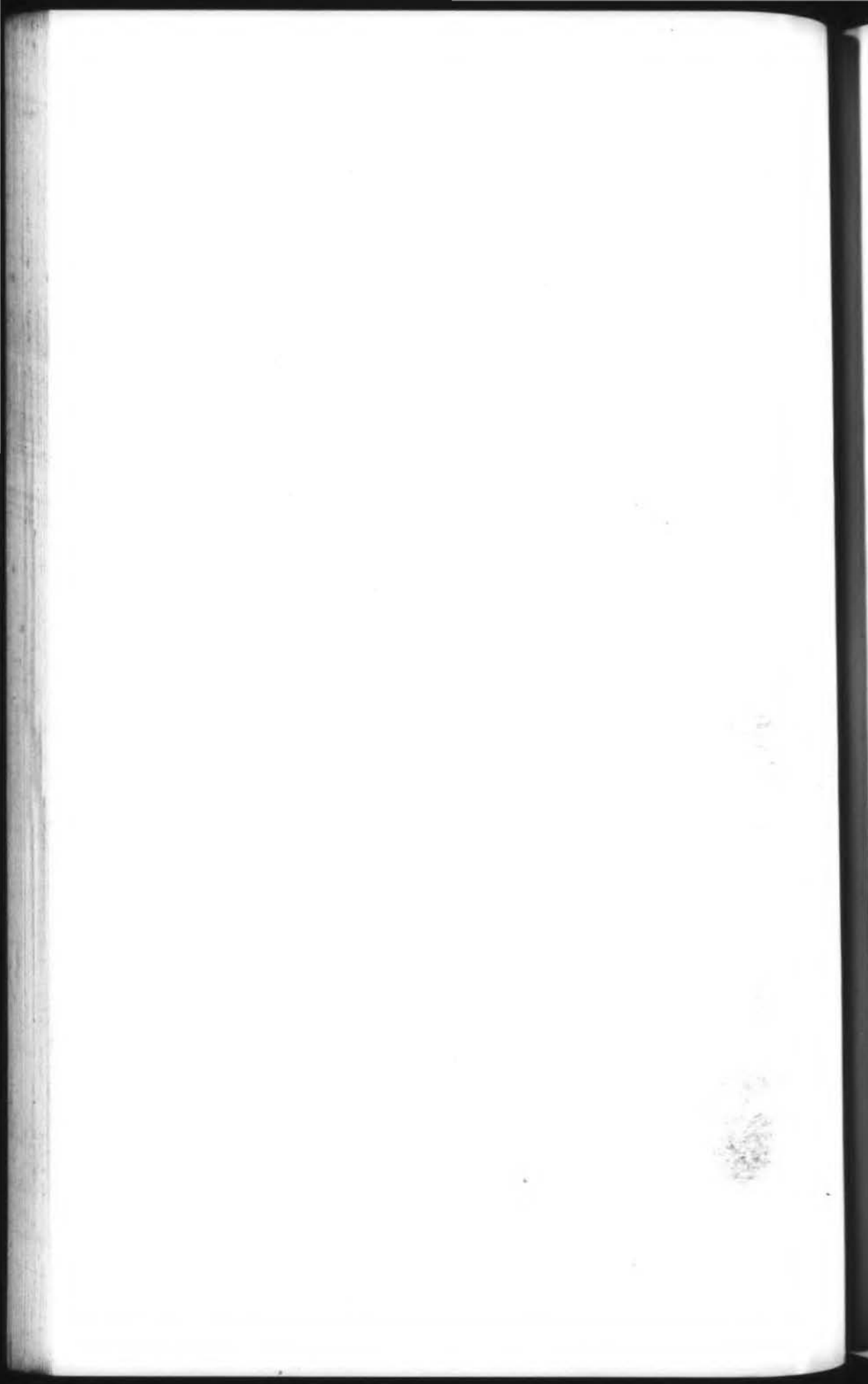
STATE ROAD DEPARTMENT OF FLORIDA

ROAD PROJECTS THAT ARE GRADED READY FOR PAVING DECEMBER 31st, 1928 (Continued)

Project No.	Road No.	County	Length Miles	Type	Cost	County Participation
718	5-A	Columbia.....	8.22	Graded.....	\$ 95,286.98	\$ 0.00
728	10	Leon.....	11.67	Graded.....	110,273.95	100,000.00
736	39	Holmes.....	8.97	Graded.....	49,300.22	42,740.75
742	13	Alachua.....	7.65	Graded.....	52,429.05	0.00
757	2	Polk.....	10.26	Graded.....	72,296.13	0.00
758	2	Polk.....	9.89	Graded.....	87,625.43	0.00
819	54	Okaloosa.....	4.69	Graded.....	23,971.83	20,979.67
823	41	Okaloosa.....	9.18	Graded.....	40,998.32	34,085.34
824	41	Okaloosa.....	9.81	Graded.....	54,977.04	59,710.97
Total.....			370.52		\$ 3,603,051.88	\$ 1,266,621.09



Rock Base Surface Treated, Road 4.



Convicts

The Department has been working convicts since 1917, and it is therefore considered the oldest branch of the Department. From 1917 to 1920, the number of prisoners allocated to the Department was small, but in 1919 the convict lease system was abolished and in January, 1920, about one thousand prisoners were turned over to the Department for work on the roads. Camps were constructed at various points in the State and the prisoners were put to work clearing and grubbing the right-of-ways and grading sections of the roads on the State System. Under this plan the camps were scattered over a large territory. This plan continued for several years but was found to be expensive, particularly with reference to supervision and economical care of the prisoners. The past five years the Department has made an effort to concentrate the camps.

The working of prisoners on the roads serves a twofold purpose, it provides a wholesome occupation for these charges of the State and at the same time provides a construction unit that can be used to regulate prices on all classes of work, and also do such work that does not interfere with contracting.

The Department has at the present 734 colored and 369 white prisoners, a total of 1103 prisoners, distributed among 20 camps. The prisoners are maintained separately, that is, the whites from the colored, in movable camp buildings. A camp consists of a stockade, kitchen, mess hall, captain and guard house, and a general small storage building. The convicts are housed, clothed, fed and cared for in accordance with the requirements of the Prison Board, which has inspectors that make regular inspections of the camps. The Department has a superintendent of convicts whose principle duty is the transferring of prisoners from the State Farm to the various camps, and returning escaped prisoners that are captured. A saving has been accomplished in the transferring.

In connection with the working of the prisoners, equipment is provided and the Department has 600 mules and 18

horses, with the necessary wagons, wheelers and slips to do the work in an economical manner. For the proper care of its mules and horses, a veterinarian is employed to inspect and advise as to the proper care of all the stock.

The twenty convict camps at present are working on the following roads: Nos. 6, 10, 13, 14, 20, 66 and 115, in Nassau, Gilchrist, Dixie, Taylor, Madison, Leon, Franklin, Bay, Gulf, Calhoun, Walton and Okaloosa counties, and all the forces are engaged in clearing and grubbing, grading and clay surfacing except two units, which are constructing lime rock surfacing in Franklin and Bay counties.

During the past two years convicts actually worked on the roads 582,092 days, at a cost for maintenance and supervision of \$844,862.30, or an average working day cost of \$1.45 per day. The accompanying tables show in detail the cost of the individual camps as to meals served, cost per meal, overhead cost and working cost. The time shown as prisoners days in camps represents the men on the yard, Sundays, holidays, sickness and time lost from bad weather.

The Department believes that the convict force is one of the greatest assets the State has for road building purposes. The current wages for common labor in the sections that convicts are working are \$2.00 per day, and on that basis a saving of \$315,322 has been accomplished in this biennium.



Rock Base Surface Treated, Road 2, Looking Over Big Charlie Creek Bridge.

STATE ROAD DEPARTMENT
STATISTICAL STATEMENT SHOWING COST OF OPERATION OF STATE ROAD CONVICT CAMPS
TWELVE MONTHS TO DECEMBER 31, 1927

Camp No.	Groceries	Salaries	All Other Expenses	Total	Daily Avg. Prisoners In Camp (Calendar)	Average Prisoners On Road (Daily)	Prisoner Days On Road	Prisoner Days In Camp	Total Prisoner Days	Captain And Guard Days	Meals Served	Average Cost Per Meal	Overhead Per Man (Daily)	Total Cost Per Man (Daily)	Total Cost Per Man On Road (Daily)
1	\$ 12,674.85	\$ 7,178.80	\$ 8,726.56	\$ 28,580.21	65	54	17,671	6,051	23,722	3,378	82,056	\$.15	\$.67	\$ 1.20	\$ 1.62
2	12,229.91	8,740.99	8,757.49	29,728.39	75	68	21,027	6,490	27,517	3,844	105,534	.12	.64	1.08	1.41
3	11,924.61	8,045.47	10,881.56	30,851.64	72	63	19,553	6,765	26,318	3,584	91,208	.13	.72	1.17	1.58
4	8,924.90	6,986.49	5,889.18	21,800.57	48	41	12,885	4,633	17,518	2,989	62,437	.14	.73	1.24	1.69
5	9,419.41	5,293.58	7,400.98	22,113.97	56	50	15,697	4,702	20,399	2,227	70,136	.13	.63	1.09	1.41
8	10,772.48	6,387.57	5,043.69	22,203.74	58	51	15,784	5,269	21,053	2,746	72,529	.15	.54	1.05	1.41
12	11,432.85	7,229.89	7,631.26	26,294.00	69	62	19,367	5,786	25,153	3,091	90,856	.13	.59	1.04	1.36
16	9,219.21	6,760.86	8,419.39	24,399.46	61	54	16,900	5,367	22,267	2,742	76,234	.12	.68	1.09	1.44
18	8,488.67	6,050.71	4,043.40	18,582.78	61	53	16,491	5,896	22,387	2,311	77,325	.11	.44	.83	1.13
26	8,968.17	6,343.13	7,821.03	23,132.33	58	50	15,649	5,233	20,882	2,778	71,599	.13	.67	1.10	1.48
27	13,365.82	7,848.57	7,733.67	28,948.06	79	69	21,508	7,251	28,759	3,552	98,700	.14	.54	1.00	1.35
28	7,522.98	6,160.80	4,132.46	17,816.24	57	49	15,383	5,658	21,041	2,647	74,570	.10	.49	.85	1.16
30	9,393.35	6,144.77	5,951.27	21,489.39	69	62	19,402	5,690	25,092	2,584	83,397	.11	.48	.86	1.10
31	9,992.94	7,047.87	7,070.62	24,111.43	63	54	16,824	5,973	22,797	3,149	80,721	.12	.62	1.05	1.43
32	8,066.71	6,216.92	8,081.85	22,365.48	51	43	13,382	5,224	18,606	2,814	64,530	.13	.76	1.20	1.67
33	9,382.95	7,648.48	5,524.60	22,556.03	61	54	16,782	5,447	22,229	3,418	81,046	.12	.59	1.01	1.34
34	7,366.35	4,290.96	3,815.47	15,472.78	40	34	10,728	3,678	14,406	2,105	59,197	.12	.56	1.07	1.44
35	5,098.98	3,787.77	7,187.12	16,073.87	57	49	7,447	2,856	10,303	1,739	36,697	.14	1.07	1.57	2.16
36	662.73	618.39	3,463.95	4,745.07	66	47	1,444	610	2,054	273	8,223	.08	1.98	2.30	3.29
	\$174,907.87	\$118,782.02	\$127,575.55	\$421,265.44	1,074	945	293,924	98,579	392,503	51,971	1,386,995	\$.13	\$.63	\$ 1.07	\$ 1.43

Camp No. 36 opened December 1, 1927. Camp No. 35 opened January 1, 1927.

STATE ROAD DEPARTMENT
STATISTICAL STATEMENT SHOWING COST OF OPERATION OF STATE ROAD CONVICT CAMPS
TWELVE MONTHS TO DECEMBER 31, 1928

Camp No.	Groceries	Salaries	All Other Expenses	Total	Daily Avg. Prisoners In Camp (Calendar)	Average Prisoners On Road (Daily)	Prisoner Days On Road	Prisoner Days In Camp	Total Prisoner Days	Captain And Guard Days	Meals Served	Average Cost Per Meal	Overhead Per Man (Daily)	Total Cost Per Man (Daily)	Total Cost Per Man On Road (Daily)
1	\$ 10,657.74	\$ 7,282.45	\$ 4,348.87	\$ 22,289.06	55	47	14,651	7,371	22,022	3,309	72,737	\$.15	\$.53	\$ 1.01	\$ 1.52
2	6,550.77	6,571.61	3,155.62	16,278.00	41	37	11,631	3,961	15,592	2,852	54,831	.12	.63	1.04	1.40
3	10,298.44	7,318.94	6,149.96	23,767.34	57	42	13,056	7,040	20,096	3,162	74,081	.14	.67	1.18	1.82
4	8,819.33	7,283.41	6,008.21	22,110.95	55	46	14,432	5,171	19,603	3,168	70,233	.13	.68	1.13	1.53
5	9,690.90	6,694.03	4,017.76	20,402.69	63	53	16,338	6,545	22,883	2,890	77,601	.12	.47	.89	1.25
8	9,367.42	6,735.11	4,064.46	20,166.99	55	53	14,615	5,312	19,927	2,957	68,654	.14	.54	1.01	1.38
12	11,128.44	7,165.90	5,599.14	23,893.48	66	55	17,023	7,275	24,298	3,093	84,293	.13	.52	.98	1.40
16	8,489.83	6,684.72	4,089.32	19,263.87	52	44	13,713	4,953	18,666	2,655	64,926	.13	.57	1.03	1.40
18	5,776.18	6,882.67	2,791.45	15,450.30	35	32	10,107	4,250	14,357	2,819	47,781	.12	.67	1.08	1.53
26	9,721.51	7,382.19	6,863.73	23,967.43	58	47	14,642	6,645	21,287	3,271	75,011	.13	.67	1.12	1.64
27	14,046.77	8,399.96	5,288.70	27,735.43	79	68	21,274	7,850	29,124	3,827	97,610	.14	.47	.95	1.30
28	7,822.34	6,081.48	3,759.18	17,663.00	62	51	15,988	6,763	22,751	2,617	79,663	.10	.43	.78	1.10
30	8,316.89	6,270.01	4,486.87	19,073.77	58	54	16,762	4,720	21,482	2,662	71,915	.11	.50	.89	1.14
31	8,858.54	6,365.28	4,983.28	20,207.10	51	40	12,414	7,258	19,672	2,755	64,434	.14	.43	1.03	1.63
32	11,237.70	7,580.56	5,796.40	24,614.66	64	54	16,724	6,718	23,442	3,437	80,670	.14	.57	1.05	1.47
33	10,148.00	8,130.05	4,513.54	22,791.59	51	40	12,433	5,415	17,848	3,618	66,744	.15	.70	1.27	1.83
34	11,012.18	6,990.77	6,316.13	24,319.08	59	49	15,283	6,082	21,365	3,105	74,197	.15	.62	1.13	1.59
35	10,719.23	7,323.63	7,133.01	25,175.87	61	53	16,367	7,138	23,505	3,329	76,618	.14	.62	1.07	1.54
36	12,485.91	7,678.15	6,914.99	27,079.05	70	58	18,067	7,563	25,630	3,384	86,750	.14	.57	1.05	1.50
37	1,519.95	1,508.70	4,318.55	7,347.20	51	41	2,648	1,111	3,759	641	13,485	.11	1.55	1.95	2.77
	\$186,668.07	\$136,329.62	\$100,599.17	\$423,596.86	1,103	927	288,168	119,141	407,309	59,552	1,402,234	\$.13	\$.58	\$ 1.04	\$ 1.47

Camp No. 37 opened October 15, 1928.

STATE ROAD DEPARTMENT OF FLORIDA
PLANS FOR BRIDGES 20 FEET AND OVER IN LENGTH
PREPARED BY BRIDGE DEPARTMENT—1927-1928

Road No.	Project No.	County	No. of Bridges	Concrete	Steel	Timber	Total Length
1	6-B	Madison.....	3	432	.....	.....	432
1	585-B	Santa Rosa.....	1	165	.....	.....	165
2	53-B	Lake.....	2	629	40	.....	669
2	661-B	Lake.....	1	43	.....	.....	43
2	695-B	Lake.....	2	.....	40	468	508
2	757-B	Polk.....	1	115	.....	.....	115
2	758-B	Polk.....	2	254	.....	.....	254
3	627-B	Putnam.....	1	307	60	.....	367
3	721-B	Putnam.....	1	264	120	.....	384
4	41-B	Dade.....	2	223	.....	.....	223
4	60-B	Volusia.....	9	559	.....	.....	559
4	653-B	Broward.....	3	463	60	.....	523
4	654-B	Broward.....	2	86	.....	.....	86
4	683-B	Palm Beach.....	1	156	40	.....	196
5	589	Lee.....	6	274	.....	.....	274
5	614	Sarasota.....	1	31	.....	.....	31
5	614-B	Sarasota.....	6	515	80	.....	595
5	615	Sarasota.....	2	86	.....	.....	86
5	615-B	Sarasota.....	2	345	120	.....	465
5	769	Lee.....	1	4,393	80	.....	4,473
5	772	Charlotte.....	1	4,183	75	.....	4,258
5-A	587-B	Columbia.....	1	99	.....	.....	99
5-A	718-B	Columbia.....	1	.....	210	.....	210
10	680	Bay.....	1	.....	2,775	.....	2,775
10	681	Bay.....	1	.....	4,438	.....	4,438
10	685-B	Franklin.....	2	.....	120	770	890
13	677-D	Levy.....	4	.....	.....	1,335	1,335
14	55-B	Alachua.....	2	263	.....	.....	263
17	710-B	Hillsboro.....	6	234	.....	.....	234
17	732-B	Polk.....	4	333	.....	.....	333
17	755	Polk.....	1	21	.....	.....	21
19	698	Leon.....	1	391	.....	.....	391
19	699	Jefferson.....	1	299	.....	.....	299
19	700-B	Jefferson.....	1	414	80	.....	494
24	62-B	Osceola.....	10	866	.....	.....	866
28	715-B	Union.....	1	.....	120	675	795

RAILROAD OVERHEAD CROSSINGS

2	573-D	Orange.....	1	92	80	.....	172
2	758-B	Polk.....	1	129	.....	.....	129
4	691	Indian River.....	1	284	.....	.....	284
10	728	Leon.....	1	.....	.....	112	112
11	720	Jefferson.....	1	.....	29	180	209
14	55-B	Alachua.....	1	197	.....	.....	197
19	698	Leon.....	1	.....	.....	112	112
20	500-C	Bay.....	1	.....	.....	112	112
24	62-B	Osceola.....	1	171	.....	.....	171
28	717	Bradford.....	1	.....	23	90	113
50	764-B	Suwannee.....	1	111	.....	.....	111
66	723	Leon.....	2	.....	.....	220	220
96	820-B	Jefferson.....	1	.....	24	90	114
		Totals.....		17,427	8,614	4,164	30,205

STATE ROAD DEPARTMENT OF FLORIDA
BRIDGE PROJECTS COMPLETED DECEMBER 31st, 1928

Project No.	Road No.	County	Length Feet	Type	Total Cost	County Participation
3	1	Gadsden-Jackson	2,627	Concrete	\$ 697,698.35	\$ 30,001.00
6-B	1	Madison	432	Concrete	61,584.97	0.00
7-B	2	Hamilton	476	Concrete and Steel	174,078.24	0.00
14-B	1	Santa Rosa	370	Concrete	167,955.01	0.00
19-B	2	Hamilton	106	Concrete	20,359.37	0.00
20	1	Holmes-Washington	2,473	Concrete	351,942.05	50,000.00
21-B	1	Baker	340	Concrete	40,778.78	0.00
29-A	5	Hillsborough	316	Concrete	69,890.79	0.00
29-B	5	Hillsborough	215	Concrete	83,455.47	0.00
30	2	Columbia and Hamilton	325	Concrete and Steel	61,996.95	13,000.00
36-A	4	Indian River	72	Concrete	14,558.70	0.00
37-D	2	Alachua	60	Concrete	32,381.50	0.00
38	1	Escambia and Santa Rosa	18,470	Concrete and Steel	987,236.47	100,000.00
39	4	Brevard and Indian River	2,221	Concrete	253,974.98	0.00
40-B	4	Brevard	148	Concrete	62,042.17	0.00
41-B	4	Dade	223	Concrete and Steel	105,647.97	0.00
45	1	Suwannee and Madison	912	Concrete and Steel	140,238.41	0.00
48-B	4	St. Johns	456	Concrete	80,347.23	0.00
49-B	4	Flagler	207	Concrete	53,035.81	0.00
50-B	14	Putnam	143	Concrete	41,101.48	0.00
53-B	2	Lake	671	Concrete and Steel	116,467.60	0.00
55-B	14	Alachua	263	Concrete	58,110.01	0.00
421	3	Nassau	560	Concrete and Steel	251,243.92	0.00
501-B	6	Calhoun	210	Timber	8,490.37	0.00
517	1	Jefferson	90	Timber	4,564.13	0.00
526	15	Pinellas	2,500	Concrete	251,902.22	0.00

STATE ROAD DEPARTMENT OF FLORIDA
BRIDGE PROJECTS COMPLETED DECEMBER 31st, 1928 (Continued)

Project No.	Road No.	County	Length Feet	Type	Total Cost	County Participation
527	17	Pinellas.....	2,833	Concrete.....	\$ 162,297.03	\$ 0.00
528	..	Seminole.....	..	Timber.....	5,000.00	0.00
534-B	24	Brevard.....	270	Timber.....	62,170.28	0.00
559	4	Brevard.....	260	Concrete.....	*10,000.00	0.00
562-A	8	Highlands.....	375	Timber.....	6,425.75	0.00
563	2	Osceola.....	315	Timber.....	11,891.82	0.00
564-C	5	Charlotte.....	143	Concrete and Steel.....	40,516.42	0.00
566	2	Lake.....	266	Timber.....	6,493.83	0.00
568	1	Jackson.....	3,315	Timber.....	109,552.65	0.00
582	8	Highlands and Okeechobee....	3,125	Timber and Steel.....	167,929.12	0.00
583	5	Hillsborough.....	170	Concrete.....	64,179.84	0.00
585-B	1	Santa Rosa.....	165	Concrete.....	26,706.01	0.00
589	5	Lee.....	274	Concrete.....	41,317.04	0.00
593	5	Manatee.....	3,600	Concrete.....	897,597.69	0.00
601	0	St. Johns.....	480	Timber.....	29,274.37	0.00
613	5	Sarasota.....	32	Concrete.....	12,839.65	0.00
614-B	5	Sarasota.....	595	Concrete and Steel.....	305,155.41	0.00
620	4	St. Lucie.....	45	Timber.....	4,354.58	0.00
621-B	1	Okaloosa.....	1,568	Concrete.....	150,507.54	0.00
621-C	1	Okaloosa.....	1,363	Concrete.....	145,786.55	0.00
621-D	1	Okaloosa.....	29	Concrete.....	18,578.93	0.00
631-B	1	Holmes.....	1,480	Timber.....	47,948.39	0.00
633-B	1	Gadsden.....	120	Concrete.....	43,744.13	0.00
634-B	1	Jackson.....	456	Concrete and Steel.....	64,826.42	0.00
641-B	4	Palm Beach.....	247	Concrete and Steel.....	163,890.41	0.00
645-B	10	Wakulla and Franklin.....	850	Timber.....	32,016.90	19,700.00
648-B	2	Hardee.....	615	Steel, Timber and Concrete...	80,073.08	0.00

655-667	18	Highlands.....	815	Timber.....	40,927.84	0.00
656	4	Palm Beach.....	727	Concrete and Steel.....	267,080.50	0.00
658-B	1	Holmes.....	525	Timber.....	19,148.98	0.00
660-B	3	Clay.....	225	Timber.....	12,699.11	0.00
661-B	2	Lake.....	43	Concrete.....	17,877.92	0.00
664	3	Clay.....	1,600	Concrete and Steel.....	231,768.82	0.00
665	3	Clay.....	1,400	Concrete and Steel.....	217,775.05	0.00
672-B	1	Leon.....	1,950	Concrete and Steel.....	191,315.70	0.00
673-B	1	Gadsden.....	416	Concrete and Steel.....	59,201.05	0.00
677-D	13	Levy.....	1,335	Timber.....	41,522.04	0.00
685-B	10	Franklin.....	890	Timber and Steel.....	104,204.69	0.00
693-B	4	St. Lucie.....	160	Concrete.....	79,423.79	0.00
694-B	4	St. Lucie.....	351	Concrete.....	100,170.14	0.00
697	1	Escambia.....	666	Timber.....	50,816.77	0.00
698-B	19	Leon.....	391	Concrete.....	36,861.00	0.00
699-B	19	Jefferson.....	299	Concrete.....	26,324.59	0.00
700-B	19	Jefferson.....	494	Concrete.....	51,057.63	0.00
706	28	Putnam.....	675	Timber.....	34,945.82	0.00
710-B	17	Hillsborough.....	234	Concrete.....	54,063.24	0.00
743	10	Bay.....	120	Timber.....	13,029.12	0.00
		Total.....	71,193		\$ 8,552,370.59	\$ 212,701.00

Note—*County participated in construction, handling its own funds.

STATE ROAD DEPARTMENT OF FLORIDA
BRIDGES UNDER CONSTRUCTION DECEMBER 31st, 1928

Project No.	Road No.	County	Length Feet	Type	Estimated Cost	Spent to Date	County Participation
680	10	Bay	4,000.0	Concrete and Steel	\$ 829,392.71	\$ 720,552.09	\$ 590,000.00
681	10	Bay	7,530.0	Concrete and Steel	1,119,966.46	996,227.48	1,031,245.27
695-B	2	Lake	508.0	Concrete	55,556.52	52,432.26	0.00
769	5	Lee	4,473.0	Concrete	585,216.61	48,033.50	0.00
		Total	16,511.0		\$ 2,590,132.30	\$ 1,817,245.33	\$ 1,621,245.27



Plain Cement Concrete, Federal Aid Road 4.

STATE ROAD DEPARTMENT OF FLORIDA
OVER-PASSES COMPLETED DECEMBER 31st, 1928

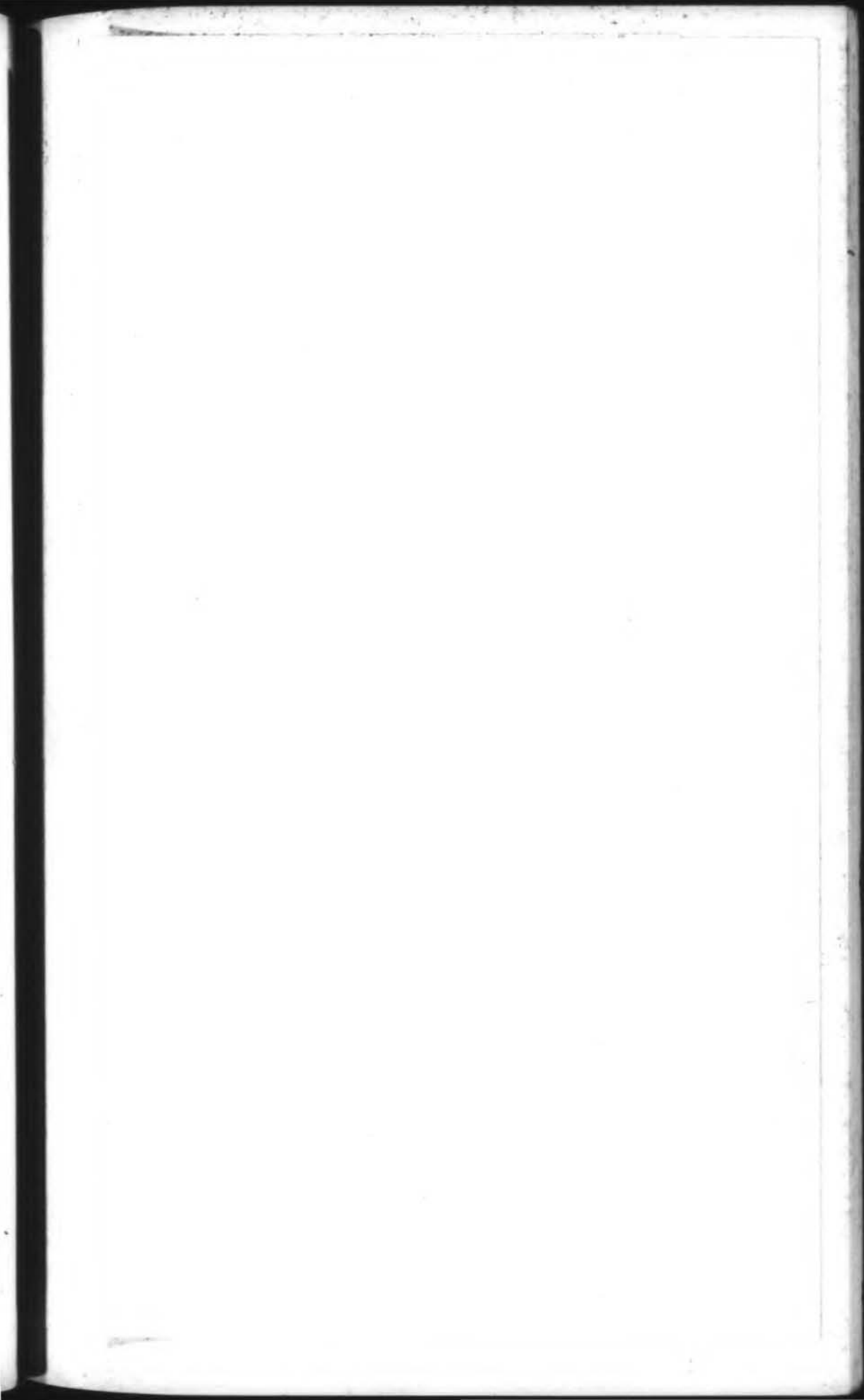
Project No.	Road No.	County	Type	Length Feet	Cost	Railroad	County Participation
43	2	Marion.....	Concrete.....	120	\$ 16,601.80	Atlantic Coast Line.....	
50-A	14	Putnam.....	Concrete.....	120	37,626.76	Atlantic Coast Line.....	
54	1	Leon.....	Timber.....	110	3,482.52	Atlantic Coast Line.....	
55	14	Alachua.....	Concrete.....	197	25,849.52	Seaboard Air Line.....	
509	1	Gadsden.....	Timber.....	185	5,338.09	Louisville & Nashville.....	
545	5	Hernando.....	Timber.....	183	8,280.56	Seaboard Air Line.....	
565	1	Suwannee.....	Timber.....	108	3,312.17	Seaboard Air Line.....	
567	1	Walton.....	Timber.....	77	1,370.44	Louisville & Nashville.....	
567	1	Walton.....	Timber.....	77	1,264.21	Louisville & Nashville.....	
573-D	2	Orange.....	Concrete & Steel.	172	31,336.09	Seaboard Air Line and Atlantic Coast Line.....	
579	1	Holmes.....	Timber.....	110	6,160.90	Louisville & Nashville.....	
621	1	Okaloosa.....	Timber.....	115	5,485.16	Louisville & Nashville.....	
625	5	Citrus.....	Timber.....	94	3,227.04	Seaboard Air Line.....	
625	5	Citrus.....	Concrete.....	130	24,875.99	Atlantic Coast Line.....	
633-B	1	Gadsden.....	Concrete.....	120	18,455.75	Seaboard Air Line.....	
640-B	4	Martin.....	Concrete.....	132	33,226.99	Florida East Coast.....	
652	33	Okaloosa.....	Timber.....	113	4,635.01	Louisville & Nashville.....	
657	6	Jackson.....	Timber.....	114	3,674.45	Louisville & Nashville.....	
670	6	Jackson.....	Timber.....	111	5,087.31	Louisville & Nashville.....	
673	1	Gadsden.....	Timber.....	111	6,195.95	Georgia, Florida & Alabama.....	
682-B	5	Citrus.....	Steel & Concrete.	247	52,614.51	Seaboard Air Line and Atlantic Coast Line.....	
687-A	2	Lake.....	Concrete.....	120	17,126.51	Atlantic Coast Line.....	
691	4	Indian River..	Concrete.....	285	60,667.78	Florida East Coast.....	
698-B	19	Leon.....	Timber.....	112	2,890.76	Atlantic Coast Line.....	
717	28	Bradford.....	Timber.....	113	5,243.76	Southern.....	
720	11	Jefferson.....	Timber.....	209	11,300.50	Seaboard Air Line and Atlantic Coast Line.....	
723	66	Leon.....	Timber.....	110	4,022.55	Georgia, Florida & Alabama.....	
723	66	Leon.....	Timber.....	110	3,930.15	Seaboard Air Line.....	
728	10	Leon.....	Timber.....	112	4,353.32	Seaboard Air Line.....	
764-B	50	Suwannee.....	Concrete.....	111	17,936.62	Atlantic Coast Line.....	
820-B	96	Jefferson.....	Timber.....	114	4,836.08	Seaboard Air Line.....	\$ 2,218.75
		Total.....		4,142.0	\$ 430,409.25		\$ 2,218.75

STATE ROAD DEPARTMENT OF FLORIDA
UNDER-PASSES COMPLETED DECEMBER 31st, 1928

Project No.	Road No.	County	Type	Cost to Date	Railroad
58	1	Jefferson.....	Concrete.....	\$ 12,599.12	Atlantic Coast Line.
553	2	Marion.....	Concrete.....	20,007.92	Seaboard Air Line.
586	1	Jackson.....	Concrete.....	35,946.72	Louisville & Nashville.
37-B	2	Alachua.....	Concrete.....	35,653.23	Seaboard Air Line.
50-A	14	Putnam.....	Concrete.....	71,960.62	Atlantic Coast Line.
		Total.....		\$ 176,167.61	

STATE ROAD DEPARTMENT OF FLORIDA
UNDER-PASSES UNDER CONSTRUCTION DECEMBER 31st, 1928

Project No.	Road No.	County	Type	Cost to Date	Railroad
698-B	19	Leon.....	Concrete.....		Seaboard Air Line.
728	10	Leon.....	Concrete.....		Seaboard Air Line.

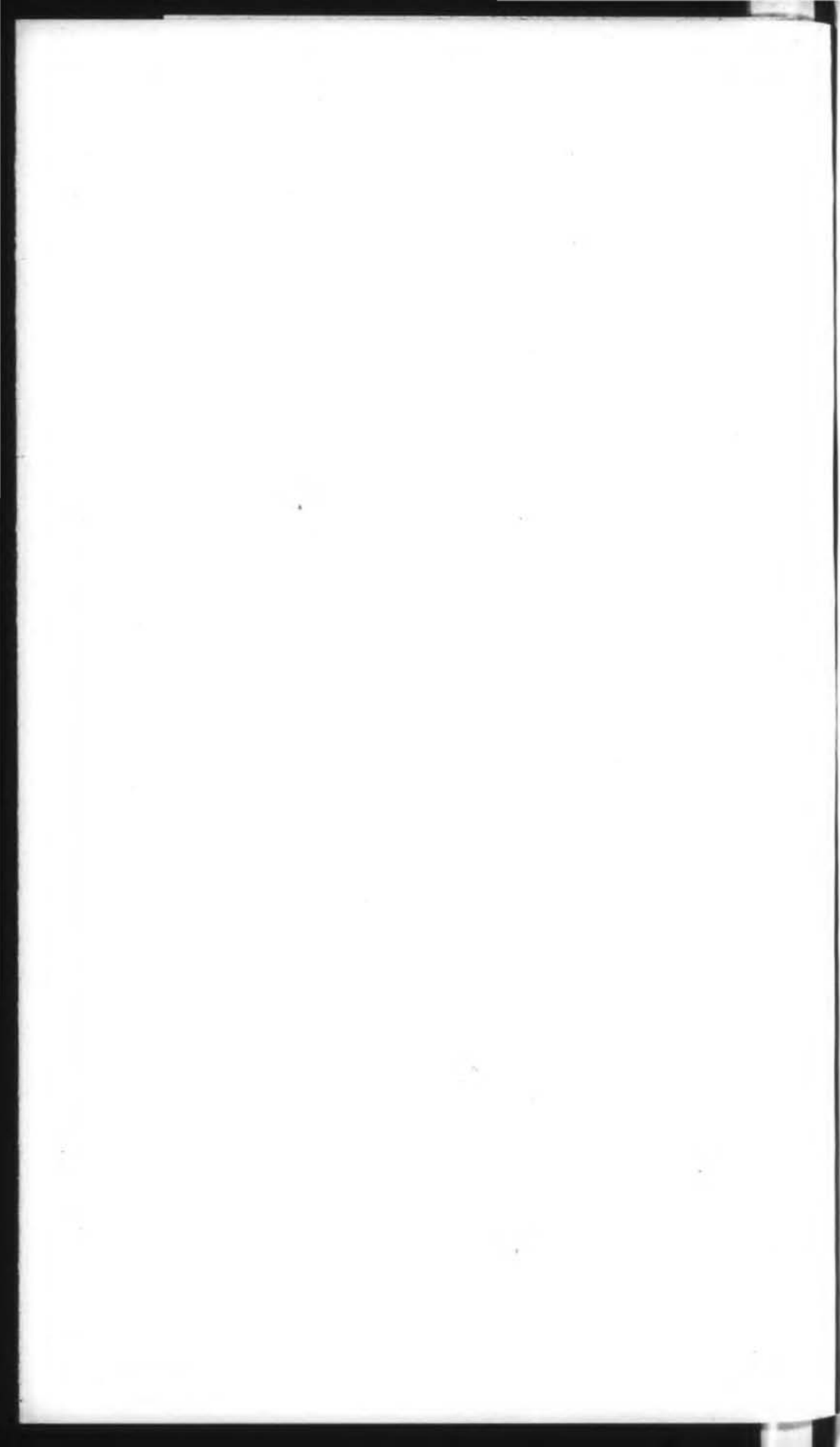


STATE ROAD DEPARTMENT OF FLORIDA
UNIT PRICES OF ITEMS LET TO CONTRACT ON BRIDGES
1927

Project No.	Contractor	Concrete—Cubic Yards						Steel—Lbs.		Machinery & Castings	Metal Hand Rail	Struc. Timber		Piling			Sand Cem. Rip-Rap Cu. Yds.	Surfacing Bridge Floor Sq. Yds.	Side-Walks Lin. feet	
		Substructure			Superstructure		Bridge Floors	Hand Rail	Reinf.			Struc.	Treated	Untreated	Treated	Untreated				Pre. Cost Conc.
		A	A-A	Seal A	A	A-A														
50-A	C. T. Dawkins	34.85			30.00			3.00	0.06											
655 & 667	Okeechobee Construction Co.											119.00	70.00	1.15						
697	C. H. Turner Co.								0.06			141.33	219.00	1.21						
665	Concrete Steel Bridge Co.	29.00		35.00	28.00			4.00	0.06	0.08	0.25	140.00		1.20	1.00	4.00	17.00	2.50		
664	Concrete Steel Bridge Co.	28.00		35.00	26.00			4.00	0.06	0.08	0.25	140.00		1.20	1.00	4.00	17.00	2.50		
640-B	Royce Kershaw, Inc.	32.00			32.00			3.00	0.06							5.30				
40-B	F. M. Stewart & Co.	34.00	36.00			32.50		3.50	0.07						1.05					
41-B	James Betteridge	20.00	25.00	24.00		26.00			0.07	0.08		6.00	190.00					2.00		
698	Peterson & Earnhart	24.00			24.00			2.50	0.05			130.00	100.00	1.20		4.60				
699	Peterson & Earnhart	24.00			24.00			2.50	0.05			130.00	100.00	1.20		4.60				
589	E. W. Parker	27.00			27.00				0.0475			115.00	70.00	1.15	0.70					
691	Murphy & Pryor	32.50			32.50			3.50	0.06							5.00				
687-A	Hayes & Kroeger	19.50			24.43			3.50	0.045					1.20						
614	E. W. Parker (150')	31.50	32.75	35.00		29.00		4.00	0.0475			125.00		1.50	0.75			1.00		
614	E. W. Parker (130')	32.75	31.50	35.00		29.00		3.00	0.0475			125.00		1.35	0.75			1.00		
614	E. W. Parker (165')		34.75	37.00		31.00		3.50	0.0475						0.80					
614	E. W. Parker (130')	31.50	32.75			29.00			0.0475						0.75					
41-B	General Construction Co.	24.00	30.00	40.00		26.00		3.00	0.0475									1.00		
53-B	W. J. Bryson Paving Co.	26.60		46.50	28.20			3.50	0.05			110.00		1.10	0.84	3.40	10.50			
685-B	Nashville Bridge Co.									0.0685	0.31	125.00								
698-B	Peterson & Earnhart											120.00	100.00	1.20						
6-B	Peterson & Earnhart	25.00			25.00			2.50	0.05						1.20	4.50	12.00			
700-B	Peterson & Earnhart	30.00			24.00		24.00	2.50	0.05	0.075	0.15				1.00	4.50				
573-D	Cox & Bryson	27.00			25.00		25.00	3.00	0.05	0.075	0.15				0.63	4.20				
661-B	The Albinson Co.	24.50			24.50			3.50	0.048											
695-B	Chas. F. McKenzie & Co.	24.00			24.00				0.08			140.00	85.00	1.00	0.85					
706	Kidd Construction Co.											110.00	80.00	1.15						
717	Gillespie & North									0.07		119.75	90.00	1.08						
585-B	Erler Corporation	29.50			24.50			2.90	0.05						1.00	5.00	10.50	2.20		

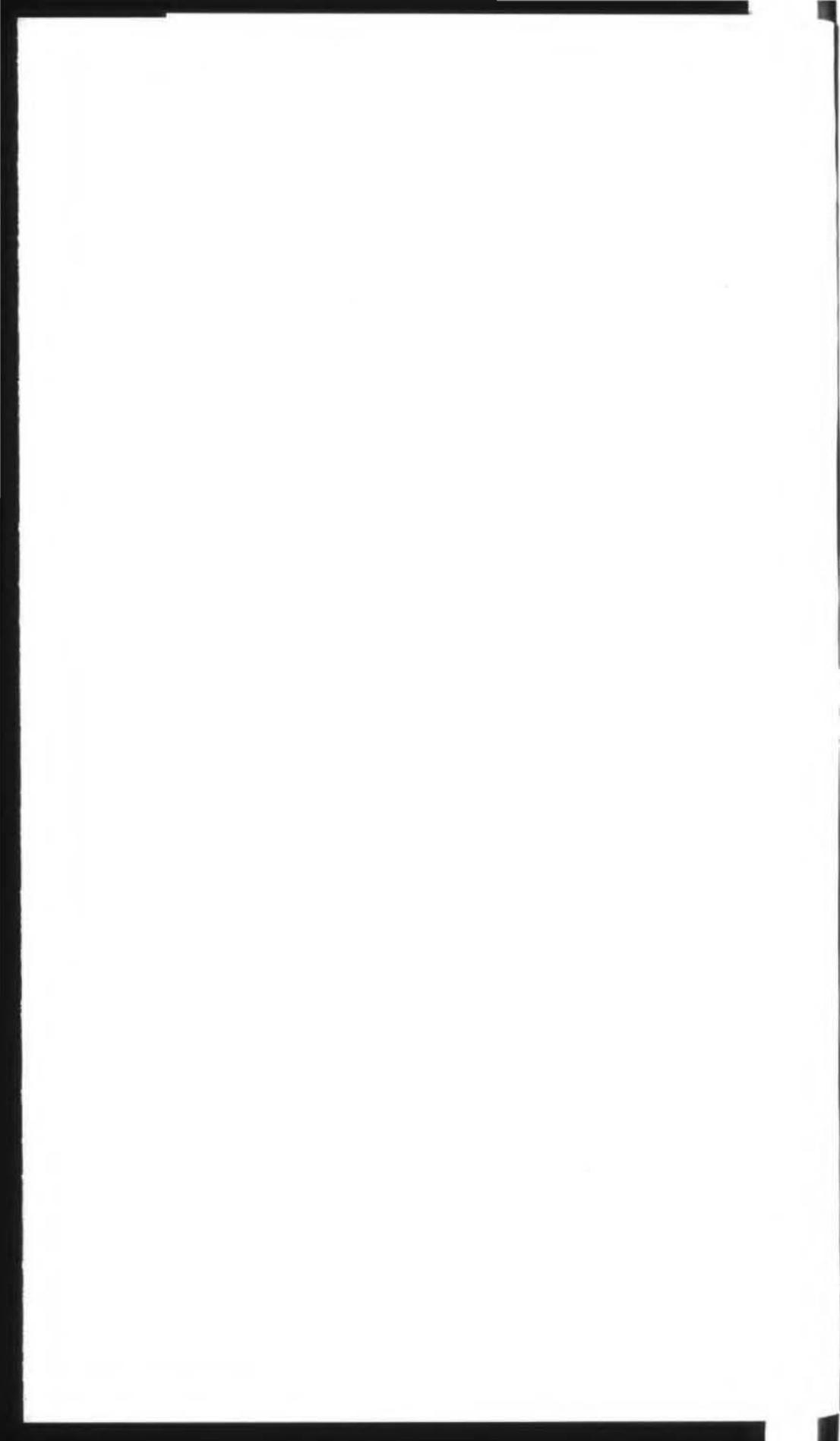
STATE ROAD DEPARTMENT OF FLORIDA
UNIT PRICES OF ITEMS LET TO CONTRACT ON BRIDGES
1928

Project No.	Contractor	Concrete—Cubic Yards						Steel—Lbs.		Machinery & Castings	Metal Hand Rail	Struc. Timber		Piling			Sand Cem. Rip-Rap Cu. Yds.	Surfacing Bridge Floor Sq. Yds.	Side-Walks Lin. feet	
		Substructure			Superstructure		Bridge Floors	Hand Rail	Reinf.			Struc.	Treated	Untreated	Treated	Untreated				Pre. Cost Conc.
		A	A-A	Seal A	A	A-A														
55-B	Sutton Brothers	23.00			23.00			1.50	0.0367					0.80		4.00	12.00			
677-D	General Construction Co.											105.00	10.00	0.89						
710-B	Frost Construction Co.	26.50			25.50				0.046					0.64						
764-B	Sutton Brothers	24.50			24.50		2.00	1.50	0.0367					1.20						
820-B	Perkins Construction Co.									0.07		108.00	85.00	1.20						
743	Maddox Foundry & Machine Co.											11.00	100.00	1.15						
769	Central Station Equipment Co.		21.65	30.23		21.81		1.40	0.044			120.00		0.95	0.68	3.05	16.00	1.95		
62-B	Gillespie & North	30.00			28.00			3.50	0.0475						0.60	3.75	10.00	1.95		
772	Central Station Equipment Co.		21.00	27.47	21.95			1.49	0.044			120.00		0.95	0.47	2.60		1.93		
683-B	R. C. Huffman Construction Co.	22.00	25.00	28.00		22.00		3.00	0.045			150.00		2.00	1.00			1.50		
654-B	R. C. Huffman Construction Co.	25.00				25.00		3.00	0.045						1.00			1.50		
653-B	R. C. Huffman Construction Co.	25.00	25.00	26.00		25.00		3.00	0.045	0.06		150.00		2.00	1.00					





Bituminous Concrete, Road 5.



Testing Division

The laboratory of the Department is located about one mile northeast of Gainesville on State Road 13 and Seaboard Air Line Railway. In addition to our own equipment, the Civil Engineering Department of the University of Florida has provided a brick rattler and a 400,000-pound universal testing machine which are available.

In 1927 the Department was requested to co-operate in a survey of sand and gravel deposits, and about 150 samples were tested. Materials were obtained from commercial plants and local deposits. In general, the unwashed samples showed low values. Results of these tests are shown in the Geological Survey's Nineteenth Annual Report (1928).

A series of tests on concrete aggregates has been commenced and about 750 specimens made and tested for compressive strength. Materials used were gravel, slag, stone, sand and cement from various sources. A small mixer was purchased so that concrete could be mixed in accordance with usual field practice. These tests will be continued and the results used to control field operations.

On large bridge projects and concrete and mixed bituminous paving, field laboratories are established to test materials at the plants. The duty of the field operator is usually to check quantities of materials, make mechanical analyses, and occasionally make additional tests such as penetration on asphalts. Two inspectors at commercial plants have been employed, one supervising shipments of lime rock and the other at plants in Hernando County furnishing concrete rock.

Some special equipment for the laboratory can only be secured by purchase, but many pieces have been assembled and erected in our own shop, generally at a saving in cost and producing more serviceable units. Articles of this type include storage tanks, drying ovens and heaters.

Tests Made During 1927-1928

3,545 Concrete blocks.
1,525 Lime and Ojus rock.
1,245 Asphalt mixtures.
1,170 Fine aggregates.
1,168 Coarse aggregates.
1,161 Concrete cores.
700 Asphalt samples.
244 Cement samples.
184 Miscellaneous.
168 Tar samples.
82 Water samples.
76 Iron samples.
32 Paint samples.

11,300 Total.

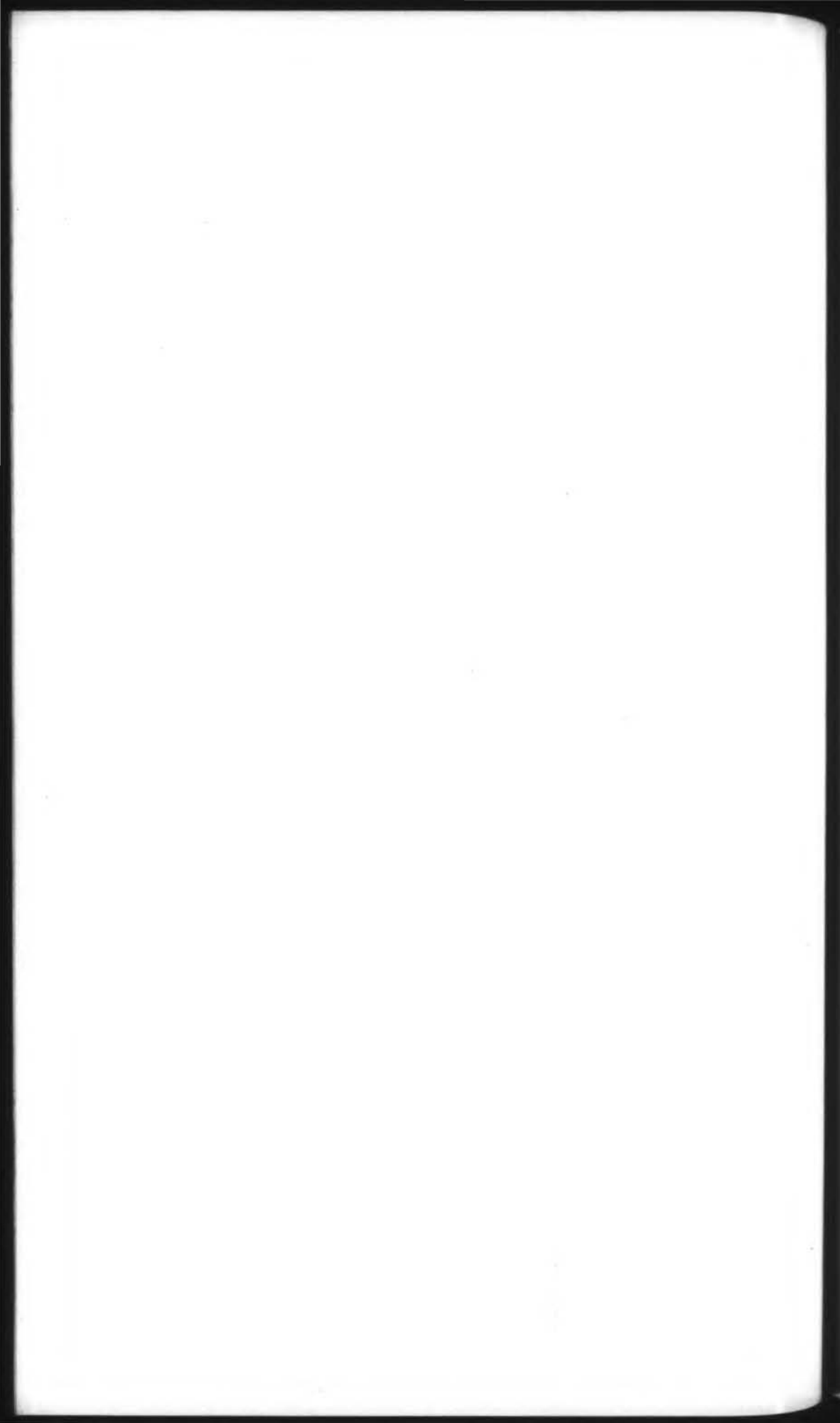
Tests Made During 1927-1928

3,545	Concrete blocks.
1,525	Lime and Ojus rock.
1,245	Asphalt mixtures.
1,170	Fine aggregates.
1,168	Coarse aggregates.
1,161	Concrete cores.
700	Asphalt samples.
244	Cement samples.
184	Miscellaneous.
168	Tar samples.
82	Water samples.
76	Iron samples.
32	Paint samples.

11,300 Total.



Creosoted Timber Bridge, Road 10, Ochlockonee River.



CONTRACTS AWARDED BY STATE ROAD DEPARTMENT
JANUARY 1st, 1927, TO DECEMBER 31st, 1927.

Project No.	Road No.	County	Contractor	Length Miles	Length Feet	Contract + 10%	Type
54	1	Leon	Noonan-Lawrence	13.00	.....	\$ 385,288.87	Concrete.
59	1	Jefferson	Higgison Construction Co.	9.10	.....	266,053.37	Concrete.
54-A, 58	1	Leon-Jefferson	Duval Engineering & Contracting Co.	12.53	.....	264,524.48	Rock Base Surface Treated.
53-A	2	Lake	Manly Construction Co.	7.10	.....	249,034.28	Bituminous Concrete.
52	1	Escambia	W. J. Bryson Paving Co.	10.089	.....	241,904.49	Clearing, Grubbing & Grading.
677-C	13	Levy	Boone & Wester	10.16	.....	224,345.97	Clearing, Grubbing & Grading.
677-D	13	Levy	Thompson & Moseley	7.58	.....	67,337.34	Clearing, Grubbing & Grading.
683-C	4	Palm Beach	Lake Worth Construction Co.	8.27	.....	44,290.95	Clearing, Grubbing & Grading.
687-B	2	Lake	B. Booth & Co.	15.22	.....	89,496.93	Clearing, Grubbing & Grading.
50-A	14	Putnam	C. T. Dawkins	.....	120	22,243.32	Concrete Overhead.
655-667	18	Highlands	Okeechobee Construction Co.	.....	915	50,006.55	Timber Bridge.
697	1	Escambia	C. H. Turner Co.	.....	488	22,911.53	Timber Bridge.
571	1	Madison	Duval Engineering & Contracting Co.	.....	.....	47,190.03	Surface Treatment.
660	3	Clay	Langston Construction Co.	.....	.....	33,538.07	Surface Treatment.
48	4	St. Johns	H. E. Wolfe	15.39	.....	370,252.82	Rock Base Surface Treated.
694	4	Martin	Nelson Brothers	8.48	.....	275,185.30	Concrete.
693	4	St. Lucie	Johnson, Drake & Piper	8.93	.....	312,662.92	Concrete.
665	3	Clay	Concrete Steel Bridge Co.	.....	1,400	208,167.96	Concrete.
664	3	Clay	Concrete Steel Bridge Co.	.....	1,600	236,366.90	Concrete.
640-B	4	Martin	Royce Kershaw, Inc.	.....	131	32,201.40	Concrete.
40-B	4	Brevard	F. M. Stuart & Co.	.....	108	40,149.91	Concrete.
641	4	Palm Beach	Langston Construction Co.	9.67	.....	188,279.21	Rock Base Surface Treated.
695	2	Lake	Samuel Vadner	10.50	.....	63,734.69	Clearing, Grubbing & Grading.
695	2	Lake	Tampa Sand & Shell Co.	.....	.....	63,368.28	Hydraulic Fill.
669-D	27	Dade	R. C. Huffman Construction Co.	12.30	.....	382,038.36	Clearing, Grubbing & Grading.
685	10	Franklin	Deen, Yarborough & Ebersbach	17.43	.....	159,980.86	Clearing, Grubbing & Grading.
648	2	Hardee	Wm. P. McDonald Construction Co.	7.14	.....	24,075.97	Surface Treatment.
676-A, B	19	Levy	L. M. Gray	24.35	.....	80,637.57	Surface Treatment.
614	5	Sarasota	West Construction Co.	17.34	.....	483,586.35	Bituminous Concrete.

CONTRACTS AWARDED BY STATE ROAD DEPARTMENT
JANUARY 1st, 1927, TO DECEMBER 31st, 1927 (Continued)

Project No.	Road No.	County	Contractor	Length Miles	Length Feet	Contract + 10%	Type
687-A	2	Lake	Manly Construction Co.	15.00	.....	\$ 436,551.76	Sheet Asphalt.
543	3	Seminole	M. C. Winterburn, Inc.	14.20	.....	405,296.30	Bituminous Macadam.
648	2	Hardee	Wm. P. McDonald Construction Co.	6.36	.....	123,804.83	Rock Base Surface Treated.
659	3	Clay	Duval Engineering & Contracting Co.	2.80	.....	49,310.97	Rock Base Surface Treated.
668	4	Brevard	C. A. Steed & Sons Co.	13.45	.....	273,640.32	Rock Base Surface Treated.
564-C	5	Charlotte	Stidham & Hughes	3.93	.....	81,173.55	Rock Base Surface Treated.
676-C	19	Levy	H. L. Clark & Sons Co.	15.01	.....	227,110.22	Rock Base Surface Treated.
573-D	2	Orange	F. X. Bradley & Co.	16.81	.....	95,642.25	Clearing, Grubbing & Grading.
713	28	Columbia	C. A. Henderson	10.00	.....	85,284.71	Clearing, Grubbing & Grading.
41-B	4	Dade	James Betteridge	.....	88	42,387.84	Concrete Bridge.
698	19	Leon	Peterson & Earnhart	.....	400	34,773.06	Concrete Bridge.
699	19	Jefferson	Peterson & Earnhart	.....	300	27,441.17	Concrete Bridge.
641	4	Palm Beach	John J. Quinn, Inc.	1.00	.....	52,494.31	Surface Treatment.
745	19	Taylor	Board County Comm's Taylor Co.	14.00	.....	12,320.00	Clearing & Grubbing.
747	35	Jefferson	Finlayson & Morris	6.50	.....	40,566.79	Clearing, Grubbing & Grading.
694	4	Martin	C. S. Maulsby	.....	.....	10,780.00	Protection.
663-679	5	Citrus-Hernando	Kibbey Engineering Co.	.....	.....	19,145.28	Guard Rail.
669-E	27	Dade	Alexander, Ramsey & Keer	4.27	.....	205,700.00	Clearing & Grubbing.
522	3	Nassau	Langston Construction Co.	4.06	.....	43,394.76	Rock Base Surface Treated.
593	5	Manatee	Walter J. Bryson Paving Co.	0.66	.....	12,058.22	Bituminous Concrete.
589	5	Lee	E. W. Parker	.....	313	43,942.63	Concrete Bridge.
691	4	Indian River	Murphy & Pryor	.....	285	60,594.05	Concrete Overhead.
687-A	2	Lake	Hayes & Kroeger	.....	120	18,264.37	Concrete Overhead.
614	5	Sarasota	E. W. Parker	.....	165	50,330.50	Concrete Bridge.
614	5	Sarasota	E. W. Parker	.....	150	71,320.42	Concrete Bridge.
614	5	Sarasota	E. W. Parker	.....	130	53,530.89	Concrete Bridge.
614	5	Sarasota	E. W. Parker	.....	130	68,210.17	Concrete Bridge.
49	4	Flagler	Cone Brothers Construction Co.	13.81	.....	252,196.06	Rock Base Surface Treated.
691	4	Indian River	Fowler & Banko, Inc.	5.52	.....	165,364.35	Concrete.

692	4	St. Lucie	Fowler & Banko, Inc.	7.38		229,002.48	Concrete.
41-B	4	Dade	General Construction Co.		132	40,529.06	Concrete.
716	28	Bradford	F. X. Bradley & Co.	11.12		57,024.85	Clearing, Grubbing & Grading.
717	28	Bradford	F. X. Bradley & Co.	10.93		85,834.62	Clearing, Grubbing & Grading.
50-B	14	Putnam	L. M. Gray	9.77		158,822.09	Rock Base Surface Treated.
50-C	14	Putnam	N. C. Cash	10.03		178,026.92	Rock Base Surface Treated.
619	5	Alachua	C. R. Scott	9.28		134,370.72	Rock Base Surface Treated.
6	1	Madison	Higgison Construction Co.	5.45		47,471.46	Clearing, Grubbing & Grading.
55	14	Alachua	Walter J. Bryson Paving Co.	16.77		142,280.00	Clearing, Grubbing & Grading.
697	1	Escambia	Harrison & Estes	14		9,013.01	Clearing, Grubbing & Grading.
706-A	28	Putnam	Franklin Construction Co.	12.09		83,567.57	Clearing, Grubbing & Grading.
714	28	Union	Walter J. Bryson Paving Co.	10.20		67,006.89	Clearing, Grubbing & Grading.
715	28	Union	Sellers Construction Co.	3.20		20,752.62	Clearing, Grubbing & Grading.
742	14	Alachua	Little & Lee, Contractors	7.65		39,762.96	Clearing, Grubbing & Grading.
680	10	Bay	Johnson, Drake & Piper		4,000	829,392.71	Concrete & Steel Bridge.
681	10	Bay	Johnson, Drake & Piper		7,530	1,119,966.46	Concrete & Steel Bridge.
764	50	Suwannee	F. W. Simpson	12.00		55,731.19	Clearing, Grubbing & Grading.
765	50	Suwannee	F. W. Simpson	7.00		30,428.89	Clearing, Grubbing & Grading.
677-A	13	Levy	Duval Engineering & Contracting Co.	6.96		88,648.38	Rock Base Surface Treated.
53-B	2	Lake	Walter J. Bryson Paving Co.		6.71	96,431.98	Concrete Bridge.
719	5-A	Suwannee	C. G. Kershaw Contracting Co.	8.57		40,355.35	Clearing, Grubbing & Grading.
53-B	2	Lake	American Bascule Bridge Corporation			16,471.40	Bascule.
685-B	10	Franklin	Nashville Bridge Co.		120	16,183.75	Steel Span.
677-D	13	Levy	N. B. Burton	3.05		15,827.95	Clearing, Grubbing & Grading.
668	4	Brevard	Carl Fay			7,326.00	Clearing, Grubbing & Grading.
698-B	19	Leon	Peterson & Earnhart		110	4,689.08	Overhead Timber.
518	5-A	LaFayette	Barnes Construction Co.	17.75		122,259.23	Clearing, Grubbing & Grading.
535	5-A	LaFayette	L. B. McLeod Construction Co.	13.19		68,103.35	Clearing, Grubbing & Grading.
624	50	Hamilton	L. B. McLeod Construction Co.	6.23		36,848.61	Clearing, Grubbing & Grading.

CONTRACTS AWARDED BY STATE ROAD DEPARTMENT
JANUARY 1st, 1927, TO DECEMBER 31st, 1927 (Continued)

Project No.	Road No.	County	Contractor	Length Miles	Length Feet	Contract + 10%	Type
706-B	28	Putnam	Curry & Turner	14.91	.....	\$ 68,419.63	Clearing, Grubbing & Grading.
708	11	Jefferson	Hardee-Fisher Co., Inc.	7.98	.....	64,781.26	Clearing, Grubbing & Grading.
720	11	Jefferson	Hardee-Fisher Co., Inc.	9.64	.....	62,038.84	Clearing, Grubbing & Grading.
722	43	Jefferson	R. J. Carroll	8.83	.....	67,379.51	Clearing, Grubbing & Grading.
723	66	Leon	L. B. McLeod Construction Co.	11.76	.....	91,031.05	Clearing, Grubbing & Grading.
763	50	Suwannee	A. E. Campbell	12.23	.....	35,291.76	Clearing, Grubbing & Grading.
6-B	1	Madison	Peterson & Earnhart	.....	297	58,579.56	Concrete Bridge.
700-B	19	Jefferson	Peterson & Earnhart	.....	496	64,333.54	Concrete Bridge.
669-X-Z	27	Collier	N. S. Golden	.....	600	8,801.65	Timber Bridge.
50-A	14	Putnam	L. M. Gray	6.18	.....	107,716.22	Rock Base Surface Treated.
669-X-Z	27	Collier	H. E. Wolfe Construction Co.	15.88	.....	220,281.24	Rock Base Surface Treated.
640-A	4	Martin	West Construction Co.	9.00	.....	245,980.24	Sheet Asphalt.
640-B	4	Martin	West Construction Co.	11.80	.....	338,710.85	Sheet Asphalt.
654	4	Brevard	S. P. Snyder & Son	6.30	.....	160,037.98	Bituminous Macadam.
615	5	Sarasota	E. F. Powers Construction Co.	11.92	.....	118,804.33	Clearing, Grubbing & Grading.
573-D	2	Orange	Cox & Bryson Paving Co.	.....	173	30,833.50	Concrete Overhead.
661-B	2	Lake	The Albinson Co.	.....	40	18,408.24	Concrete Bridge.
695-B	2	Lake	Chas. F. McKenzie & Co.	.....	509	36,939.21	Concrete Bridge.
706	28	Putnam	Kidd Construction Co.	.....	675	33,826.98	Timber Bridge.
717	28	Bradford	Gillespie & North	.....	110	5,139.64	Timber Overhead.
593	5	Manatee	W. M. Hartin	.....	.....	7,012.50	Remove Bridge.
659	3	Clay	Duval Engineering & Contracting Co.	2.53	.....	38,223.30	Rock Base Surface Treated.
695-B	2	Lake	American Bascule Bridge Corporation	.....	.....	17,585.70	Bascule.
695	2	Lake	Langston Construction Co.	.43	.....	6,141.57	Rock Base Surface Treated.
669-W	27	Collier	H. E. Wolfe	9.00	.....	154,692.78	Surface Treatment.
585-B	1	Santa Rosa	The Erler Corporation	.....	165	27,386.33	Concrete Bridge.
521	3	Nassau	Hatheway & Demarest	.22	.....	4,991.80	Clearing, Grubbing & Grading.
Totals				667.84	22,471	\$13,926,983.25	



Plain Cement Concrete, Road 1.



CONTRACTS AWARDED BY STATE ROAD DEPARTMENT

JANUARY 1st, 1928, TO DECEMBER 31st, 1928.

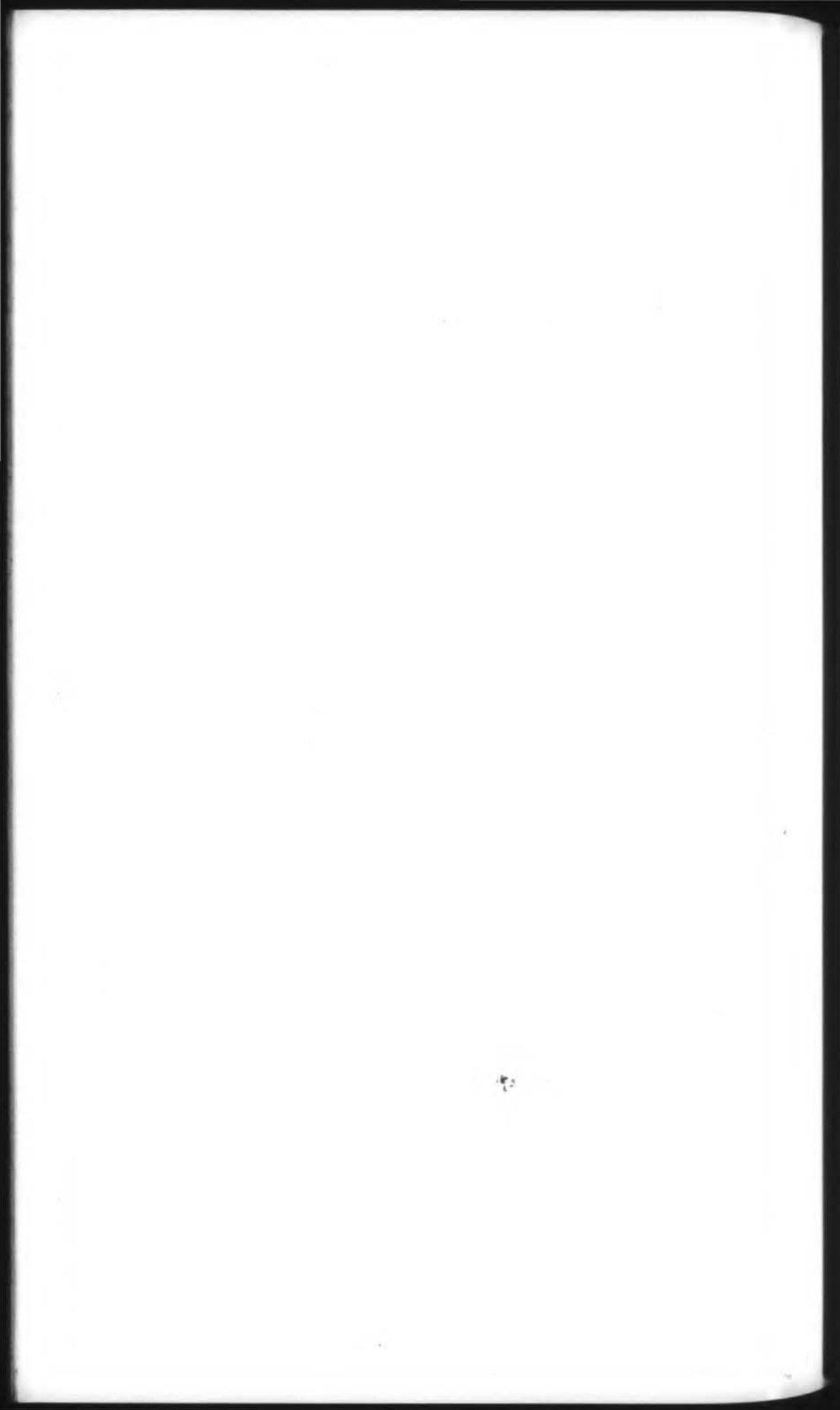
Project No.	Road No.	County	Contractor	Length Miles	Length Feet	Contract + 10%	Type
55-B	14	Alachua	Sutton Brothers		457	\$ 54,272.17	Concrete Bridge.
677-D	13	Levy	General Construction Co.		1,335	43,000.98	Timber Bridge.
710-B	17	Hillsborough	Frost Construction Co.		260	54,775.16	Concrete Bridge.
764-B	50	Suwannee	Sutton Brothers		113	19,043.82	Concrete Overhead.
677-D	13	Levy	Duval Engineering & Contracting Co.	8.91		110,370.35	Rock Base Surface Treated
710-C	17	Hillsborough	E. F. Powers Construction Co.	12.69		95,125.45	Clearing, Grubbing & Grading.
755	17	Polk	B. Booth	11.22		58,518.86	Clearing, Grubbing & Grading.
757	2	Polk	Little & Lee	10.26		63,516.17	Clearing, Grubbing & Grading.
758	2	Polk	Little & Lee	8.34		46,380.34	Clearing, Grubbing & Grading.
732	17	Polk	Gilbert & Hadsock	8.94		57,077.26	Clearing, Grubbing & Grading.
736	39	Holmes	C. G. Kershaw Contracting Co.	8.58		45,097.93	Clearing, Grubbing & Grading.
709	39	Holmes	C. G. Kershaw Contracting Co.	9.09		53,931.71	Clearing, Grubbing & Grading.
61-C	1	Gadsden	Morgan-Hill Paving Co.	9.77		244,581.31	Concrete.
710-A	17	Hillsborough	Franklin Construction Co.	6.18		38,592.08	Clearing, Grubbing & Grading.
697	1	Escambia	E. M. Chadham	.20		3,401.25	Surface Treatment.
683-A	4	Palm Beach	Robert G. Lassiter & Co.	8.04		264,999.43	Concrete.
695	2	Lake	Rutherford Construction Co.	3.00		46,992.07	Rock Base Surface Treated.
827	1	Escambia	S. G. Collins	8.12		50,151.29	Clearing, Grubbing & Grading.
61-A	1	Gadsden	M. C. Winterburn, Inc.	10.00		231,578.27	Concrete.
669-X	27	Collier	Phoenix Asphalt Paving Co.	9.40		28,814.77	Surface Treatment.
62-C	24	Osceola	Everglades Construction Co.	11.83		115,303.71	Clearing, Grubbing & Grading.
62-D	24	Osceola	A. D. Weeks	12.62		83,299.72	Clearing, Grubbing & Grading.
62-A	24	Osceola	A. D. Weeks	12.52		66,571.01	Clearing, Grubbing & Grading.
807-A	25	Palm Beach	R. C. Huffman Construction Co.	10.81		201,713.22	Rock Base Surface Treated.
807-C	25	Palm Beach	R. C. Huffman Construction Co.	6.14		99,923.54	Rock Base Surface Treated.
804	67	Glades	C. A. Steed & Sons, Inc.	18.55		416,083.52	Rock Base Surface Treated.
659	3	Clay	Duval Engineering & Contracting Co.	7.25		95,449.25	Rock Base Surface Treated.

CONTRACTS AWARDED BY STATE ROAD DEPARTMENT
JANUARY 1st, 1928, TO DECEMBER 31st, 1928 (Continued)

Project No.	Road No.	County	Contractor	Length Miles	Length Feet	Contract + 10%	Type
815	54	Okaloosa	Silas Gibson	13.58	.....	\$ 60,680.23	Clearing, Grubbing & Grading.
819	54	Okaloosa	Walter J. Bryson Paving Co.	4.69	.....	20,979.67	Clearing, Grubbing & Grading.
823	41	Okaloosa	Walter J. Bryson Paving Co.	9.18	.....	34,085.34	Clearing, Grubbing & Grading.
824	41	Okaloosa	Walter J. Bryson Paving Co.	9.81	.....	59,898.02	Clearing, Grubbing & Grading.
820-B	96	Jefferson	Perkins Construction Co.	.....	114	4,881.25	Timber Bridge.
743	10	Bay	Maddox Foundry & Machine Co.	.....	120	13,774.47	Timber Bridge.
802-A	10	Okaloosa	C. C. Hayes	8.67	.....	76,559.56	Clearing, Grubbing & Grading.
661	2	Lake	P. B. Alsobrook	0.10	.....	1,980.00	Clearing, Grubbing & Grading.
769	5	Lee	Central Station Equipment Co.	.....	4,430	585,216.61	Concrete Bridge.
803	10	Okaloosa	Collins Construction Co.	11.13	.....	82,342.33	Graded & Drained.
56	10	Leon	Nelson Brothers	17.982	.....	430,409.67	Concrete.
53-C	2	Lake	Manly Construction Co.	2.85	.....	67,778.57	Bituminous Concrete.
719	5-A	Suwannee	Broadbent Construction Co.	8.57	.....	91,044.80	Rock Base Surface Treated.
764	50	Suwannee	Duval Engineering & Contracting Co.	12.00	.....	140,666.31	Rock Base Surface Treated.
687-B	2	Lake	Wm. P. McDonald Construction Co.	15.22	.....	209,058.24	Rock Base Surface Treated.
802-C	10	Okaloosa	Curry & Turner	10.24	.....	45,545.92	Graded & Drained.
669-W	27	Collier	H. E. Wolfe	16.00	.....	58,856.15	Surface Treated.
740	10	Gulf	Baker-Lewis Construction Co.	9.62	.....	37,400.00	Hauling Rock.
669	27	Collier	Alexander, Ramsey & Kerr	32.00	.....	38,720.00	Guard Rail.
685	10	Franklin	L. L. Pararo Construction Co.	18.46	.....	93,500.00	Hauling Rock.
669-V	27	Collier	H. E. Wolfe	31.62	.....	116,314.44	Surface Treatment.
723-724	66	Leon	W. B. Wright	21.41	.....	33,000.00	Hauling.
672	1	Leon	J. B. McCrary Engineering Corporation	9.92	.....	208,641.07	Concrete.
673	1	Gadsden	Robert G. Lassiter & Co.	14.97	.....	292,508.17	Concrete.
733	33	Walton	Walter J. Bryson Paving Co.	4.23	.....	47,373.22	Sand Clay.
734	40	Walton	Penton-Mathis Construction Co.	7.89	.....	69,581.67	Sand Clay.
735	40	Walton	Walter J. Bryson Paving Co.	13.71	.....	130,342.33	Sand Clay.

751	40	Walton.....	Walter J. Bryson Paving Co.....	7.28	.....	53,739.49	Sand Clay.
752	40	Walton.....	Walter J. Bryson Paving Co.....	8.72	.....	63,037.15	Sand Clay.
788	10	Walton.....	Walter J. Bryson Paving Co.....	17.54	.....	164,274.07	Graded & Drained.
842	115	Walton.....	Walter J. Bryson Paving Co.....	10.00	.....	76,591.51	Graded & Drained.
669-C	27	Dade.....	H. E. Wolfe Construction Co.....	12.00	.....	74,142.12	Surface Treated.
669-Y	27	Collier.....	Wm. P. McDonald Construction Co....	13.55	.....	117,119.60	Graded & Drained.
764*	50	Suwannee.....	Duval Engineering & Contracting Co...	.....	.....	42,380.41	Surface Treatment.
52	1	Escambia.....	Robert G. Lassiter & Co.....	10.00	.....	203,982.90	Concrete.
763	50	Suwannee.....	Duval Engineering & Contracting Co...	12.23	.....	150,199.28	Rock Base Surface Treated.
765	50	Suwannee.....	Duval Engineering & Contracting Co...	7.00	.....	94,113.63	Rock Base Surface Treated.
614*	5	Sarasota.....	Walter J. Bryson Paving Co.....	.....	.....	38,095.31	Asphalt Surface.
669-D	27	Dade.....	H. E. Wolfe Construction Co.....	5.00	.....	11,035.53	Surface Treatment.
669-E	27	Dade.....	Alexander, Ramsey & Kerr, Inc.....	.....	.....	5,505.50	Guard Rail.
669-W	27	Collier.....	Alexander, Ramsey & Kerr, Inc.....	.....	.....	6,380.00	Guard Rail.
743	10	Bay.....	Baker-Lewis Construction Co.....	18.25	.....	77,000.00	Hauling.
Totals.....				605.88	6,829	\$ 6,917,299.18	

Note—* Included in contract above.





Rock Base Surface Treated, Road 4.



STATE ROAD DEPARTMENT OF FLORIDA
TRAFFIC CENSUS REPORT FOR 1927

Road No.	Location	January		February		March		April		May		June	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Bridge...	537	185	417	122	546	184	1,186	81	.....	.....	688	147
1	Victory Bridge.....	605	89	639	107	639	111	1,222	24	724	96	638	104
1	Ellaville.....	424	35	268	27	394	74	708	33	622	67	417	42
2	White Springs.....	800	89	749	106	782	74	.....	.....	.....	.....	.....	.....
2	High Springs.....	1,777	158	1,427	117	1,167	191	1,137	155	2,039	133	1,606	176
2	3 Miles South Ocala.....	.....	.....	.....	.....	.....	.....	1,718	60	.....	.....	1,396	169
2	Kissimmee-Orlando.....	2,854	400	2,478	218	2,988	52	2,456	250	1,956	66	2,339	85
2	Kissimmee-Lakeland.....	1,900	108	1,991	117	1,746	97	1,684	112	1,531	109	1,362	188
2	Arcadia.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....
3	Wilds Landing.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....
3	Orlando-Sanford.....	.....	.....	2,086	299	1,023	300	.....	.....	1,690	335	1,299	189
4	Callahan.....	740	137	722	125	960	154	973	46	730	129	979	126
4	North St. Augustine.....	852	126	735	192	1,244	246	955	205	864	166	903	110
4	South of Hastings.....	845	62	628	79	764	101	644	140	472	87	499	93
4	Sharpes.....	1,078	.....	1,176	.....	1,410	.....	982	.....	1,043	.....	863	.....
4	Vero.....	1,872	402	1,678	226	1,663	204	1,454	317	1,256	230	922	2
4	Dade-Broward.....	4,729	570	4,532	869	4,345	881	3,458	849	4,039	256	2,934	543
5	Montbrook.....	675	77	629	41	694	58	787	46	769	80	591	73
5	Brooksville.....	1,722	103	1,705	57	1,789	42	2,938	43	3,093	53	2,391	21
5	Palmetto-Elleston.....	2,727	303	1,652	241	1,085	150	1,274	126	1,000	149	1,039	126
17	Lakeland-Plant City.....	1,999	372	2,331	357	2,196	307	1,694	283	1,271	212	1,130	189
17	Tampa-Oldsmar.....	1,996	154	1,999	197	1,585	206	1,522	175	1,240	175	1,139	139
24	Kissimmee-Melbourne.....	357	.....	.....	.....	389	.....	452	.....	380	.....	512	.....
	Total.....	28,489	3,370	27,842	3,497	28,409	3,432	27,244	2,945	24,719	2,343	23,647	2,585

STATE ROAD DEPARTMENT OF FLORIDA
TRAFFIC CENSUS REPORT FOR 1927 (Continued.)

Road No.	Location	July		August		September		October		November		December	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Bridge.....	.....	.....	752	150	996	168	743	179	732	124	927	156
1	Victory Bridge.....	1,202	24	814	76	865	87	731	101	917	91	.....	.....
1	Ellaville.....	699	33	608	82	.....	.....	486	54	687	59	1,117	36
2	White Springs.....	.....	.....	.....	.....	.....	.....	.....	.....	846	106	1,045	88
2	High Springs.....	1,929	101	864	82	1,288	73	1,043	100	.....	.....	1,361	79
2	3 Miles South Ocala.....	1,965	36	1,321	182	1,662	126	1,197	148	1,780	59	2,025	77
2	Kissimmee-Orlando.....	1,211	.....	1,641	240	2,400	75	1,840	181	1,653	92	2,475	187
2	Kissimmee-Lakeland.....	1,682	.....	1,407	138	1,705	.....	1,604	297	.....	.....	2,054	140
2	Arcadia.....	.....	.....	.....	.....	.....	.....	.....	.....	1,058	61	939	93
3	Wilds Landing.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	576	.....
3	Orlando-Sanford.....	1,437	274	1,225	217	1,261	256	1,326	255	735	96	1,058	175
4	Callahan.....	867	115	943	148	1,085	81	849	82	1,064	68	1,209	119
4	North St. Augustine.....	1,176	154	1,036	154	960	122	1,035	129	2,346	76	1,249	152
4	South of Hastings.....	316	53	237	61	300	62	186	21	296	50	841	62
4	Sharpes.....	914	.....	896	.....	636	.....	1,062	.....	1,270	.....	1,751	.....
4	Vero.....	986	32	1,307	272	1,664	206	1,359	272	1,166	.....	1,776	301
4	Dade-Broward.....	.....	.....	1,073	157	1,006	130	1,145	71	1,508	509	1,308	162
5	Montbrook.....	949	87	688	92	698	76	605	93	819	37	1,278	22
5	Brooksville.....	2,671	44	2,852	12	1,546	106	1,119	134	2,863	182	2,961	101
5	Palmetto-Ellenton.....	885	109	933	125	915	155	2,239	139	1,185	141	1,109	160
17	Lakeland-Plant City.....	1,254	175	1,078	170	1,144	217	1,182	187	1,724	101	1,252	228
17	Tampa-Oldsmar.....	1,397	159	1,137	141	1,321	171	1,208	151	1,716	115	1,702	134
24	Kissimmee-Melbourne.....	422	.....	567	.....	.....	.....	590	.....	722	.....	1,041	.....
	Total.....	21,962	1,396	21,379	2,499	21,452	2,111	21,549	2,594	25,087	1,967		



S. A. L. Under-Pass, Road 2.



STATE ROAD DEPARTMENT OF FLORIDA
TRAFFIC CENSUS REPORT FOR 1928.

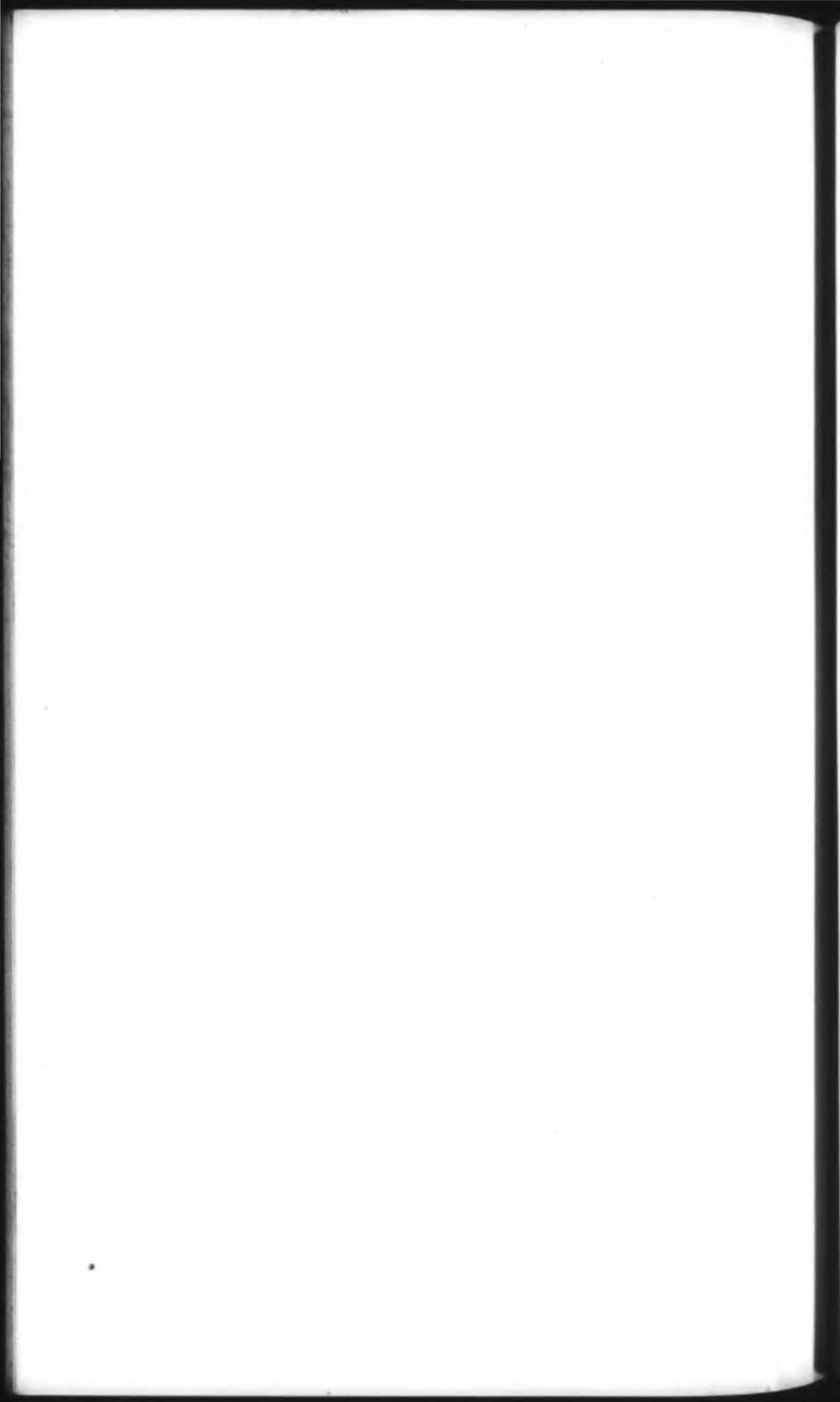
Road No.	Location	January		February		March		April		May		June	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Bridge . . .	826	110	582	72	921	128	.....	.....	696	110	1,092	44
1	Victory Bridge . . . . .	.....	.....	.....	.....	776	84	1,024	99	825	88	1,005	41
1	Ellaville	576	77	442	53	615	52	516	74	755	72	731	43
2	White Springs	534	78	638	86	775	100	789	.....	721	.....	1,100	44
2	High Springs	1,472	140	1,777	230	1,384	95	829	102	840	105	820	95
2	Bellevue	1,261	174	1,221	179	1,460	216	1,103	199	1,257	185	1,597	193
2	Kissimmee-Orlando	2,600	167	3,012	160	2,959	91	2,503	89	2,051	81	1,861	105
2	Kissimmee-Lakeland	2,078	185	2,224	264	2,210	147	1,983	248	1,713	185	1,508	149
2	Arcadia	644	92	503	56	599	102	663	137	.....	.....	796	183
3	Wilds Landing	338	.....	337	.....	503	.....	326	.....	385	.....	796	.....
3	Orlando-Sanford	1,068	152	1,563	240	2,617	71	1,321	264	1,116	191	1,354	302
3	Dunn's Creek	452	122	491	100	683	41	425	118	459	104	518	88
4	Callahan	762	111	518	115	810	159	585	106	721	67	1,051	62
4	Bunnell	732	56	582	113	579	79	563	133	512	288	579	80
4	Sharpes	1,369	.....	1,222	.....	1,943	.....	1,456	.....	1,131	.....	1,243	.....
4	Vero	1,618	269	1,807	262	1,741	230	1,533	334	1,638	249	1,456	153
4	Dade-Broward	2,052	92	3,647	621	3,295	836	3,618	218	2,297	540	1,066	.....
5	Dunnellon	1,122	165	1,084	148	1,253	153	1,100	156	1,354	198	788	163
5	Brooksville	2,845	53	2,806	63	2,754	122	2,523	105	2,094	55	2,154	71
5	Palmetto-Elleston	942	131	1,221	179	1,105	229	1,140	203	994	200	1,095	218
17	Lakeland-Plant City	1,422	237	1,632	231	1,453	239	1,132	233	1,262	231	1,174	178
17	Tampa-Oldsmar	1,388	140	1,553	159	1,365	145	1,060	167	977	110	1,351	89
24	Kissimmee-Melbourne	703	.....	1,080	.....	728	.....	517	.....	441	.....	277	49
27	Carnestown	No reports were taken here until September.											
	Total	26,804	2,551	29,942	3,331	32,528	3,318	26,709	2,985	24,239	3,059	25,412	2,350

STATE ROAD DEPARTMENT OF FLORIDA
TRAFFIC CENSUS REPORT 1928 (Continued.)

Road No.	Location	July		August		September		October		November		December	
		Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks	Cars	Trucks
1	Blackwater River Bridge...	835	164	1,026	184	776	110	623	173	1,040	119	1,123	56
1	Victory Bridge.....	851	149	734	154	690	153	683	141	972	136	1,040	91
1	Ellaville.....	622	61	50	546	578	87	543	94	550	54	731	53
2	White Springs.....	799	96	1,420	168	891	128	684	155	782	104	1,042	141
2	High Springs.....	892	125	881	155	829	110	622	120	778	110	1,196	76
2	Bellevue.....	1,472	183	.....	.....	974	73	1,236	79	1,573	99	2,501	200
2	Kissimmee-Orlando.....	1,369	63	1,649	83	1,274	104	1,424	174	1,510	179	2,080	178
2	Kissimmee-Lakeland.....	1,447	110	1,702	169	1,650	208	1,766	198	1,653	132	2,345	186
2	Arcadia.....	603	48	895	91	592	79	699	127	761	175	1,010	182
3	Wilds Landing.....	485	.....	548	.....	.....	.....	521	.....	559	.....	636	55
3	Orlando-Sanford.....	893	103	.....	.....	.....	.....	799	130	799	113	1,031	124
3	Dunn's Creek.....	392	100	.....	.....	399	103	509	99	650	41	779	36
4	Callahan.....	750	181	696	173	902	157	881	168	786	115	1,130	199
4	Bunnell.....	560	102	1,013	.....	547	84	482	110	620	124	883	114
4	Sharpes.....	1,101	.....	1,197	.....	1,504	.....	1,344	.....	1,018	.....	2,082	.....
4	Vero.....	1,281	223	1,280	185	1,423	174	1,464	169	1,574	137	1,544	249
4	Dade-Broward.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....	.....
5	Dunnellon.....	1,037	203	.....	.....	998	237	998	229	1,064	242	1,245	77
5	Brooksville.....	1,868	125	2,191	118	2,367	79	2,659	102	3,029	141	3,087	102
5	Palmetto-Elleston.....	1,059	205	1,044	206	1,079	230	1,057	299	1,040	258	999	175
17	Lakeland-Plant City.....	1,136	190	989	168	1,108	196	1,519	198	1,409	79	1,419	174
17	Tampa-Oldsmar.....	1,021	104	1,002	85	652	139	840	125	978	131	1,188	136
24	Kissimmee-Melbourne.....	396	.....	448	.....	655	.....	724	.....	902	.....	910	.....
27	Carnestown.....	.....	.....	.....	.....	184	57	212	45	368	36	409	23
	Total.....	20,869	2,535	18,765	2,485	20,072	2,508	22,189	2,935	24,415	2,525	30,410	2,627



Rock Base Surface Treated, Road 5.



Maintenance

Maintenance is something that follows all construction if the money invested is safeguarded. Maintenance of roads is a problem in all Highway Departments. This Department has recognized the importance of maintenance, as by proper maintenance more service is rendered by the highways. The Department starts maintenance as soon as construction finishes on all projects.

On the early projects constructed by the Department maintenance costs were high due largely to the isolation of the project or section of road. During this biennium gaps have been closed on our main roads and maintenance is practically continuous. The Department, several years ago, inaugurated the patrol method of maintenance and under this method roads were divided into sections and a patrolman placed in charge and given the necessary equipment and labor to properly maintain the road. The work of maintenance consists principally of four operations: Maintenance of right-of-way, road bed and ditch, surfaces and structures.

From the accompanying tables it will be noted the larger percentage of cost is represented by maintenance of the road bed and ditches and of this cost the greater percentage is in maintaining the proper drainage of the road bed, the Department feeling that no road is properly constructed unless it is adequately drained.

The cost of maintaining a road surface is about inversely in proportion to the cost of surface to be maintained; that is, the cheaper the surface, the higher the maintenance cost. The Budget for Maintenance for 1927 called for an expenditure of \$1,888,645.94 on 2,104 miles of road, of which a large percentage was sand-clay surface. A sand-clay road being of a low type of cost necessitates a high maintenance cost. Therefore, the Department developed a surface treatment for sand-clay at a very nominal cost that reduced the cost of maintenance per mile. The actual amount of monies spent in 1927 on maintenance exceeded the Budget by \$853,000.00, the greater percentage of this representing the cost of the surface treatment on those sections of sand-clay roads, together with an increased mileage actually maintained during that year. This was reflected in the 1928 work, which called for \$2,028,642.77 to be expended on 2,785 miles, being

an increase in mileage of 681 miles and an increased cost of \$220,000.00, but the Department spent \$112,000.00 less on maintenance in 1928 than was estimated in the Budget. The improvement in the type of surface on sand-clay roads being largely responsible for this saving.

It is noted by the Budget for Maintenance for 1929 that the mileage under maintenance has increased to 3,331 miles and the routine maintenance cost is \$1,182,669.00, which is a decided reduction in the cost of routine maintenance. Routine maintenance covers those items previously mentioned as "right-of-way, road bed and ditches, surfaces and structures." This reduction has been accomplished by two things: (1) Improving the type of surfaces; that is, the elimination of large sand-clay mileage and (2), closing of gaps on roads, enabling patrol sections to be lengthened and an increased efficiency on the part of the workmen.

It will also be noticed that this mileage covers all types of road, both built by the State and sections built by the counties and taken over for maintenance by this Department. There are 2,861 miles of State built roads and 470 miles county construction under maintenance. The accompanying tables show the individual types of road. In addition to the routine maintenance, the distribution for this year includes an item of "betterment," which is handled as maintenance. The tentative budget contemplates \$401,478.00 for these betterment items in the various Divisions. This is for improving the types of road surfaces and structures, particularly on those sections of County built roads that have been taken over for maintenance by the Department, as well as further surface treatments on sand-clay roads.

In 1927 the Department, in connection with its maintenance work and as an added safety to the traveling public, started center line marking of its roads and in the following year placed center line marking and stripes on a large mileage of its main roads; some sections continuous, others only on the curves and over hills. It is felt that this is a feature of service to the traveling public and it is contemplated that we will have a continuation of the marking so that all main highways will have a center stripe the entire length of the road. The cost of this work has been absorbed in the regular maintenance as it was not provided for in the Budget at that time.



Sheet Asphalt, Road 4.

STATE ROAD DEPARTMENT OF FLORIDA
BUDGET OF MAINTENANCE FOR YEAR 1929.

Division	Roads, No. of Miles	Bridges, No. of Miles	Cost of Regular Maint.	Average Cost Per Mile	Bridge Operation and Maint.	Recommend- ed Better- ments	Total for Division
1st Div...	955.00	3.16	\$ 382,175.00	\$ 400.18	\$ 11,340.00	\$ 88,500.00	\$ 482,015.00
2nd Div...	604.26	1.23	190,689.68	315.58	23,079.84	62,905.00	276,674.52
3rd Div...	270.54	1.50	64,467.57	238.29			64,467.57
4th Div...	674.17	.48	228,090.00	338.33	4,800.00	71,000.00	303,890.00
5th Div...	364.93	.66	134,062.50	367.36	3,360.00	46,019.90	183,442.40
6th Div...	138.60	.16	59,767.50	431.22	5,250.00	1,250.00	66,267.50
7th Div...	323.81	....	123,417.00	381.14		131,803.00	255,220.00
Total..	3,331.31	7.19	\$1,182,669.25	.....	\$ 47,829.84	\$ 401,477.90	\$1,631,976.00

TYPES OF ROADS UNDER MAINTENANCE

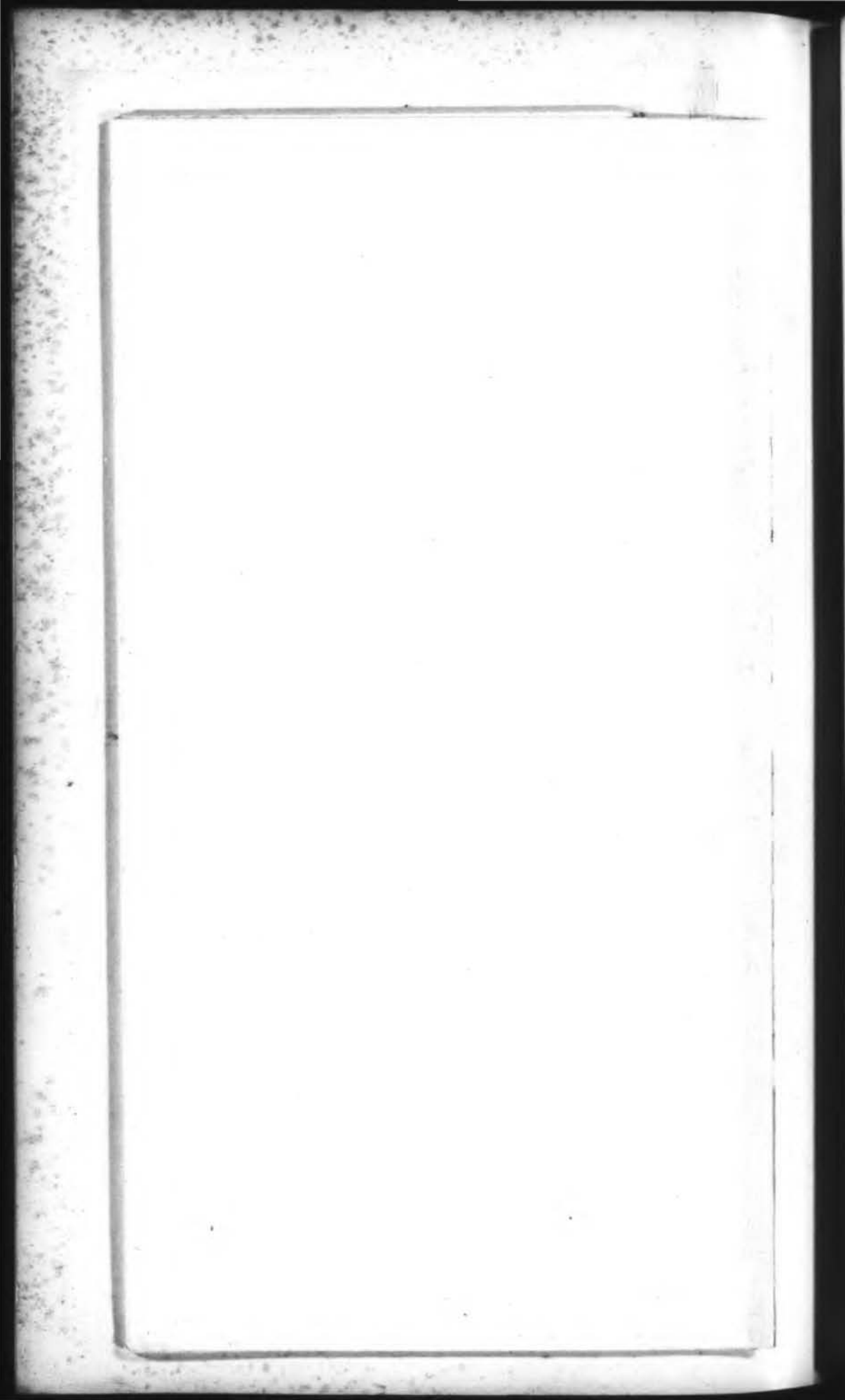
Concrete 364.80 Miles	Sheet Asphalt 179.17 Miles	Bit. Concrete 39.47 Miles	Bit. Macadam 95.39 Miles	Brick 68.11 Miles	Asphalt Block 18.23 Miles
Rock Base S. T. 1,384.88 Miles	Sand Clay S. T. 246.30 Miles.	Brick 9' Surf. Treat. 45.41 Miles.	Concrete Rock Sh'lds S. T. 16.00 Miles.		
Asphalt Rock Shoulders S. T. 28.50 Miles.	Shell 13.70 Miles.	Sand Clay 138.70 Miles.	Marl 27.28 Miles.	Grade 625.87 Miles	Shell 15.00 Miles.
Rock Base Not Treated. 24.50 Miles.	Total: 3,331.31 Miles.				
Number Miles State Construction. 2,861.17 Miles.	Number Miles County Construction. 470.14 Miles.				



Sheet Asphalt, Road 4.

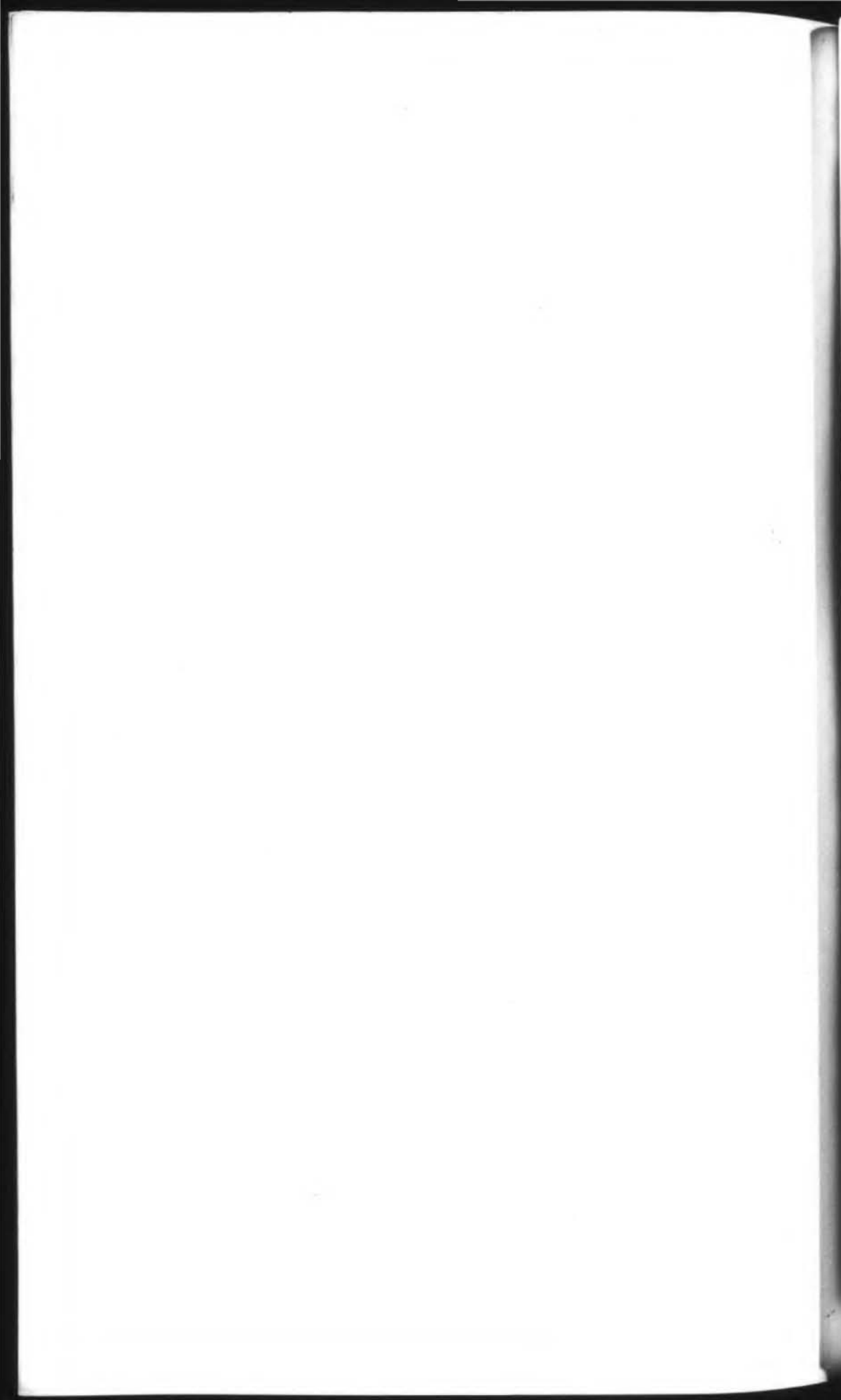
STATE ROAD DEPARTMENT OF FLORIDA
SECTIONS OF ROADS UNDER MAINTENANCE 1927

Road No.	County	From	To	Length Miles	Type	Roadbed	Surfacing	Surface Treatment	Structures	Storm	Miscellaneous	Engineering	Total
1	Escambia	Perdido River—Pensacola		15.9	Concrete	\$ 16,140.50	\$ 381.48			\$ 877.11	\$ 2,075.24	\$ 360.03	19,894.36
1	Escambia	Santa Rosa	Blackwater—Milton	14.2	Concrete Treated Sand Clay	3,782.19	7,292.88	14,509.49	573.33	395.85	20,592.37		47,236.11
1	Escambia	Santa Rosa	Blackwater—Okaloosa County Line	16.0	Concrete, Brick, Surf. Treat. S.C.	3,326.75	5,367.47	21,515.91	158.76		213.68		30,582.57
1	Okaloosa	Crestview—Santa Rosa County Line		14.5	Surf. Treatment Sand Clay	6,154.92	7,852.02	26,220.01	31.18		14.80	188.17	40,461.10
1	Okaloosa	Crestview—Walton County Line		11.2	Surf. Treatment Sand Clay	4,300.66	6,709.04	27,560.54	5.35		8.10		38,583.69
1	Walton	Bearhead—Okaloosa County Line		11.3	Surface Treated Sand Clay	3,118.37	4,492.99	20,801.79			98.55		28,511.61
1	Walton	Argyle—Bearhead		10.0	Surface Treated Sand Clay	3,083.67	9,892.04	20,005.48	196.80		43.96		33,654.88
1	Holmes	Argyle—Ponce de Leon		8.7	Sand Clay	4,390.71	6,024.26	20,005.48			56.20		10,749.99
1	Washington-Holmes	Ponce de Leon—Caryville		9.9	Surface Treated Sand Clay	5,768.34	12,710.97	18,836.88	278.82		1,168.54	85.30	39,348.11
1	Washington-Holmes	Caryville—Chipley		10.7	Surface Treated Sand Clay	7,999.69	15,164.92	41,487.94	6.03		185.38		64,843.66
1	Jackson—Washington	Marianna—Chipley		11.2	Surface Treated Sand Clay	7,487.95	6,230.89	34,364.57	15.90		93.80		48,193.11
1	Jackson	Marianna—Cypress		11.2	Surface Treated Sand Clay	3,720.60	12,808.71	22,692.77	197.08		106.14		39,675.30
1	Jackson	Cypress—Chattahoochee		11.0	Surface Treated Sand Clay	2,441.87	8,388.56	27,061.90			135.40		38,017.67
1	Jackson-Gadsden	Victory Bridge		1.0	Concrete & Creosoted Timber				2,161.16		12.84		2,174.00
1	Gadsden	Chattahoochee—Quincy		20.2	Sand Clay	4,850.43	10,385.06		24.28		64.47		15,333.24
1	Gadsden	Quincy—Ochlocknee River		16.7	Sand Clay	3,506.06	4,371.12		187.41		251.64		8,316.23
1	Leon	Ochlocknee River—Tallahassee		10.0	Concrete	3,154.12	7,028.38		110.75		19.15	17.33	10,329.73
1	Leon	Tallahassee—13 Miles East		13.0	Concrete	3,931.85	20,187.02		521.99		1,264.26		27,905.12
1	Leon-Jefferson	Monticello—13 Miles West		13.0	Concrete	4,911.23	2,038.57		94.55		212.90	12.84	7,290.09
1	Jefferson	Monticello—Aucilla River		9.1	Concrete	5,954.21	1,157.55		26.72		181.57		7,320.05
1	Madison	Greenville—Aucilla River		5.60	Graded	3,698.62	549.70		39.60		2,630.38		6,924.30
1	Madison	Madison—Greenville		14.73	Rock Base Surface Treated	7,114.81	807.20		100.70		3,776.93	41.25	11,709.99
1	Madison	Madison—Suwannee River		15.64	Concrete	9,197.49	934.67		99.36		1,784.89	8.70	14,925.95
1	Suwannee	Live Oak—Suwannee River		12.14	Concrete	5,086.61	422.18				1,115.07		7,623.86
1	Suwannee	Live Oak—Wellborn		13.47	Rock Base Surface Treated	5,843.86	1,432.32				1,291.98		8,568.16
1	Columbia	Lake City—Wellborn		9.15	Rock Base Surface Treated	6,414.36	510.89				1,291.98	17.44	8,224.07
1	Columbia	Lake City—Columbia County Line		9.88	Bituminous Macadam	7,568.18	2,746.35				878.26		11,594.55
1	Columbia	Columbia County Line—Project No. 11		12.00									12.00
1	Baker	Baldwin—Glen St. Mary		15.39	Concrete	8,248.87					413.52	31.88	8,694.27
1	Baker	Baldwin—Macclenny		5.68	Concrete	2,465.36	39.80				448.60	60.00	3,013.76
1	Duval	Enterprise—Baldwin		12.80	Concrete	3,520.96	67.15				651.64		4,239.75
1	Duval	Jacksonville—Enterprise		3.5	Concrete	2,539.71	15.00				765.00		3,319.71
2	Hamilton	Jasper—Jennings		13.0	Rock Base Surface Treated	10,116.20	232.79		326.50		223.73	24.20	10,923.42
2	Hamilton	Genoa—Jasper		11.86	Rock Base Surface Treated	9,646.19	859.21		99.90		1,246.57	1.03	11,852.90
2	Hamilton	Genoa—White Springs		8.20	Rock Base Surface Treated	1,138.77	475.89				303.00	0.50	1,918.16
2	Columbia	Lake City—White Springs		11.01	Concrete	3,893.38	6.00				902.57		4,801.95
2	Columbia	Lake City—Project No. 505		12.36	Concrete	1,671.49	2,390.50				981.33		5,043.32
2	Columbia	Suwannee River Bridge		0.062	Steel				76.60			16.91	83.60
2	Columbia	Columbia Springs—Project No. 27		11.05	Rock Base Surface Treated	1,563.43	2,284.75		61.08		580.00		4,489.26
2	Alachua	Columbia County Line—Burnett's Lake		11.7	Bituminous Macadam	2,286.70	64.10				159.73		2,510.53
2	Alachua	Burnett's Lake—Gainesville		12.8	Bituminous Macadam	3,784.71	24.52				253.57		4,062.80
2	Alachua	Gainesville—Marion County Line		14.2	Sheet Asphalt	7,227.34	15,980.75		8.03		291.42		23,507.54
2	Marion	Alachua—Lowell		10.9	Rock Base Surface Treated	3,851.46	554.81				48.85		4,435.08
2	Marion	Lowell—Ocala		10.4	Sheet Asphalt	4,169.25	34.35		30.27		16.16		4,241.03
2	Marion	Ocala—Bellevue		9.1	Rock Base Surface Treated	3,507.12	1,087.25				37.13		4,631.50
2	Marion	Bellevue—Lake County Line		15.6	Rock Base Surface Treated	3,089.84	706.16						3,796.00
2	Lake	Marion County Line—Leesburg		10.7	Bituminous Concrete	3,937.80					85.17		4,022.97
2	Lake	Leesburg—Lake Park—Tavares		9.0	Concrete	2,435.05					24.85		2,459.90
2	Orange	Mt. Dora—Plymouth		8.6	Sheet Asphalt	2,107.13					497.75		2,595.27
2	Orange	Lockhart—Plymouth		8.0	Surface Treatment over Brick	5,811.31	1,174.79				61.08		7,047.18
2	Orange	Lockhart—Orlando City Limits		5.0	Surface Treatment over Brick	3,124.45	2,201.77		228.20		326.20		5,880.62
2	Orange	Orlando City Limits—Osceola County Line		14.0	Surface Treatment over Brick	9,239.82	22,769.50		2,402.77		813.90		35,223.99
2	Osceola	Kissimmee City Limits—Orange County Line		2.6	Surface Treatment over Brick	1,538.77	1,572.27				20.00		3,131.04
2	Osceola	Campbell—Kissimmee		4.7	Surface Treatment over Brick	3,950.78	593.10		31.70		40.00	515.35	14,135.85
2	Osceola	Campbell—Polk County Line		4.7	Surface Treatment over Brick	4,729.24	6,998.29		1,835.97		40.00		12,552.50
2	Hardee	Wauchula—Peace River		7.9	Brick	8,246.06	13,301.92		1,880.66		33,559.94	43.16	57,031.74
2	Hardee	Peace River—DeSoto County Line		12.0	Rock Base Surface Treated	14,439.00	13,579.74		4,367.79		88,762.61	525.00	121,674.14
2	Lake	Helena Run—Groveland		15.0	Sheet Asphalt	2,790.24			16.14		23.75		1,567.36
2	Lake	Leesburg—Helena Run		15.32	Graded	1,507.36							2,034.71
2	Lake	Groveland—Polk County Line		15.23	Graded	217.80							217.80
2	Lake	Bowling Green—Wauchula		4.5	Surface Treatment	6,213.55	12,252.25		400.94				18,866.74
2	Hardee	Zolfo Springs—Brownsville		14.19	Rock Base Surface Treated	7,641.74	194.12		322.40		637.16	45.97	8,841.39
2	DeSoto	Aradina—Hardee County Line		9.00	Rock Base Surface Treated	1,291.47	1,218.35		341.95		17.54	30.48	2,680.87
2	DeSoto	Aradina—Carlstrom Field		9.48	Concrete and Brick	3,021.70	35,417.15				52,076.01	1,627.10	102,365.25
2	DeSoto	Carlstrom Field—Charlotte County Line		7.1	Marl	2,718.19	163.42				7.78	307.16	12,508.41
2	Charlotte	DeSoto County Line—Lee County Line		20.18	Marl	3,234.57	10,713.51		403.30		22.26	65.74	14,439.38
3	Nassau	Bridge		0.106	Bridge						15.00		15.00
3	Nassau	St. Mary's River Bridge		11.62	Concrete and Steel	41.85	62.30		1,536.37				1,640.72
3	Nassau	St. Mary's River—Yulee		11.62	Concrete	10,349.93	190.67				399.07	352.50	11,772.26
3	Nassau	Project No. 46—Duval County Line		4.52	Rock Base Surface Treated	8,581.63	975.42		15,893.72		47,741.04	2,404.29	75,596.10
3	Clay	Orange Park—Green Cove Springs		25.00	Rock Base Surface Treated	6,301.05	872.21		2,895.99		273.63		10,342.88
3	Clay	Green Cove Springs—Putnam County Line		10.52	Rock Base Surface Treated	4,663.45	1,200.65		238.50		283.00		6,385.60
3	Putnam	Palatka—Clay County Line		10.82	Rock Base Surface Treated	3,847.73	206.45		13.58		75.22		4,130.98
3	Putnam	Rice Creek—Palatka		10.3	Rock Base Surface Treated	3,135.45	676.73						3,812.18
3	Putnam	Sand Mateo—Dunn's Creek		6.3	Rock Base Surface Treated	2,205.50	251.56		575.42		195.44		3,572.92
3	Putnam	Sisco—Dunn's Creek		5.46	Rock Base Surface Treated	1,603.64	2,708.21		575.41		8.50		4,895.76
3	Putnam	Sisco—Volusia County Line		12.2	Bituminous Macadam	3,363.65	337.65				34.06		3,735.36
3	Volusia	Through Crescent City		2.3	Sheet Asphalt	621.13							621.13
3	Volusia	Putnam County Line—DeLeon Springs		18.9	Rock Base Surface Treated	5,223.40	3,113.58		31.00		409.21		8,776.59
3	Volusia	DeLeon Springs—DeLand		9.9	Rock Base Surface Treated	3,443.93	793.67						4,239.60
3	Volusia	DeLand—Monroe Bridge		9.9	Rock Base Surface Treated	4,564.12	8.15						4,572.27
3	Volusia	Monroe Bridge							1,998.17		5,268.74	100.79	7,367.70
3	Seminole	Maitland—Lake Monroe		17.2	Brick	24,227.65	5,063.16	37,891.48	1,638.42		44,617.75	774.07	114,182.53
4	Nassau	Project No. 32—Georgia State Line		12.41	Rock Base Surface Treated	5,064.75	402.34		178.20		28.25	154.73	5,828.27
4	Nassau	Duval County Line—10 Miles North		10.0	Bituminous Macadam	4,435.77	57.56		8.80		43.16		4,545.29
4	St. Johns	Duval County Line—St. Augustine		14.88	Concrete	2,596.83					2.90		2,599.73
4	St. Johns	Duval County Line—Flagler County Line		45.0	Brick & Rock Surface Treated	2,097.40	15,905.68		871.49			337.50	19,212.07
4	St. Johns	St. Augustine—Palmer's Creek		15.39	Rock Base Surface Treated	3,783.25	706.04		72.14				4,569.63
4	Flagler	Palmer's Creek—Bunnell		13.8	Rock Base Surface Treated	83.97	118.57						202.54
4	Flagler	Bunnell—Reilly's Asphalt Road		9.0	Rock Base Surface Treated	4,011.12	1,766.17		22.96				5,800.25
4	Flagler	St. Johns County Line—Bunnell		13.0	Brick & Rock Surface Treated	444.17	4,667.50						5,111.67
4	Volusia	Allandale—New Smyrna		7.32	Surface Treatment	3,570.46	254.68				8.70	174.23	4,008.07
4	Volusia	Brevard County Line—New Smyrna		16.74	Rock Base Surface Treated	1,948.94	89.04				110.22		2,149.20
4	Brevard	Titusville—Volusia County Line		13.6	Rock Base Surface Treated	5,171.65	157.10				120.22	230.13	5,679.10
4	Brevard	Project No. 546—Titusville		6.72	Rock Base Surface Treated	2,519.21	428.93				202.88	155.08	3,306.10
4	Brevard	Project No. 608—4.51 Miles North		4.51	Rock Base Surface Treated	2,199.17	213.42				4.20	148.21	2,560.50
4	Brevard	Sharpes—Bonaventure		13.43	Rock Base Surface Treated	1,062.03							1,062.03
4	Brevard	Project No. 608—Bonaventure		3.8	Rock Base Surface Treated	2,628.21	454.47				62.75	175.79	3,207.22
4	Brevard	Melbourne—Project No. 554		9.25	Rock Base Surface Treated	2,851.13	33.85		30.92		80.51	139.14	3,135.55
4	Brevard	Melbourne—Sebastian River		16.17	Rock Base Surface Treated	5,897.63	812.98				316.70	206.59	7,233.90
4	Brevard-Indian River	Sebastian River Bridge Approaches			Rock Base Surface Treated	383.69	6.00				6.00	10.72	403.41
4	Indian River	Vero Beach—Wabasso		7.72	Bituminous								





Plain Cement Concrete, Federal Aid, Road 10.

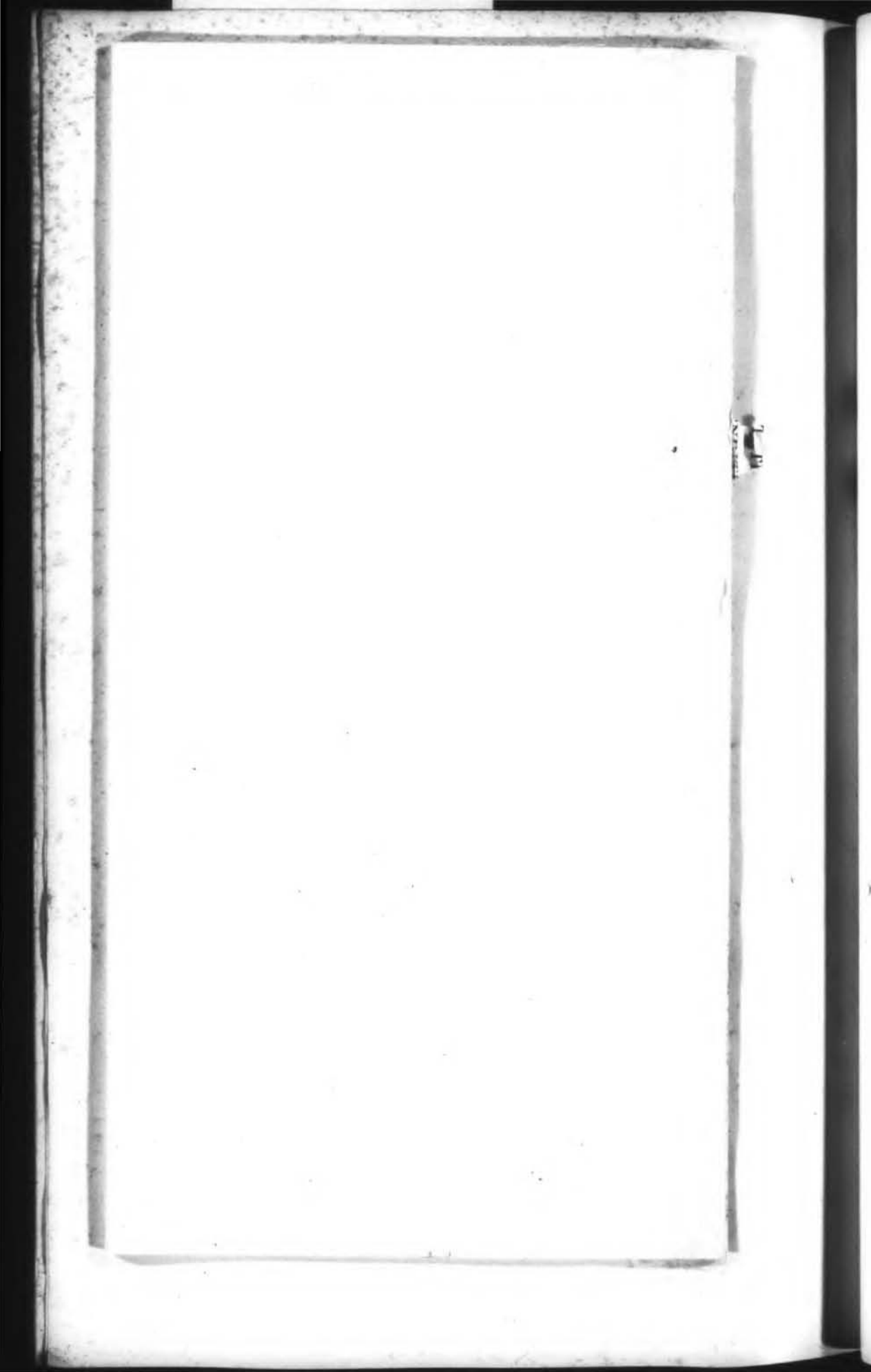




Rock Base Surface Treated, Road 4.

STATE ROAD DEPARTMENT OF FLORIDA
SECTIONS OF ROADS UNDER MAINTENANCE 1928

County	From	To	Length Miles	Type	Roadbed	Surfacing	Surface Treatment	Structures	Storm	Miscellaneous	Engineering	Total
Alachua	Perdido River—Pensacola		16.0	Concrete	6,134.51	218.92		122.37	474.05	185.50		7,135.35
Alachua	Pensacola—Escambia Bridge		10.1	Graded	4,808.36	1,112.35		73.00				6,035.99
Alachua	Escambia River—Milton		0.4	Crescoted Lumber and Steel				2,372.57		107.99		2,480.56
Alachua	Blackwater River—Milton		11.95	Surface Treatment	3,887.24	5,406.64		104.60		245.22		9,643.70
Alachua	Blackwater River—Okaloosa County Line		16.0	Concrete, Brick, Surf. Treated	1,706.98							1,706.98
Alachua	Crestview—Santa Rosa County Line		14.5	Sand Clay Surface Treated	5,222.63	2,273.25	58.52		13,200.31	74.10		5,660.00
Alachua	Crestview—Walton County Line		11.2	Sand Clay Surface Treated	5,246.90	2,401.10		12.24	117.90	673.31		21,827.02
Alachua	Head—Okaloosa County Line		11.3	Sand Clay Surface Treated	4,100.32	2,259.88		7.50		164.31		7,984.45
Alachua	Head—Argyle		10.0	Sand Clay Surface Treated	5,755.01	3,726.43		14.21		2,610.38		12,112.03
Holmes	Argyle—Ponce de Leon		8.7	Sand Clay	4,820.73	10,080.80	20,348.66		4,543.98	4,640.23		45,355.20
Washington-Holmes	Ponce de Leon—Caryville		9.9	Sand Clay Surface Treated	5,635.05	2,270.89	2,420.60	61.39	26,896.69	377.75		37,480.17
Washington-Holmes	Caryville—Chipley		16.7	Sand Clay Surface Treated	7,292.01	2,554.09		8.00	2,078.39	18.72		12,910.24
Jackson-Washington	Marianna—Chipley		17.3	Sand Clay Surface Treated	4,704.81	3,003.42	1,902.89			104.98		8,551.82
Jackson	Marianna—Cypress		11.2	Sand Clay Surface Treated	4,150.82	1,877.94		2.76	319.72	170.35		6,521.59
Jackson	Cypress—Victory Bridge		11.0	Sand Clay Surface Treated	4,064.38	1,354.64		253.52		270.97		6,125.91
Jackson-Gadsden	Victory Bridge		1.0	Concrete & Crescoted Timber		10.25	182.40			115.88		2,991.39
Gadsden	Victory Bridge—Quincy		21.0	Concrete	2,713.32	2,279.49		10.61	101.89	4,690.31		7,484.78
Gadsden	Quincy—Leon County Line		16.7	Concrete	4,247.21	7,394.55		404.17	8,700.69	364.24		13,812.81
Leon	Ochlocknee River—Tallahassee		11.0	Sand Clay	3,556.71	6,478.17		11.12	1,040.62	108.48		11,088.99
Leon	Tallahassee—13 Miles East		13.0	Concrete	4,429.67			67.44		91.90		4,589.01
Jefferson	Baum—Monticello		13.0	Surface Treated	3,429.90	734.55		3.50	673.04	0.90		4,841.59
Jefferson	Monticello—Aucilla River		9.1	Concrete	4,036.38	41.55		20.75	720.50	15.90		4,844.78
Madison	Aucilla River—Madison		20.39	Graded and Surface Treated	10,591.50	102.81		372.07		408.72	209.38	13,884.48
Madison-Suwannee	Live Oak—Lake City		28.45	Concrete	5,324.44	1,579.69		13,631.71	13,377.11	1,218.41	201.50	33,622.86
Madison-Suwannee	Madison—Live Oak		28.45	Concrete	4,164.52	5,183.19		1,381.67	20,054.34	740.15	270.01	11,512.51
Suwannee-Columbia	Lake City—Glen St. Mary		18.79	Bituminous Macadam & Concrete	5,365.70	150.00				313.64		6,022.30
Columbia and Baker	Glen St. Mary—2.32 Miles West of Baldwin		18.70	Concrete	3,813.24					291.68		4,104.92
Madison	2.32 Miles West of Baldwin—Jacksonville		18.67	Concrete	4,387.89	7.01				193.26		4,581.16
Hamilton	Georgia Line—3.33 Miles North of Genoa		22.53	Surface Treated	4,771.44	1,720.29		200.99		169.30	186.66	7,048.68
Hamilton-Columbia	3.33 Miles North of Genoa—Lake City		22.53	Concrete and Surface Treatment	4,431.75	284.46				207.16	231.04	5,154.41
Columbia	Lake City—Santa Fe River		21.15	Concrete and Surface Treatment	5,961.38	362.07				217.06	248.22	6,788.73
Alachua	Columbia County Line—Burnett's Lake		14.7	Bituminous Macadam	1,550.94	403.94				7.40		2,062.28
Alachua	Burnett's Lake—Gainesville		12.7	Bituminous Macadam	3,167.57	241.53				28.31		3,437.41
Alachua	Gainesville—Marion County Line		14.8	Sheet Asphalt	4,471.07	770.92		17.00		30.37		4,981.00
Marion	Alachua—Lowell		10.9	Surface Treatment	2,490.37	923.74		3.11		30.37		3,453.59
Marion	Lowell—Ocala		10.4	Sheet Asphalt	3,298.59	29.50		62.83		155.54		3,546.46
Marion	Ocala—Bellevue		9.1	Surface Treatment	2,635.41	257.06				90.60		2,983.07
Marion	Bellevue—Lake County Line		15.6	Surface Treatment	2,862.07	194.55				35.00		3,091.62
Lake	Marion County Line—Leesburg		10.7	Bituminous Concrete	3,029.04	8.00				9.00		3,038.04
Lake	Leesburg—Hanna Run		3.5	Sheet Asphalt	2,501.81					12.80		2,514.61
Lake	Hanna Run—Groveland		30.3	Sheet Asphalt	9,710.99			407.01		42.71		10,130.80
Lake	Silver Lake Forks—Tavares		9.0	Bituminous Concrete	2,097.55	30.00		135.78		42.71		2,306.04
Lake	Silver Lake Forks via Eustis and Mt. Dora		10.5	Surface Treatment	1,378.65	215.48		3.60				1,597.73
Orange	Plymouth—Mt. Dora		8.6	Rock Base Surface Treated	5,375.35	292.64						5,668.00
Orange	Orlando—Lockhart		5.0	Loose brick Line rock shoulders	2,865.02	1,845.00						4,710.02
Orange	Orlando—Ocala County Line		14.0	Loose brick Line rock shoulders	3,412.54	3,412.46				55.00		7,980.00
Osceola	Campbell—Polk County Line		4.7	Grouted brick, rock shoulders	732.29	4,647.88						5,380.17
Polk	Bartow—Fort Meade		10.0	Graded	1,880.91							1,880.91
Hardee	Bartow—Fort Meade		4.32	Bartow clay, Surface Treated	2,449.11	741.66						3,190.77
Hardee	Wauchula—Zolfo Springs		4.0	Grouted brick, rock shoulders	1,105.02	354.25		2,516.48				4,065.73
Hardee	Zolfo Springs—Brownsville		14.19	Rock Base Surface Treated	5,537.13	285.95		6.40		212.38	228.44	6,272.30
DeSoto	Aracadia—Brownsville		9.00	Rock Base Surface Treated	2,529.57	1,098.13		1,305.54		214.28	145.90	4,949.48
DeSoto	Aracadia—Dorr Field and Carlstrom Field		10.00	Concrete and Brick	9,288.11	432.00		2,631.18		175.20		12,526.49
DeSoto	Carlstrom Field—Charlotte County Line		7.10	Marl	1,542.56	3,680.45		164.91		118.90	114.44	5,705.27
Charlotte	DeSoto County Line—Lee County Line		20.18	Marl	2,171.17	6,910.30				332.22	325.08	9,003.68
Nassau	Georgia Line—Project No. 522		11.52	Concrete	3,757.69	1,365.12		1,767.82		191.48	181.68	7,265.79
Duval-Nassau	Project No. 46—Jacksonville		14.09	Asphalt	7,391.21	2,863.82				1,854.14	144.72	12,208.09
Clay-Duval	Jacksonville—Black Creek Bridge		13.54	Surface Treatment	5,030.44	446.34		1,756.64		121.54		7,355.00
Clay	Black Creek Bridge—Putnam County Line		13.55	Surface Treatment	5,187.49	234.91		1,095.58		120.27	139.43	6,777.68
Putnam	Palatka—Clay County Line		10.50	Surface Treatment	1,957.79	350.15				68.02		2,375.96
Putnam	Rice Creek—Palatka		3.00	Brick, Surface Treated Shoulders	910.42	181.82				10.26		1,102.50
Putnam	Through Crescent City		2.50	Sheet Asphalt	313.33	30.00				8.00		3,516.51
Volusia	Sisoua—Volusia County Line		12.40	Bituminous Concrete	2,822.82					84.67		2,907.49
Volusia	San Mateo—Dunn's Creek		6.30	Surface Treatment	1,167.09	147.63				4,600.55		1,399.39
Volusia	Monroe Bridge		9.90	Timber		3.58				44.00		2,101.52
Volusia	DeLand—Monroe Bridge		9.90	Surface Treatment	2,877.73	228.46				8.00		5,249.81
Volusia	DeLand—Monroe Bridge		9.90	Surface Treatment	2,877.73	228.46				8.00		5,249.81
Putnam	DeLand—Monroe Bridge		18.90	Surface Treatment	2,032.43	61.00		20.57		7.80		19,706.45
Seminole	Putnam County Line—DeLeon Springs		17.20	Bituminous Macadam	14,339.91	4,193.77				1,172.77		19,706.45
Dade	Broward County Line—Miami		11.76	Sheet Asphalt	3,799.29	24.42		631.16		1,077.49		5,532.36
Broward	Dade Canal—Fort Lauderdale		14.00	Penetration Surface						50.40		289.90
Palm Beach	West Palm Beach—Delray		8.40	Concrete	284.22					75.60		359.82
Palm Beach	East Canal Bridge			Concrete				1,546.67				1,546.67
Palm Beach	Jupiter River Bridge		0.1377	Concrete				1,582.83				1,582.83
Palm Beach	Jupiter—West Palm Beach		13.80	Surface Treatment	7,220.11	1,274.10				1,645.41	328.68	10,541.18
Martin-Palm Beach	Salerno—Jupiter		14.94	Concrete	3,639.80					8.60		3,793.74
St. Lucie-Martin	Salerno—White City		15.00	Sheet Asphalt and Concrete	3,901.28	1,323.23		16.36		666.70		6,053.97
St. Lucie	Indrio—White City		14.00	Concrete	4,498.26	283.53				1,009.10		5,928.80
Indian River-St. Lucie	Winter Beach—Indrio		14.00	Concrete, Bit. Mac., Surf. Tre't.	4,127.48	138.40		21.43		1,013.20		5,439.90
Brevard & Indian R.	Grant—Winter Beach		14.00	Concrete, Bit. Mac., Surf. Tre't.	4,127.48	138.40		57.99		1,013.20		5,439.90
Brevard	Eau Gallie—Grant		15.00	Concrete Surface Treatment	5,617.19	863.51				938.83		7,419.53
Brevard	Bonaventure—Eau Gallie		14.00	Concrete, Surface Treatment	4,277.82	357.86				2,132.90		5,813.99
Brevard	Indian River City—Bonaventure		14.00	Concrete, Surface Treatment	4,441.61	641.89				679.58		5,430.01
Brevard	Scottsboro—Indian River City		14.72	Surface Treatment	3,652.79	686.62				1,062.78		5,430.01
Volusia-Brevard	Scottsboro—Indian River City		14.72	Surface Treatment	3,652.79	686.62				1,062.78		5,430.01
Volusia	Allendale—Oak Hill		15.00	Surface Treatment	4,053.15	299.16		17.86		802.27	158.33	5,275.91
Flagler	Bunnell—E. Reilly's Asphalt Road		9.00	Surface Treatment	2,086.64	158.62				260.42		2,245.26
Flagler	Pellicier's Creek—Bunnell		13.8	Surface Treatment	6,501.15	882.21		12.56		130.75		7,643.78
St. Johns	St. Augustine—Pellicier's Creek		13.8	Surface Treatment	5,729.89	286.04				1,211.08		7,188.39
St. Johns	Duval County Line—St. Augustine		15.4	Concrete	4,129.79					126.33		4,256.12
Duval	Jacksonville—Bayard		18.0	Asphalt	19,872.42	1.50				159.48	2,437.13	22,470.53
Duval	Callahan—Jacksonville		16.6	Surface Treatment	7,252.30	127.14		23.65		626.52		8,848.49
Nassau-Duval	Callahan—Jacksonville		16.6	Surface Treatment	4,006.40	348.75			826.25	303.30	170.39	5,655.09
Nassau	Georgia Line—Callahan		10.6	Surface Treatment								
LaFayette	Taylor County Line—Brantford		3.00	Graded	12,666.30					265.80	307.92	13,240.02
Columbia-Suwannee	Brantford—Santa Fe River		20.81	Graded	7,754.20	55.15				184.29	213.49	8,207.13
Alachua	High Springs—Santa Fe River		1.50	Surface Treatment	580.26	59.91						640.17
Alachua	High Springs—Newberry District Line		10.9	Surface Treatment	2,971.28	28.25						2,999.53
Alachua	High Springs District Line—Archer District Line		10.9	Surface Treatment	3,049.13	83.32						3,132.45
Alachua	Archer District Line—Newberry to Levy Co. Line		10.9	Surface Treatment	3,524.27	395.71		25.64		58.62		4,004.24
Levy	Alachua County Line—Marion County Line		20.5	Surface Treatment	2,283.49	712.76				2,996.25		5,992.50
Marion	Dunnellon—Levy County Line		10.8	Surface Treatment	2,648.33	624.98				34.21		3,307.52
Citrus	Dunnellon—Holder		10.7	Surface Treatment	2,720.05	87.19		5.84		146.92		1,969.00
Citrus	Holder—Inverness		10.7	Surface Treatment	2,019.54	239.25		136.20		81.58		2,476.57
Citrus	Inverness—Floral City		6.6	Surface Treatment	1,055.06	134.16				14.74		1,203.96
Citrus	Floral City—Hernando County Line		6.0	Surface Treatment	1,061.19	74.73				23.35		1,159.27
Hernando	Citrus County Line—Brooksville		7.3	Rock Base Surface Treated	7,549.07	232.27		158.27		229.32		



STATE ROAD DEPARTMENT OF FLORIDA
EXPENDITURES BY COUNTIES AND FUNDS USED
IN CONSTRUCTION AND MAINTENANCE
OCTOBER, 1915—DECEMBER, 1927.

County	Federal Aid Construction	County Funds Construction	State Funds Construction	State Funds Maintenance	Total
Alachua.....	\$ 575,745.79	\$ 779,950.52	\$ 731,336.85	\$ 168,243.66	\$ 2,255,276.82
Baker.....	403,115.36	55,119.08	337,607.58	73,827.39	869,669.41
Bay.....	0.00	577,798.53	30,226.21	14,935.14	622,959.88
Bradford.....	0.00	100,000.00	145,982.02	28,699.14	274,631.16
Brevard.....	485,799.04	381,770.94	2,239,677.00	478,932.89	3,586,179.87
Broward.....	0.00	0.00	125,113.88	162,283.69	287,397.57
Calhoun.....	0.00	0.00	27,016.28	37,130.12	64,146.40
Charlotte.....	0.00	50,000.00	731,679.38	96,354.95	878,034.33
Citrus.....	0.00	0.00	545,693.87	28,793.15	574,487.00
Clay.....	0.00	100,000.00	987,566.77	118,399.57	1,205,966.38
Collier.....	0.00	0.00	1,048,129.45	826.45	1,048,955.90
Columbia.....	578,828.31	294,120.43	876,157.66	152,993.08	1,902,099.46
Dade.....	199,966.08	125,000.00	2,437,581.43	33,016.85	2,795,564.36
DeSoto.....	75,245.57	191,422.14	522,751.77	261,550.76	1,050,970.24
Dixie.....	0.00	0.00	392,809.00	2,083.52	391,892.52
Duval.....	259,766.44	0.00	466,540.78	81,804.22	808,111.44
Escambia.....	634,458.02	471,898.58	292,106.03	133,536.53	1,531,999.16
Flagler.....	128,422.46	34,474.89	340,960.25	220,385.66	724,243.26
Franklin.....	0.00	156,768.65	272,680.47	54,345.33	483,794.45
Gadsden.....	104,257.78	30,500.00	906,497.94	68,502.42	1,109,758.14
Glades.....	0.00	0.00	26,986.08	87,547.17	114,533.25
Gulf.....	0.00	0.00	580,370.59	90,559.28	670,929.87
Gilchrist.....	0.00	0.00	6,241.77	0.00	6,241.77
Hamilton.....	463,002.17	169,951.19	515,793.41	165,185.00	1,313,931.77
Hardee.....	0.00	0.00	357,132.65	254,206.90	611,339.55

STATE ROAD DEPARTMENT OF FLORIDA
EXPENDITURES BY COUNTIES AND FUNDS USED
IN CONSTRUCTION AND MAINTENANCE
OCTOBER, 1915—DECEMBER, 1927 (Continued)

County	Federal Aid Construction	County Funds Construction	State Funds Construction	State Funds Maintenance	Total
Hendry.....	\$ 0.00	\$ 0.00	\$ 3,729.03	\$ 69,202.61	\$ 72,931.64
Hernando.....	0.00	89,899.09	465,179.74	12,511.80	567,590.63
Highland.....	0.00	0.00	1,672,925.73	198,619.01	1,871,544.74
Hillsboro.....	74,340.39	0.00	932,797.81	43,508.09	1,050,646.29
Holmes.....	89,827.00	39,999.49	447,675.09	104,632.87	682,134.46
Indian River.....	56,709.94	0.00	994,106.58	61,674.90	1,112,491.42
Jackson.....	104,257.78	30,001.00	1,166,002.54	273,446.40	1,573,707.72
Jefferson.....	56,796.75	376,545.11	415,270.34	41,123.84	889,736.04
LaFayette.....	0.00	0.00	73,335.93	20,415.86	93,751.79
Lake.....	255,238.32	834,725.00	694,692.37	46,664.84	1,831,320.53
Lee.....	0.00	0.00	351,338.88	13,110.93	364,449.81
Leon.....	54,529.11	466,531.05	798,712.41	33,356.16	1,353,128.73
Levy.....	0.00	809,134.88	638,168.88	129,343.50	1,576,647.26
Liberty.....	0.00	0.00	27,248.66	18,426.13	45,674.79
Madison.....	40,678.06	310,376.15	1,278,565.03	143,101.75	1,772,720.99
Manatee.....	103,867.12	0.00	1,319,095.22	75,081.96	1,498,044.30
Marion.....	148,154.25	55,216.49	462,984.27	111,131.23	777,486.24
Martin.....	0.00	0.00	683,059.57	1,307.50	684,367.07
Monroe.....	0.00	0.00	6,195.53	13,925.89	20,121.42
Nassau.....	475,390.17	0.00	1,198,232.93	194,313.09	1,867,936.19
Okaloosa.....	16,938.10	9,989.40	636,618.92	154,445.68	817,992.10
Okeechobee.....	0.00	0.00	336,668.90	39,031.74	375,700.64
Orange.....	134,518.71	0.00	284,945.39	314,065.29	733,529.39
Osceola.....	20,000.00	188,087.95	0.00	129,542.65	337,630.60
Palm Beach.....	0.00	0.00	941,105.26	38,427.23	979,532.49

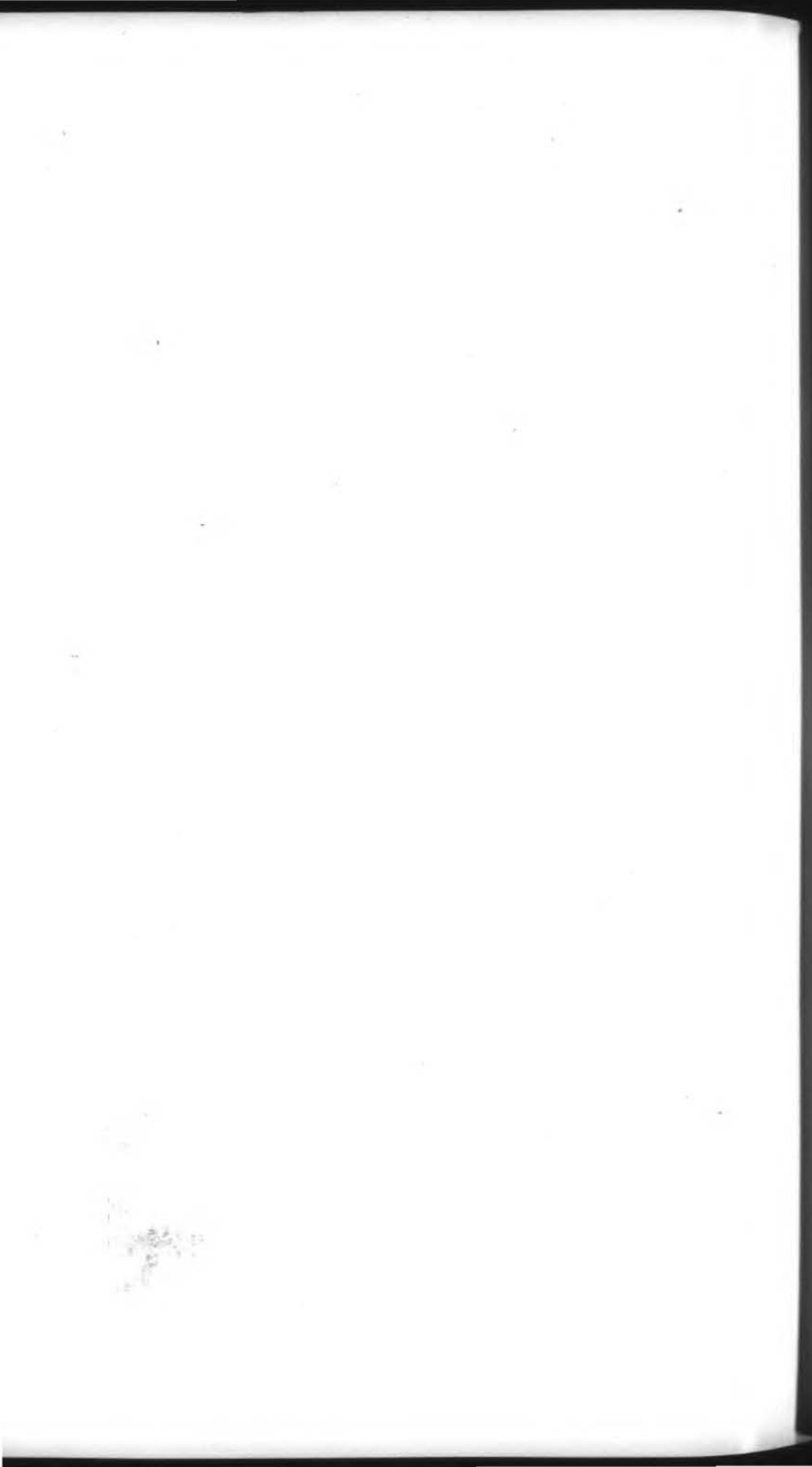
STATE ROAD DEPARTMENT OF FLORIDA
EXPENDITURES BY COUNTIES AND FUNDS USED
IN CONSTRUCTION AND MAINTENANCE

OCTOBER, 1915 - DECEMBER, 1927 (Continued)

County	Federal Aid Construction	County Funds Construction	State Funds Construction	State Funds Maintenance	Total
Hendry	\$ 0.00	\$ 0.00	\$ 3,729.03	\$ 69,202.61	72,931.64
Hernando	0.00	89,899.09	465,179.74	12,511.80	567,590.63
Highland	0.00	0.00	1,672,925.73	198,619.01	1,871,544.74
Hillsboro	74,340.39	0.00	932,797.81	43,508.09	1,050,646.29
Holmes	89,827.00	39,999.49	447,675.09	104,632.87	682,134.46
Indian River	56,709.94	0.00	994,106.58	61,674.90	1,112,491.42
Jackson	104,257.78	30,001.00	1,166,002.54	273,446.40	1,573,707.72
Jefferson	56,796.75	376,545.11	415,270.34	41,123.84	889,736.04
LaFayette	0.00	0.00	73,335.93	20,415.86	93,751.79
Lake	255,238.32	834,725.00	694,692.37	46,664.84	1,831,320.53
Lee	0.00	0.00	351,338.88	13,110.93	364,449.81
Leon	54,529.11	466,531.05	798,712.41	33,356.16	1,353,128.73
Levy	0.00	809,134.88	638,168.88	129,343.50	1,576,647.26
Liberty	0.00	0.00	27,248.66	18,426.13	45,674.79
Madison	40,678.06	310,376.15	1,278,565.03	143,101.75	1,772,720.99
Manatee	103,867.12	0.00	1,319,095.22	75,081.96	1,498,044.30
Marion	148,154.25	55,216.49	462,984.27	111,131.23	777,486.24
Martin	0.00	0.00	683,059.57	1,307.50	684,367.07
Monroe	0.00	0.00	6,195.53	13,925.89	20,121.42
Nassau	475,390.17	0.00	1,198,232.93	194,313.09	1,867,936.19
Okaloosa	16,938.10	9,989.40	636,618.92	154,445.68	817,992.10
Okeechobee	0.00	0.00	336,668.90	39,031.74	375,700.64
Orange	134,518.71	0.00	284,945.39	314,065.29	733,529.39
Osceola	20,000.00	188,087.95	0.00	129,542.65	337,630.60
Palm Beach	0.00	0.00	941,105.26	38,427.23	979,532.49



Rock Base Surface Treated, Road 3.



STATE ROAD DEPARTMENT OF FLORIDA
EXPENDITURES BY COUNTIES AND FUNDS USED
IN CONSTRUCTION AND MAINTENANCE
OCTOBER, 1915—DECEMBER, 1927 (Continued)

County	Federal Aid Construction	County Funds Construction	State Funds Construction	State Funds Maintenance	Total
Pasco.....	\$ 0.00	\$ 0.00	\$ 536,247.18	\$ 60,783.61	\$ 597,030.79
Pinellas.....	0.00	0.00	417,938.05	88,244.62	506,182.67
Polk.....	0.00	0.00	463,819.29	466,301.00	930,120.29
Putnam.....	470,488.61	724,793.42	732,663.88	71,770.90	1,999,716.81
St. Johns.....	453,975.84	0.00	851,997.82	518,384.53	1,824,358.19
St. Lucie.....	277,200.69	381,000.00	573,279.55	17,726.41	1,249,206.65
Santa Rosa.....	385,531.81	114,546.27	500,938.80	164,800.57	1,165,817.45
Sarasota.....	0.00	100,000.00	1,012,051.92	33,554.89	1,145,606.81
Seminole.....	0.00	0.00	474,834.56	177,151.96	651,986.52
Sumter.....	0.00	0.00	115.61	2,704.68	2,820.29
Suwannee.....	27,763.45	200,000.00	731,084.69	97,505.25	1,056,353.39
Taylor.....	0.00	0.00	177,403.93	20,463.94	197,867.87
Union.....	0.00	65,426.20	0.00	0.00	65,426.20
Volusia.....	0.00	493,067.83	1,159,687.13	148,081.99	1,800,836.95
Wakulla.....	0.00	9,700.00	259,739.79	34,430.52	303,870.31
Walton.....	0.00	0.00	162,007.41	86,666.98	248,674.39
Washington.....	77,064.48	25,000.00	310,757.82	96,164.29	508,986.59
Total.....	\$7,231,877.60	\$ 8,842,814.28	\$39,137,811.57	\$ 7,179,289.96	\$62,391,793.41

STATE ROAD DEPARTMENT OF FLORIDA
EXPENDITURES BY COUNTIES AND FUNDS USED
IN CONSTRUCTION AND MAINTENANCE

OCTOBER, 1915—DECEMBER 31, 1928

County	Federal Aid Construction	County Funds Construction	State Funds Construction	State Funds Maintenance	Total for Each County
Alachua.....	\$ 663,007.90	\$ 869,026.40	\$ 724,962.01	\$ 220,478.55	\$ 2,477,474.86
Baker.....	477,312.77	55,119.08	263,409.02	44,641.08	840,481.95
Bay.....	0.00	1,951,073.72	434,014.87	91,132.43	2,476,221.02
Bradford.....	0.00	125,000.00	209,119.72	41,311.03	375,430.75
Brevard.....	516,193.70	385,060.36	2,273,477.77	401,179.75	3,575,911.58
Broward.....	0.00	0.00	307,800.41	162,737.15	470,537.56
Calhoun.....	0.00	0.00	65,505.01	49,129.32	114,634.33
Charlotte.....	0.00	50,000.00	733,662.99	115,561.43	899,224.42
Citrus.....	0.00	0.00	540,075.25	37,251.03	577,326.28
Clay.....	0.00	100,000.00	1,230,128.47	135,731.46	1,465,859.93
Collier.....	0.00	0.00	1,848,545.09	152,017.48	2,000,562.57
Columbia.....	577,122.00	294,120.43	887,772.37	182,343.49	1,941,358.29
Dade.....	259,675.85	125,000.00	3,654,881.17	102,035.65	4,141,592.67
DeSoto.....	75,245.57	191,422.14	527,077.20	293,383.67	1,087,128.58
Dixie.....	0.00	0.00	439,664.36	5,385.91	445,050.27
Duval.....	220,570.86	0.00	506,639.24	165,002.84	892,212.94
Escambia.....	657,948.34	510,238.58	393,130.36	179,886.56	1,741,203.84
Flagler.....	216,883.12	34,474.89	370,860.90	229,533.34	851,752.25
Franklin.....	0.00	156,768.65	575,135.43	92,774.18	824,678.26
Gadsden.....	297,511.07	283,710.00	870,718.82	109,508.30	1,561,448.19
Gilchrist.....	0.00	0.00	77,907.99	32.29	77,940.28
Glades.....	0.00	0.00	215,655.50	**87,877.60	303,533.10
Gulf.....	0.00	0.00	936,675.71	161,988.66	1,098,664.37
Hamilton.....	476,498.01	169,951.19	535,515.82	169,322.92	1,351,287.94
Hardee.....	0.00	0.00	360,106.35	263,752.87	623,859.22

Hernando.....	0.00	89,899.09	466,127.03	86,494.15	642,520.27
Hendry.....	0.00	0.00	7,145.39	12,511.80	19,657.19
Highlands.....	0.00	0.00	1,624,677.90	313,748.23	1,938,426.13
Hillsborough.....	74,340.39	0.00	1,169,308.47	59,830.84	1,303,479.70
Holmes.....	89,827.00	125,480.99	463,076.60	170,066.81	848,451.40
Indian River.....	0.00	0.00	1,063,846.83	43,344.15	1,107,190.98
Jackson.....	104,257.78	30,001.00	1,222,798.53	370,869.21	1,727,926.52
Jefferson.....	61,375.50	423,288.86	602,591.32	83,734.77	1,170,990.45
LaFayette.....	0.00	100,000.00	182,659.11	28,961.24	311,620.35
Lake.....	316,442.69	1,017,735.00	933,495.78	66,192.02	2,333,865.49
Lee.....	0.00	0.00	371,846.75	21,593.33	393,440.08
Leon.....	111,702.95	616,531.05	912,990.13	167,927.10	1,809,151.23
Levy.....	0.00	809,134.88	1,016,038.22	58,429.12	1,883,602.22
Liberty.....	0.00	0.00	30,979.07	25,028.10	56,007.17
Madison.....	46,996.08	310,376.15	1,431,230.68	166,222.33	1,954,825.24
Manatee.....	103,867.12	0.00	1,302,924.04	106,482.42	1,513,273.58
Marion.....	171,195.12	55,216.49	444,643.95	134,028.49	805,084.05
Martin.....	0.00	0.00	1,065,870.88	8,334.47	1,074,205.35
Monroe.....	0.00	0.00	7,822.09	13,925.89	21,747.98
Nassau.....	462,727.54	0.00	1,254,236.84	155,668.40	1,872,632.78
Okaloosa.....	26,247.84	403,042.62	423,638.33	219,374.38	1,072,303.17
Okeechobee.....	0.00	0.00	366,172.43	27,415.06	393,587.49
Orange.....	134,518.71	0.00	339,529.35	334,766.26	808,814.32
Osceola.....	90,829.05	290,783.38	77,707.61	255,829.81	715,149.85
Palm Beach.....	0.00	27,000.00	1,538,500.58	62,495.04	1,627,995.62

STATE ROAD DEPARTMENT OF FLORIDA
EXPENDITURES BY COUNTIES AND FUNDS USED
IN CONSTRUCTION AND MAINTENANCE
OCTOBER, 1915—DECEMBER 31, 1928 (Continued)

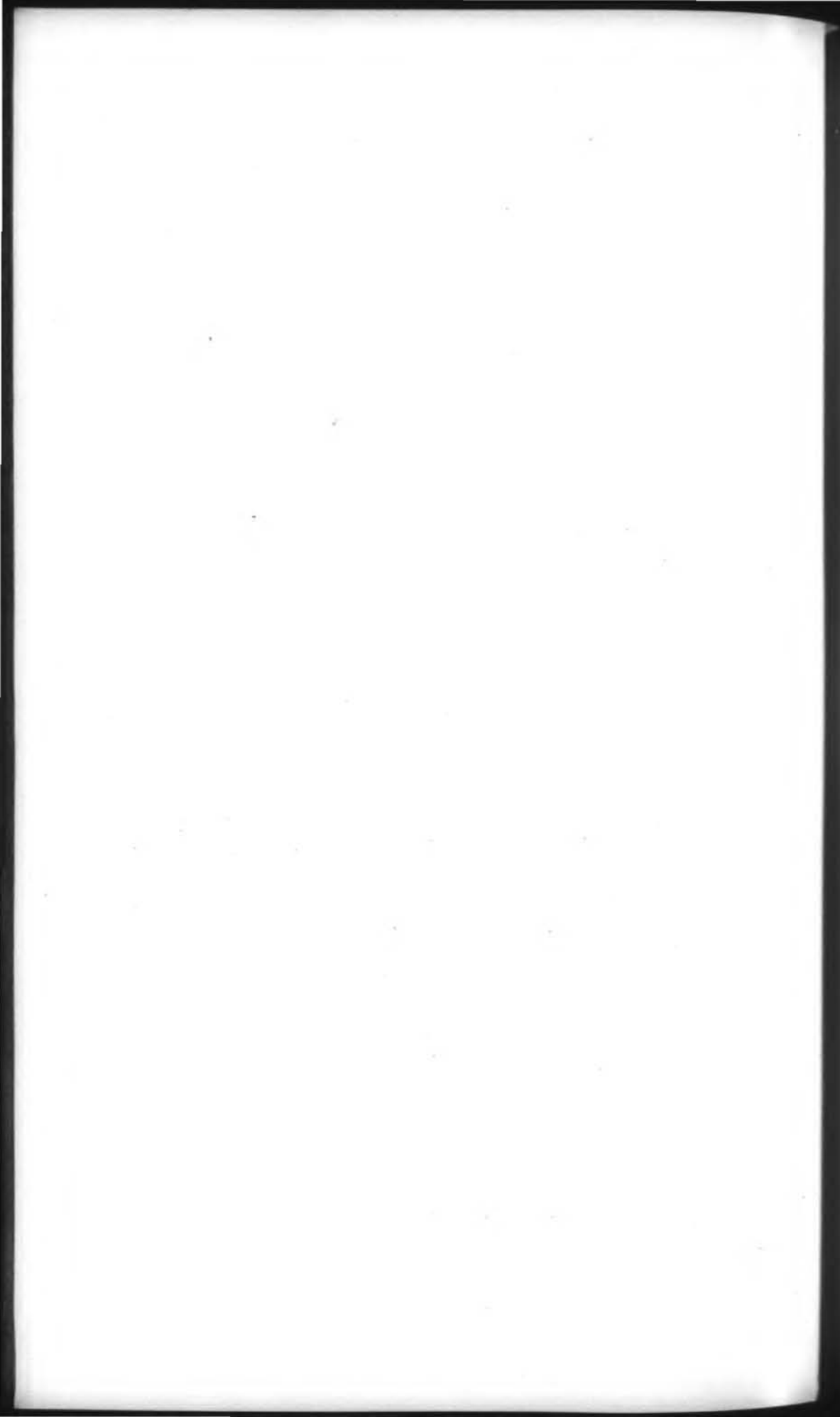
County	Federal Aid Construction	County Funds Construction	State Funds Construction	State Funds Maintenance	Total for Each County
Pasco.....	\$ 0.00	\$ 0.00	\$ 536,222.23	\$ 86,831.77	\$ 623,054.00
Pinellas.....	0.00	0.00	417,938.05	107,175.09	525,113.14
Polk.....	0.00	0.00	772,550.23	502,081.05	1,274,631.28
Putnam.....	512,833.13	724,793.42	987,477.12	101,520.75	2,326,624.42
St. Johns.....	507,227.60	0.00	808,192.28	532,022.51	1,847,442.39
St. Lucie.....	333,910.63	381,000.00	522,517.16	57,185.94	1,294,613.73
Santa Rosa.....	385,531.81	114,546.27	609,591.08	181,016.68	1,290,685.84
Sarasota.....	0.00	100,000.00	1,554,129.73	44,026.90	1,698,156.63
Seminole.....	0.00	0.00	603,297.12	195,367.38	798,664.50
Sumter.....	0.00	0.00	115.61	21,470.98	21,586.59
Suwannee.....	55,526.90	400,000.00	741,459.56	136,128.48	1,333,114.94
Taylor.....	0.00	0.00	325,687.48	32,361.33	358,048.81
Union.....	0.00	*171,000.00	0.00	6,940.39	136,550.88
Volusia.....	0.00	493,067.83	1,162,139.09	176,080.59	1,831,287.51
Wakulla.....	0.00	9,700.00	618,427.05	46,657.36	674,784.41
Walton.....	0.00	6,389.02	172,301.81	104,292.78	282,983.61
Washington.....	77,064.48	25,000.00	308,445.66	129,129.22	539,639.36
Total.....	\$ 8,104,391.51	\$12,024,951.49	\$47,442,791.00	\$ 8,947,561.61	\$76,478,306.10

Note—*Credit of Union County, Unspent County Funds \$41,389.51.

**Credit of Glades County, Unspent County Funds for Maintenance \$4,000.00.



Rock Base Surface Treated, Federal Aid, Road 4.



STATUS OF CONSTRUCTION BY COUNTIES
IN MILES

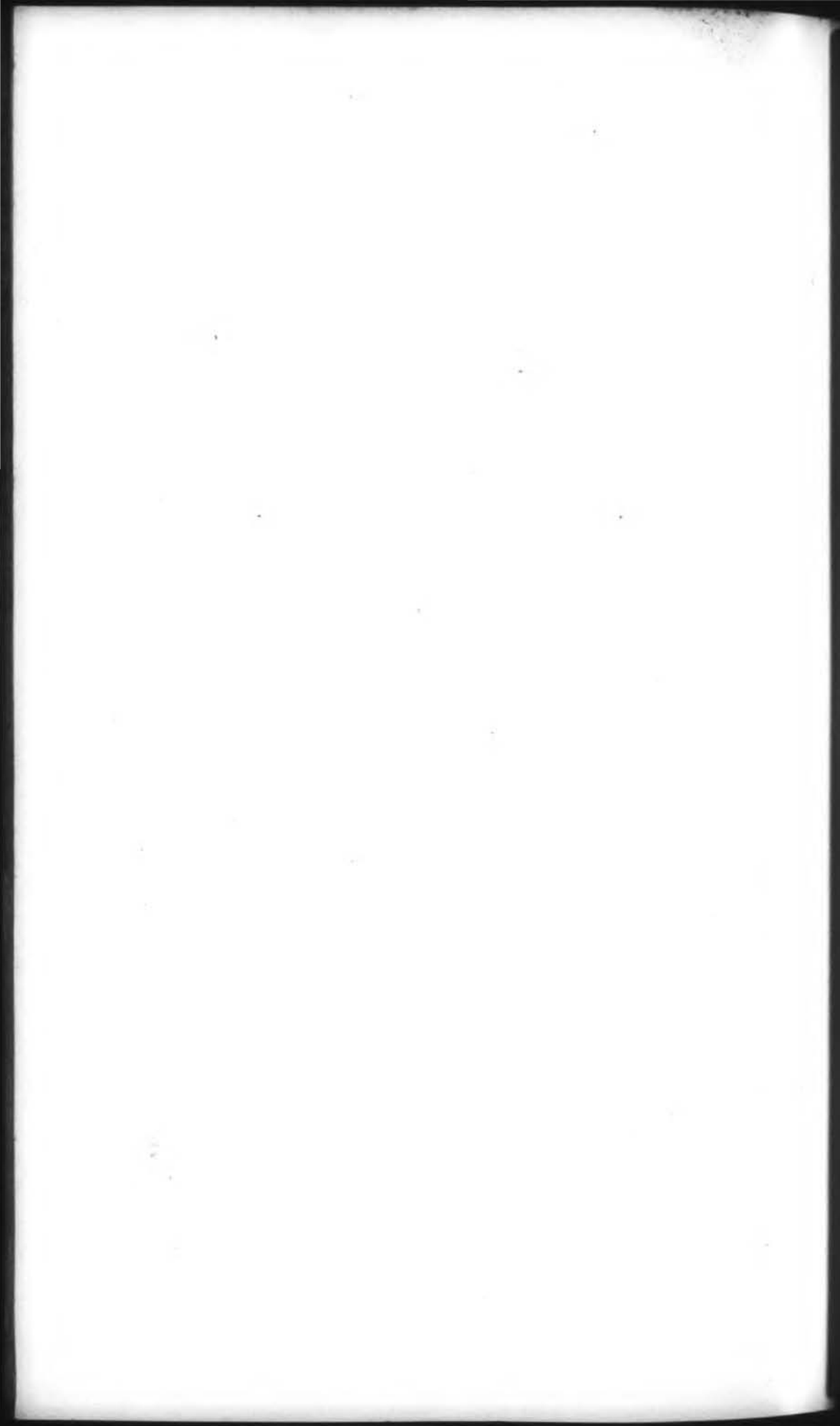
County	Paved	Paving under Construction	Graded .	Grading under Construction
Alachua.....	62.50	16.80	7.65	*
Baker.....	25.49	.....	.....	.....
Bay.....	10.00	18.30	18.10	46.37
Bradford.....	21.70	.....	22.05	.....
Brevard.....	83.92	.....	2.00	.....
Broward.....	6.30	.....	13.46	.....
Calhoun.....	.....	.....	.....	34.10
Charlotte.....	25.13	.....	20.18	.....
Citrus.....	29.80	.....	.....	.....
Clay.....	29.44	.....	6.00	.....
Collier.....	63.55	.....	.....	.....
Columbia.....	54.30	.....	21.61	.....
Dade.....	50.30	.....	.....	.....
DeSoto.....	26.61	.....	7.10	.....
Dixie.....	18.21	.....	12.59	.....
Duval.....	18.67	.....	.....	.....
Escambia.....	29.33	10.10	10.10	7.10
Flagler.....	22.80	.....	.....	.....
Franklin.....	28.50	8.00	.....	.....
Gadsden.....	19.80	15.88	.....	.....
Gilchrist.....	.....	.....	.....	18.21
Glades.....	.....	18.55	.....	.....
Gulf.....	30.82	.....	20.00	7.1
Hamilton.....	34.06	.....	6.44	.....
Hardee.....	14.19	.....	.....	.....
Hendry.....	.....	.....	.....	.....
Hernando.....	18.67	.....	.....	.....
Highlands.....	48.37	.....	9.00	.....
Hillsboro.....	12.10	.....	19.91	.....
Holmes.....	26.00	.....	18.06	.....
Indian River.....	31.20	.....	.....	.....
Jackson.....	82.06	.....	7.40	2.00
Jefferson.....	17.00	.....	41.08	8.83
LaFayette.....	.....	.....	30.95	.....
Lake.....	37.99	17.07	7.50	.....
Lee.....	8.67	.....	.....	.....
Leon.....	19.00	27.92	21.45	22.43
Levy.....	55.27	.....	21.74	.....
Liberty.....	.....	.....	.....	.....
Madison.....	43.19	.....	30.39	.....
Manatee.....	21.83	.....	.....	.....
Marion.....	71.73	.....	11.78	.....
Martin.....	29.76	.....	.....	.....
Monroe.....	.....	.....	.....	.....
Nassau.....	35.12	.....	.....	15.30
Okaloosa.....	45.27	.....	23.68	48.34
Okeechobee.....	18.31	11.00	17.62	.....
Orange.....	8.59	.....	16.81	.....
Osceola.....	11.41	.....	.....	37.50
Palm Beach.....	29.10	16.95	8.27	.....
Pasco.....	20.08	.....	.....	.....
Pinellas.....	.....	.....	.....	.....

STATUS OF CONSTRUCTION BY COUNTIES
IN MILES (Continued)

County	Paved	Paving under Construction	Graded	Grading under Construction
Polk.....	12.36	.....	20.15	20.99
Putnam.....	66.10	.....	21.01	.....
Santa Rosa.....	28.00	.....	.....	.....
Sarasota.....	27.98	.....	11.92	.....
Seminole.....	14.20	.....	.....	.....
St. Johns.....	31.40	.....	.....	.....
St. Lucie.....	38.97	.....	.....	.....
Sumter.....	.....	.....	.....	.....
Suwannee.....	25.40	39.80	.....	.....
Taylor.....	10.00	.....	15.95	19.57
Union.....	.....	.....	13.90	.....
Volusia.....	60.59	.....	.....	.....
Wakulla.....	25.49	.....	5.05	.....
Walton.....	21.30	.....	.....	96.27
Washington.....	14.30	.....	.....	.....
Total.....	1,772.23	200.37	540.90	384.11



Bituminous Macadam, Federal Aid, Road 4.



MILEAGE OF STATE SYSTEMS
AND
STATUS OF CONSTRUCTION ON THE PREFERENTIAL SYSTEMS

AS OF JANUARY 1st, 1929.

No. miles continuous through cities.	No. miles of construction.	
First Preferential.....	2,588.29 = 30%	2,406.26 = 29%
Second Preferential.....	883.24 = 10%	874.24 = 11%
Remainder of System.....	5,042.47 = 60%	5,043.50 = 60%
Total for State.....	8,524.00 = 100%	8,324.00 = 100%

FIRST PREFERENTIAL SYSTEM

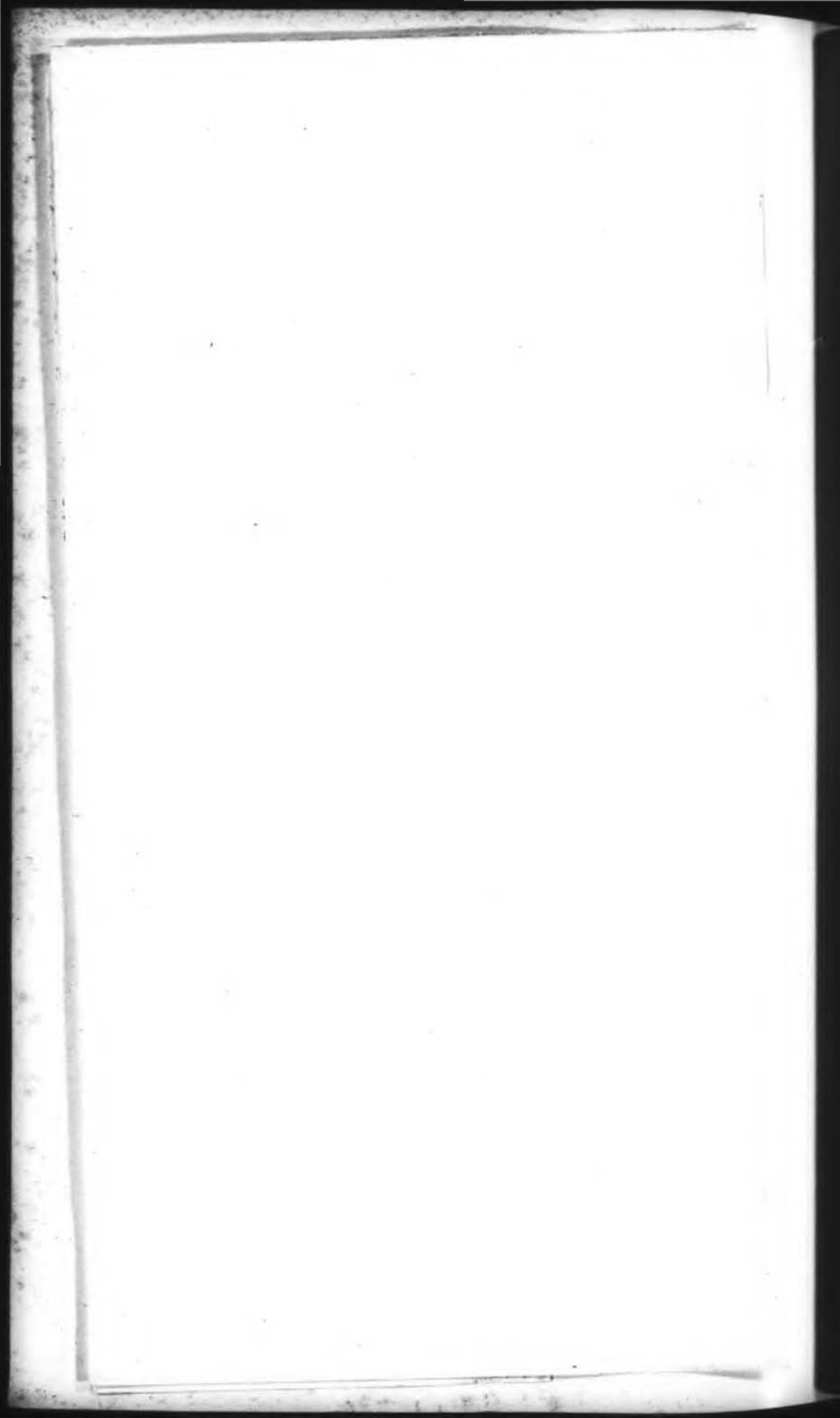
Number of miles State Surfacing completed.....	1,587.67 = 66%	
Number miles State Surfacing under construction....	58.87 = 2%	
Number miles State Graded ready for paving.....	213.55 = 9%	
Number miles Grading under construction.....	78.06 = 3%	
Number miles County Paving.....	426.33 = 18%	
Number miles on which no work done.....	41.78 = 2%	
Total.....	2,406.26 = 100%	2,406.26

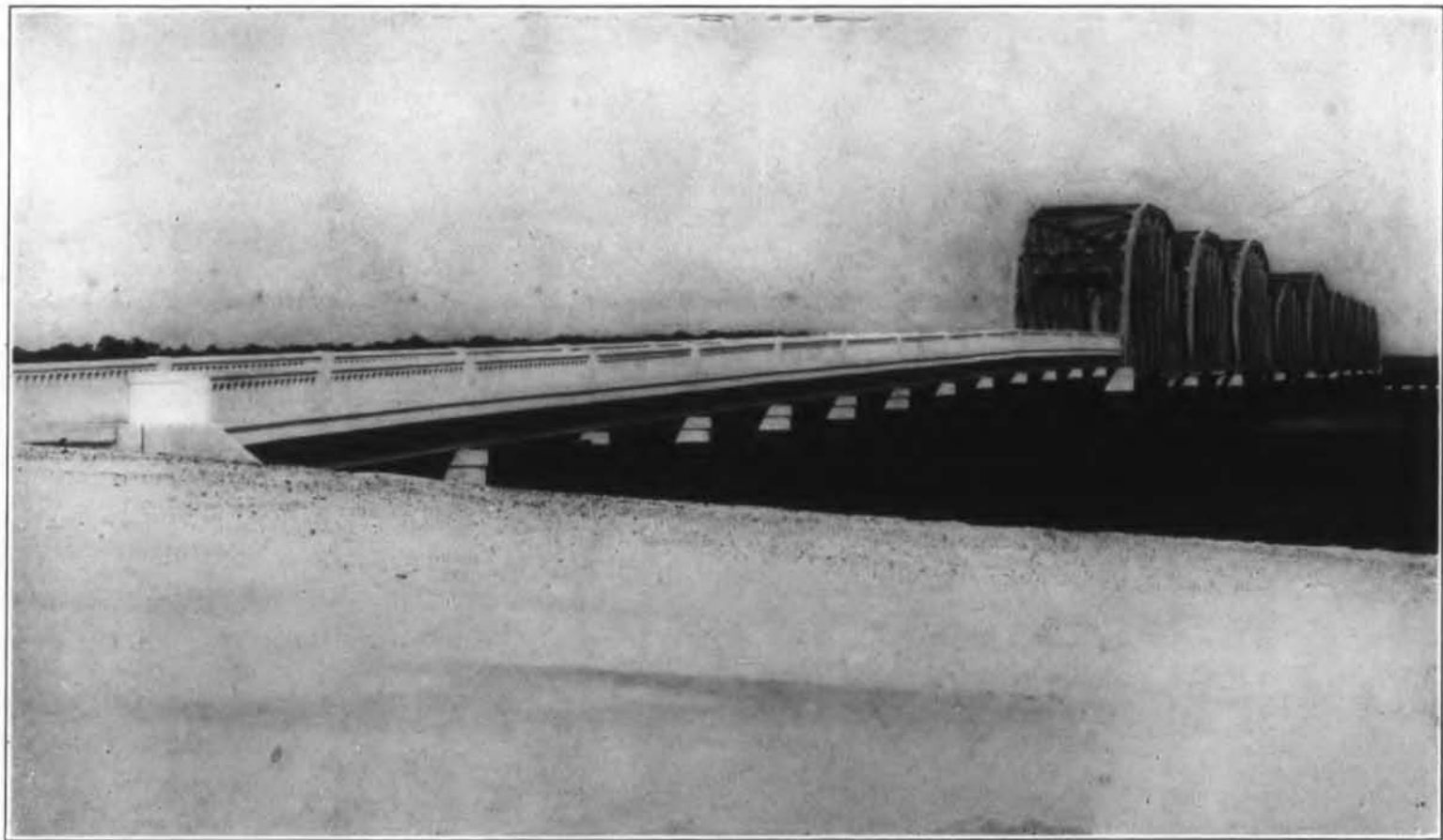
SECOND PREFERENTIAL SYSTEM

Number miles State Paving completed.....	131.66 = 15%	
Number miles State Paving under construction....	26.87 = 3%	
Number miles State Graded ready for paving.....	234.36 = 27%	
Number miles State Grading under construction....	119.48 = 14%	
Number miles County Paving (completed).....	95.67 = 11%	
Number miles on which no work has been done....	266.20 = 30%	
Total.....	874.24 = 100%	874.24
Total First and Second Preferential Systems.....		3,280.50

1927

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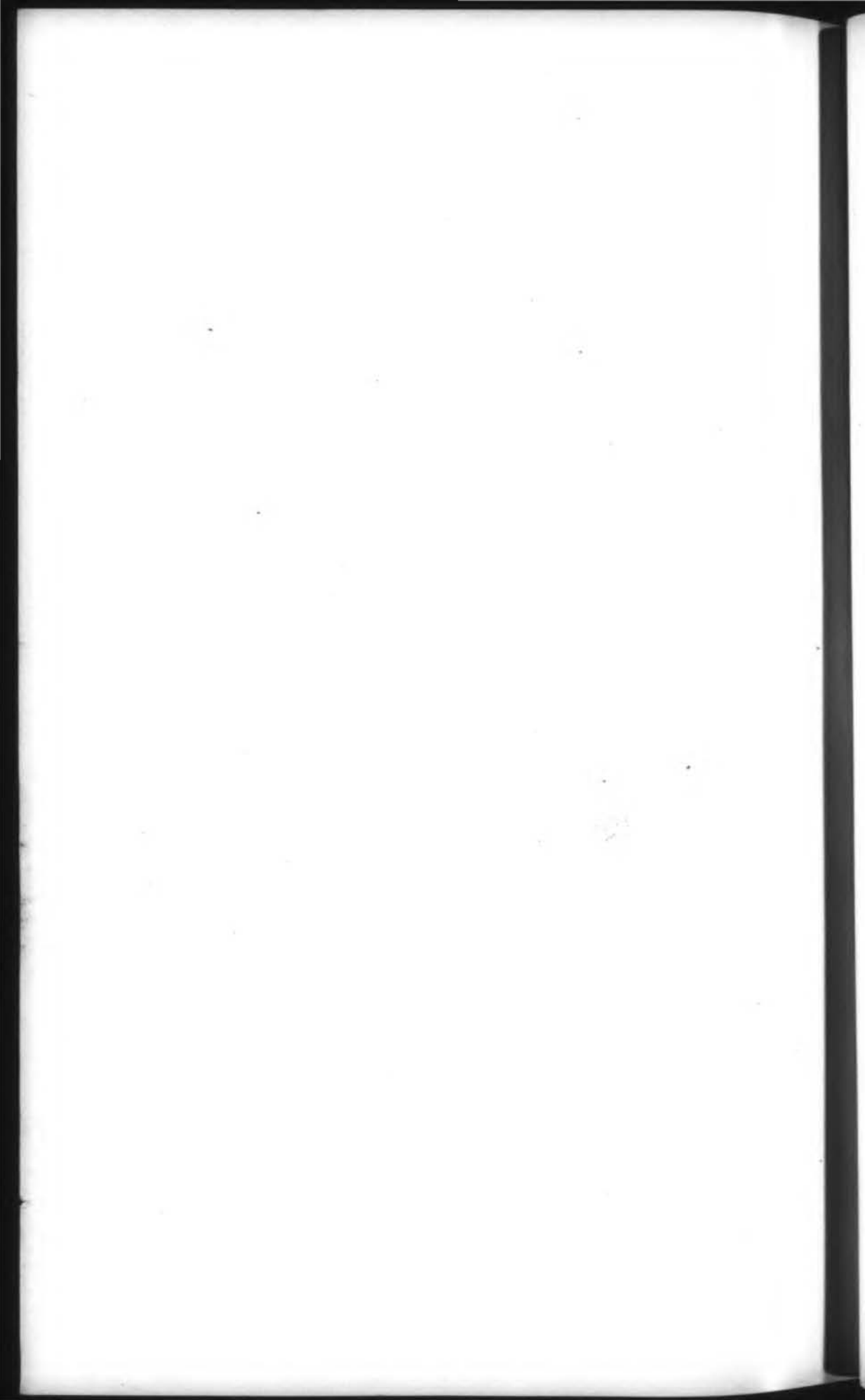
Approach to East Bay Bridge, Road 10.

STATE ROAD DEPARTMENT OF FLORIDA
UNIT PRICES OF ITEMS LET TO CONTRACT ON ROADS
1928

Project No.	Contractor	Clearing and Grubbing Acres	Regular Excavation Cubic Yards	Broken Stone Base No. 2 Squar. Yards	Single Bituminous Treatment Sq. Yds.	Surfacing—Square Yards					Concrete Class A—Cu. Yd.		Cement Concrete Curb Lin. feet	Comb. Curb-Gutter Lin. feet	Woven Wire G. Rail Lin. feet	Rock Asphalt Square Yards	Rein. Steel lbs.	Pipe for x-Roads								Pipe for Struct.				Structural Timber		Foundation Piling		Man Hole	Concrete Class B Cubic Yards	Extra Rock Tons																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																	
						Bituminous Macadam	Bituminous Material	Bituminous Concrete	Sheet Asphalt	Plain Cem. Concrete	Head-Walls	Culverts						Corr. Metal				Vit. Clay		Conc.		Cast Iron				Treat. MFBM	Untre. MFBM	Treat. Lin. feet	Untre. Lin. feet																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																				
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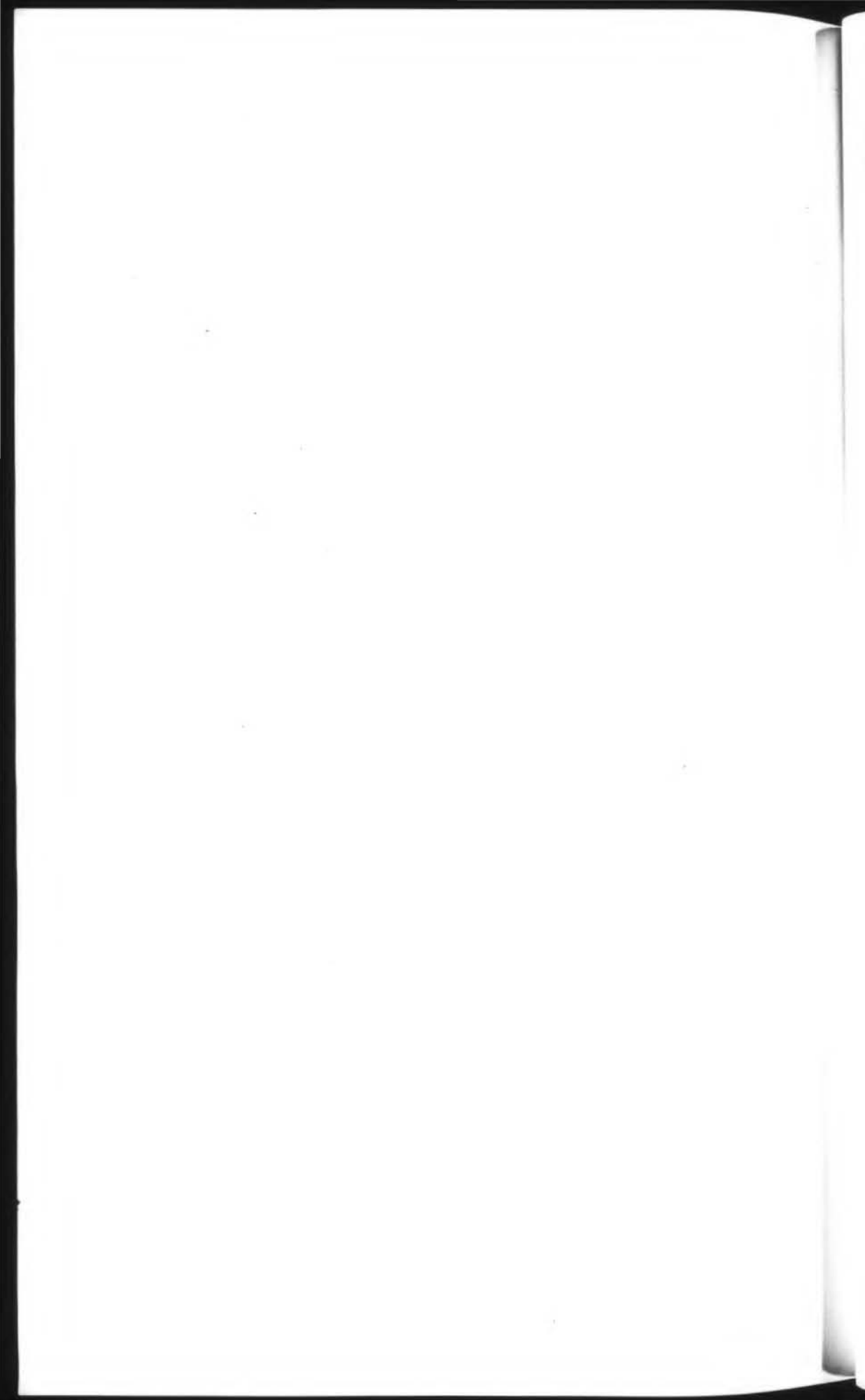


Plain Cement Concrete, Road 13.



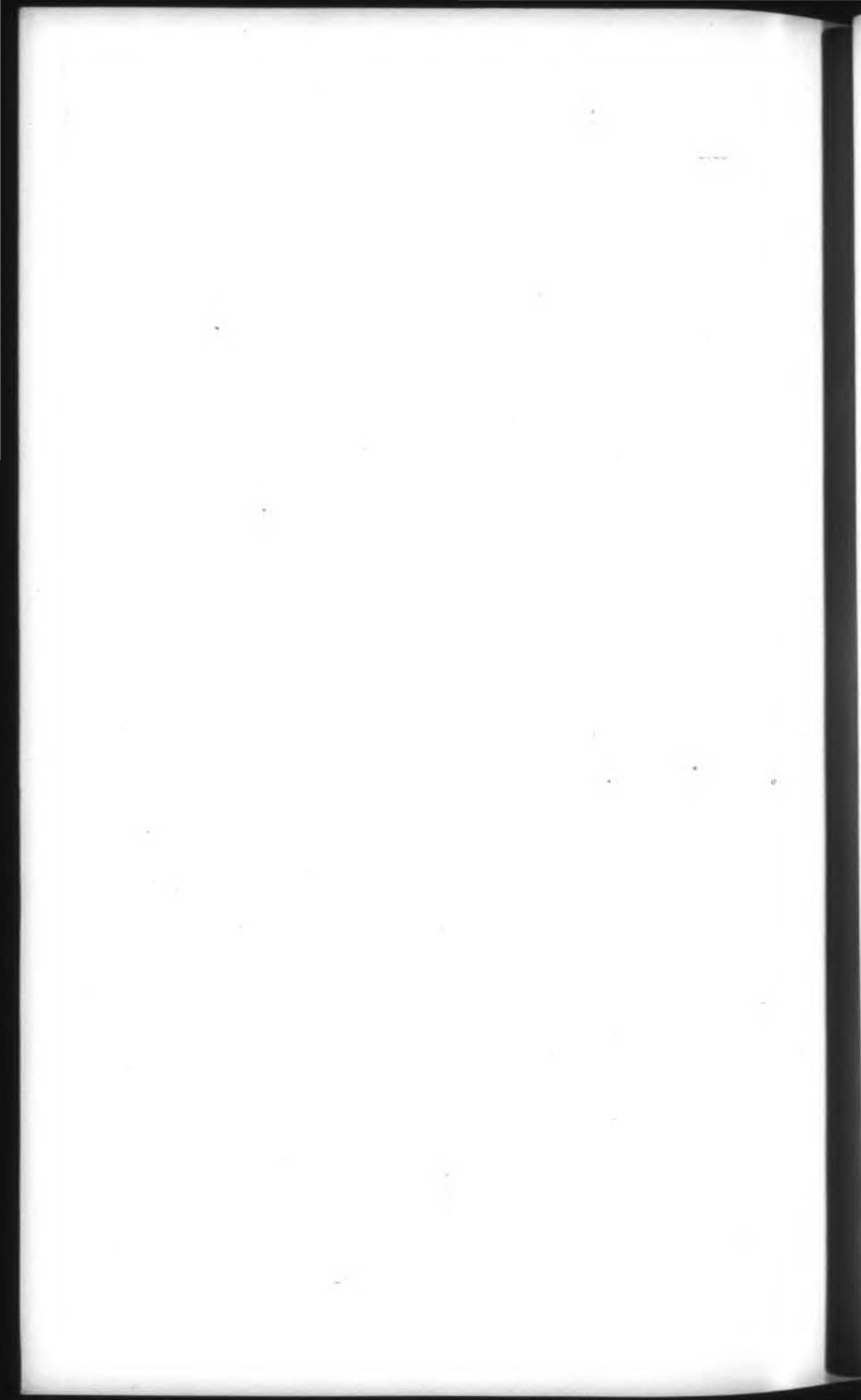


Bituminous Macadam, Road 4.



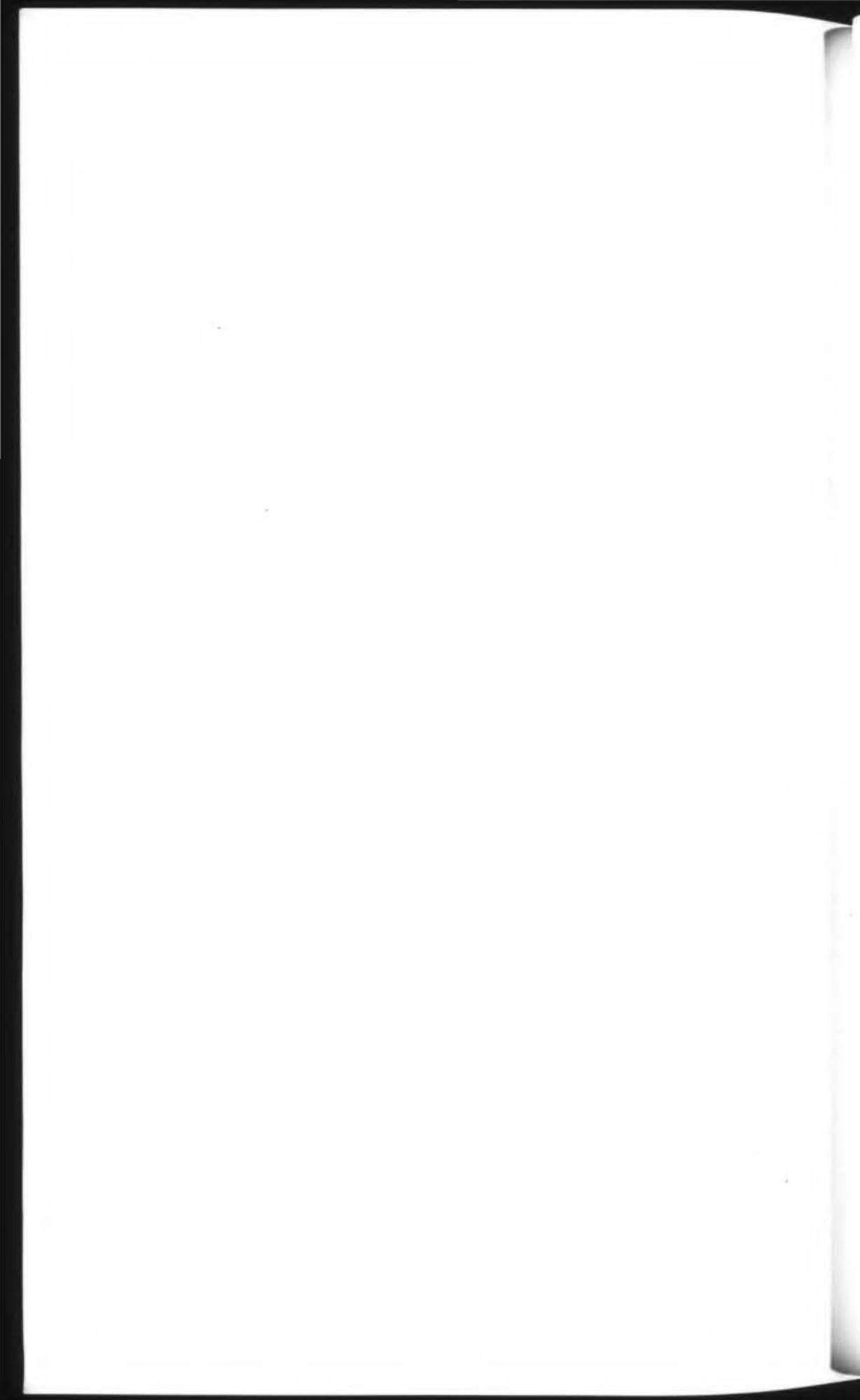


Rock Base Surface Treated, Road 2.





Rock Base Surface Treated, Road 4, Showing the Indian River.



AUDITING DEPARTMENT

The tables immediately following are taken from the monthly report of the Auditor of the Department and give the actual cash transactions as to receipts and disbursements, which receipts and disbursements are checked with the Comptroller each month.

Tables inserted are for the month of December 31st, 1926, December 31st, 1927, and December 31st, 1928.

By publishing the report for December, 1926, the balance on hand at the beginning of this biennial as well as the disbursements at the same time are given in order that they may be carried forward into the biennial.

In addition to these monthly statements for each December of the years mentioned, there follows a statistical sheet showing the resources from the several tax funds from the creation of the Department to the end of this biennial.

STATE ROAD DEPARTMENT OF FLORIDA
DISBURSEMENTS TO DECEMBER 31st, 1926.

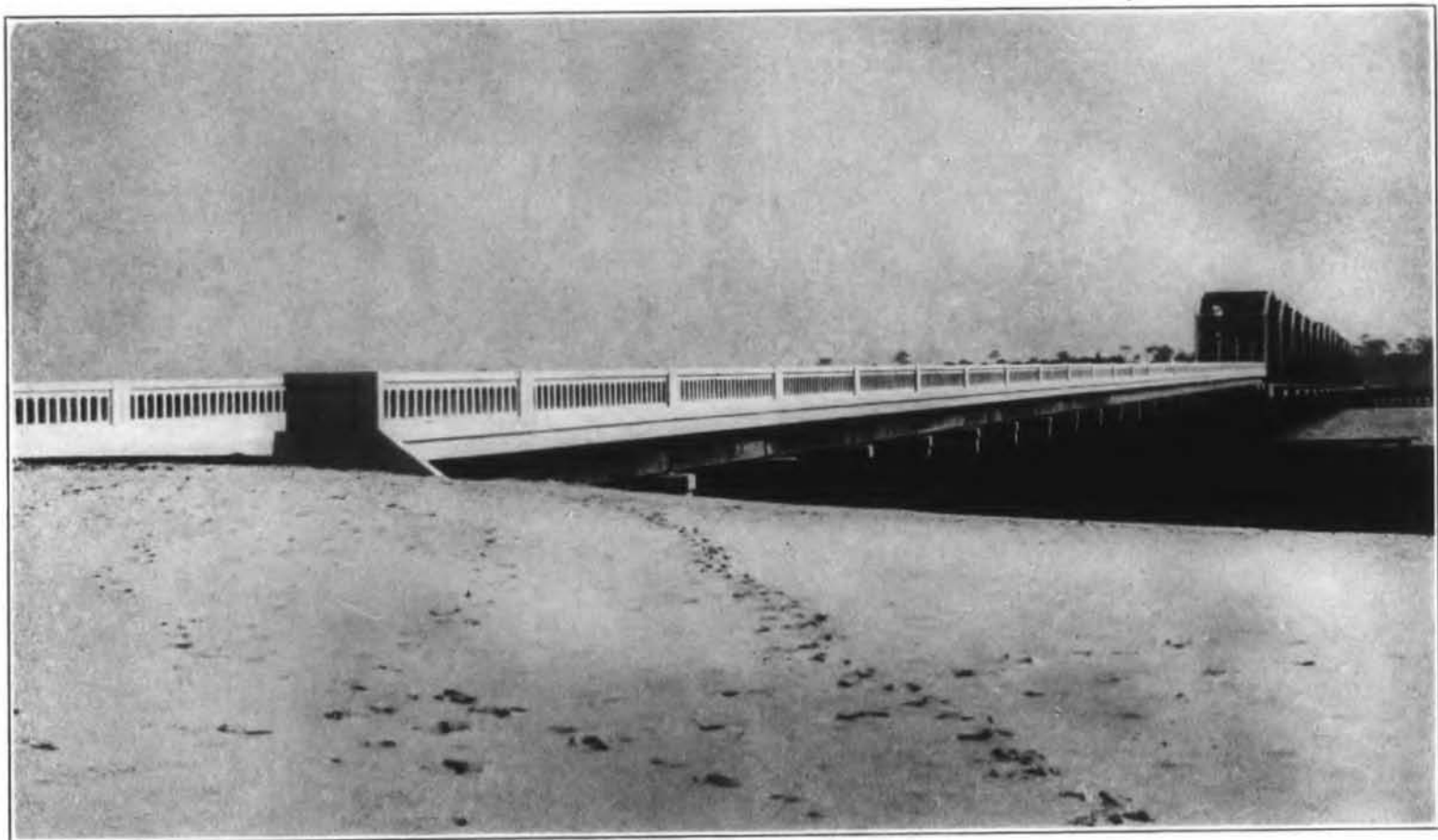
	Month of December		Total Twelve Months		Net to Date	
	Debits	Credits	Debits	Credits	Debits	Credits
ADMINISTRATIVE:						
Salaries.....	\$ 1,325.00		\$ 12,800.00		\$ 71,299.97	
All Others.....	820.93		7,964.12		58,144.84	
Legal.....	324.44		4,997.38		15,180.44	
Traffic Expenses.....	250.00		2,697.46		2,697.46	
GENERAL OFFICE:						
Salaries.....	2,038.33		17,995.11		103,793.74	
Stationery.....	450.23		6,109.42		40,427.74	
Furniture and Fixtures.....	86.65		1,198.95		7,625.41	
Rents.....	225.00		2,675.00		9,881.00	
Telephone and Telegraph.....	236.12		2,247.38		9,501.53	
ENGINEERING EXPENSES:						
Salaries.....	7,792.33		72,593.05		377,882.04	
Testing Division.....	1,664.03		17,065.34	\$ 1,644.22	50,464.04	
All Others.....	6,975.25		59,157.39		211,868.45	
Maintenance—Pool.....	2,141.51		15,905.59		15,905.59	
MAINTAINING ROADS:						
Engineering.....	1,697.57		10,251.65		39,342.18	
Maintenance.....	242,239.95		1,863,441.27		4,548,980.56	
CONSTRUCTION OF ROADS:						
Engineering.....	63,557.51		589,667.89		1,759,401.66	
Construction.....	1,670,187.02		10,735,900.74		32,842,577.46	
SUPPLY ACCOUNT:						
Parts.....	18,966.91	\$ 47,918.73	182,327.12	188,533.12	176,910.85	
Convict Supplies.....	46.65	5,083.77	22,543.27	24,029.67	9,342.23	
Stationery Stock.....	202.16	453.16	4,854.24	4,339.25	2,558.56	

EQUIPMENT ACCOUNT:						
Equipment, New.....	13,934.84		279,140.64			
Equipment, Overhauled.....	44,796.52		186,549.81	3,676.24		
Equipment, Live Stock.....	4,465.35		20,342.80	300.00	\$ 799,558.11	
Gainesville Shop.....					94,806.85	
Convicts Camp Equipment.....					102,720.33	
SUSPENSE ACCOUNTS:						
Shops Operation.....	14,281.09	30,124.16	117,873.25	126,751.07	42,703.83	
Work in Process.....	23,511.59	24,719.90	119,886.94	126,786.07	13,710.52	
Parts—Junk.....	92.47		1,634.79		2,732.26	
Bills—Receivable.....	286.82		1,050.19	169.15	2,623.39	
Depreciation Reserve.....		20,734.16		244,882.34		
Convict Supplies—Raiford.....	496.10	311.85	1,382.70	3,010.81		\$ 956.44
Pineola Quarry.....					5,048.31	
Yukon Shop.....					25,000.00	
Equipment Donated.....			4,230.76		35,415.32	
Road Signs.....	2,056.97		16,651.84		16,651.84	
NET DISBURSEMENTS.....						
	\$ 1,995,803.61		\$13,657,014.15		\$41,493,800.07	
WORKING FUND:						
Contingent Fund.....	854.88		27,056.11		122,888.81	
Treasurer.....		1,316,389.53	2,222,841.90		4,554,159.84	
GROSS TREASURER'S RECEIPTS:.....						
	\$ 680,268.96		\$15,906,912.16		\$46,170,848.72	

STATE ROAD DEPARTMENT OF FLORIDA

CASH STATEMENT—DECEMBER, 1926

	Brought Forward From 1925	Receipts For Dec., 1926	Receipts For Twelve Months, 1926	Working Fund Oct., 1915, to Dec. 31, 1926	Disbursements Oct., 1915, to Dec. 31, 1926	Balance to Dec. 31st, 1926
Motor Vehicle License—5% Fund	\$ 56,511.44	\$	\$ 314,244.20	\$ 1,080,983.53	\$ 836,924.62	\$ 244,058.91
Motor Vehicle License—85% Fund	3,635.77	.....	.....	1,529,511.14	1,525,935.60	3,575.54
State Federal Aid—70% Fund	129,794.77	.....	4,399,418.71	10,586,553.53	9,867,461.92	719,091.61
State Road Tax Fund	1,347,225.42	.....	.....	.....	.....	.....
State Road Tax Fund—1 Mill .....	.....	29,410.32	.....	.....	.....	.....
State Road Tax Fund—Counties .....	.....	.....	1,138,563.70	8,795,396.88	7,773,727.64	1,021,669.24
Federal Aid Road Fund	78,586.82	.....	1,007,484.16	5,543,475.98	5,408,103.86	135,372.12
State Road License Fund Gas— (3c. Tax)..	714,155.65	650,858.64	8,636,358.93	17,532,087.23	15,136,943.70	2,395,143.53
Maintenances Fund—Gas and Oil Inspection .....	.....	.....	210,842.46	752,839.03	752,816.03	23.00
State Road Tax Fund for Maintenance of State Roads	1,408.07	.....	200,000.00	350,001.40	314,775.51	35,225.89
Total—Balance—Comptroller	\$ 2,331,317.94	\$ 680,268.96	\$15,906,912.16	\$46,170,848.72	\$41,616,688.88	\$ 4,554,159.84



Approach to West Bay Bridge, Road 10.

STATE ROAD DEPARTMENT OF FLORIDA
STATISTICAL SHEET SHOWING RESOURCES FROM THE SEVERAL TAX FUNDS SINCE INAUGURATION
OF STATE ROAD DEPARTMENT—OCTOBER, 1915, TO DECEMBER 31st, 1928.

Year	(1) 15% & 5% General Administrative	(2) 85% Auto License	(3) 70% Auto License	(4) State Tax Advalorem	(5) Federal Government	(6) Gasoline Inspection	(7) Gasoline 1c., 2c. and 3c.	(8) Road Maintenance Fund	(9) County and Town Donations	Total Receipts
Nov. and Dec. 1915..	\$ 3,646.90									\$ 3,646.90
" " " 1916..	30,246.23									30,246.23
" " " 1917..	21,439.42									21,439.42
" " " 1918..	42,164.42	\$ 236,300.00		\$ 175,842.11	\$ 9,972.55				\$ 238.79	475,390.30
" " " 1919..	52,443.82	297,034.63		290,532.41	11,759.19				11,111.22	722,927.81
" " " 1920..	73,725.00	425,775.00		694,814.77	209,060.84	\$ 17,850.32			71,157.76	1,660,546.42
" " " 1921..	101,133.40	570,401.51		643,373.16	490,308.38	55,824.74	\$ 228,056.41		239,320.49	2,200,806.36
" " " 1922..	71,623.51		1,002,729.20	396,206.19	842,965.84	80,803.14	601,336.71		111,708.76	3,576,381.51
" " " 1923..	92,968.50		1,301,560.08	427,243.17	820,063.74	91,884.70	1,261,845.08		580,716.92	4,511,553.62
" " " 1924..	112,579.83		1,576,118.04	424,392.88	1,071,287.04	126,325.66	2,448,855.49	\$ 70,001.40	515,988.35	6,708,780.17
" " " 1925..	164,768.30		2,306,727.50	505,109.94	1,080,574.24	169,308.01	4,355,634.71	80,000.00	879,219.83	10,351,979.03
" " " 1926..	314,244.20		4,399,418.71	524,426.09	1,007,484.16	210,842.46	8,636,358.93	200,000.00	1,689,856.43	15,906,912.16
" " " 1927..	301,710.05		4,221,924.26	132,566.97	1,688,401.62	298,688.75	8,243,691.18	90,000.00	614,137.61	19,403,095.86
" " " 1928..	228,623.58		3,200,716.39	4,649.30	872,513.91		6,770,508.10		4,426,113.03	14,050,879.26
Total.....	\$ 1,611,317.16	\$ 1,529,511.14	\$18,009,194.18	\$ 4,219,156.99	\$ 8,104,391.51	\$ 1,051,527.78	\$32,546,286.51	\$ 440,001.40	\$12,113,437.17	\$79,624,823.84
DISBURSEMENTS:										
Nov. 1915-Dec. 1928	\$ 1,604,830.10	\$ 1,525,935.60	\$17,998,856.73	\$ 4,219,156.99	\$ 8,099,240.54	\$ 1,051,527.36	\$32,535,551.05	\$ 439,998.06	\$12,041,483.46	\$79,516,579.89
BALANCES:										
Dec. 31st, 1928.....	\$ 6,487.06	\$ 3,575.54	\$ 10,337.45		\$ 5,150.97	\$.42	\$ 10,735.46	\$ 3.34	\$ 71,953.71	\$ 108,243.95

- (1) November, 1915, to December 31st, 1921, Inclusive—15 per cent of net receipts from sale of Auto License was allotted for salaries of Chairman and Members: also for maintaining General Office. Effective January 1st, 1922, this was reduced to 5 per cent.
- (2) & (3) Year 1918 to 1921, Inclusive. 85 per cent of net receipts from sale of Auto License was allotted Counties, but turned over to this Department for disbursement. Proportion allowed each County was on percentage basis, using Tax Valuation as a basis. This manner of handling was changed 1922, as follows: All receipts pooled. From pool, actual expenses of getting out tags are deducted. The net is then divided—25 per cent remitted to Counties direct: 5 per cent allowed for General Office Maintenance, and 70 percent for construction of roads.
- (4) Advalorem Tax on real and personal property for road purposes. This Tax was repealed Legislature Session 1927.
- (5) Amount received from Federal Government. This does not represent the amount allotted, but is amount actually paid in.
- (6) Effective 1921, an Inspection Tax of $\frac{1}{8}$ cent per gallon was imposed on gasoline sold within the State. No revenue from this source year 1928.
- (7) Effective 1921, a tax of one cent per gallon was imposed on gasoline. July, 1923, this tax was increased to 2 cents, and July, 1925, increased to three cents.
- (8) Represents proportion allowed as net receipts from sale of Auto Registration Certificates. No revenue from this source year 1928.
- (9) Paid in by Counties as contribution on roads constructed or to be constructed.

Tables 1, 2, 3, 4, 6, 7, and 8 do not represent total collection from taxes, as named, but is amount allowed this Department for maintaining Headquarters, Construction and Maintaining Roads.

STATE ROAD DEPARTMENT OF FLORIDA
DISBURSEMENTS TO DECEMBER 31st, 1927

	Month of December		Total Twelve Months		Net to Date	
	Debits	Credits	Debits	Credits	Debits	Credits
ADMINISTRATIVE:						
Salaries.....	\$ 1,225.00		\$ 13,875.00		\$ 85,174.97	
All Others.....	1,285.17		11,493.36		69,638.20	
Legal.....	1,534.82		6,268.00		21,448.44	
Traffic Expenses.....	250.00		3,250.00		5,947.46	
GENERAL OFFICE:						
Salaries.....	2,065.29		25,859.17		129,652.91	
Stationery.....	1,377.95		8,780.58		49,208.32	
Furniture and Fixtures.....	68.40		1,578.73		9,204.14	
Rents.....			2,755.00		12,636.00	
Telephone and Telegraph.....	189.14		2,321.30		11,822.83	
ENGINEERING EXPENSES:						
Salaries.....	7,939.50		95,169.59		473,051.63	
All Others.....	2,844.71		50,616.64		262,485.09	
Testing Division.....	1,258.92		16,675.66		67,139.70	
Testing Division—Pool.....			2,112.31	2,112.31		
Maintenance—Pool Engineer.....	3,143.64		29,190.87	15,905.59	29,190.87	
Maintenance—Pool Maintenance.....	2,162.97	75.00	6,193.15	75.00	6,118.15	
Construction—Pool Engineer.....			1,419.66		1,419.66	
MAINTAINING ROADS:						
Engineering.....	517.21		29,089.20		68,431.38	
Maintenance.....	338,957.91		2,561,878.02		7,110,858.58	
CONSTRUCTION OF ROADS:						
Engineering.....	71,389.94		896,036.09		2,655,437.75	
Construction.....	1,249,947.37		19,714,488.24		52,557,065.70	
SUPPLY ACCOUNTS:						
Parts.....	22,975.75	12,803.47	228,182.51	205,137.26	199,956.10	
Convict Supplies, General.....		4,217.21	36,176.62	43,511.65	2,007.20	
Stationery Stock.....	43.00	484.01	5,252.71	5,089.95	2,721.32	

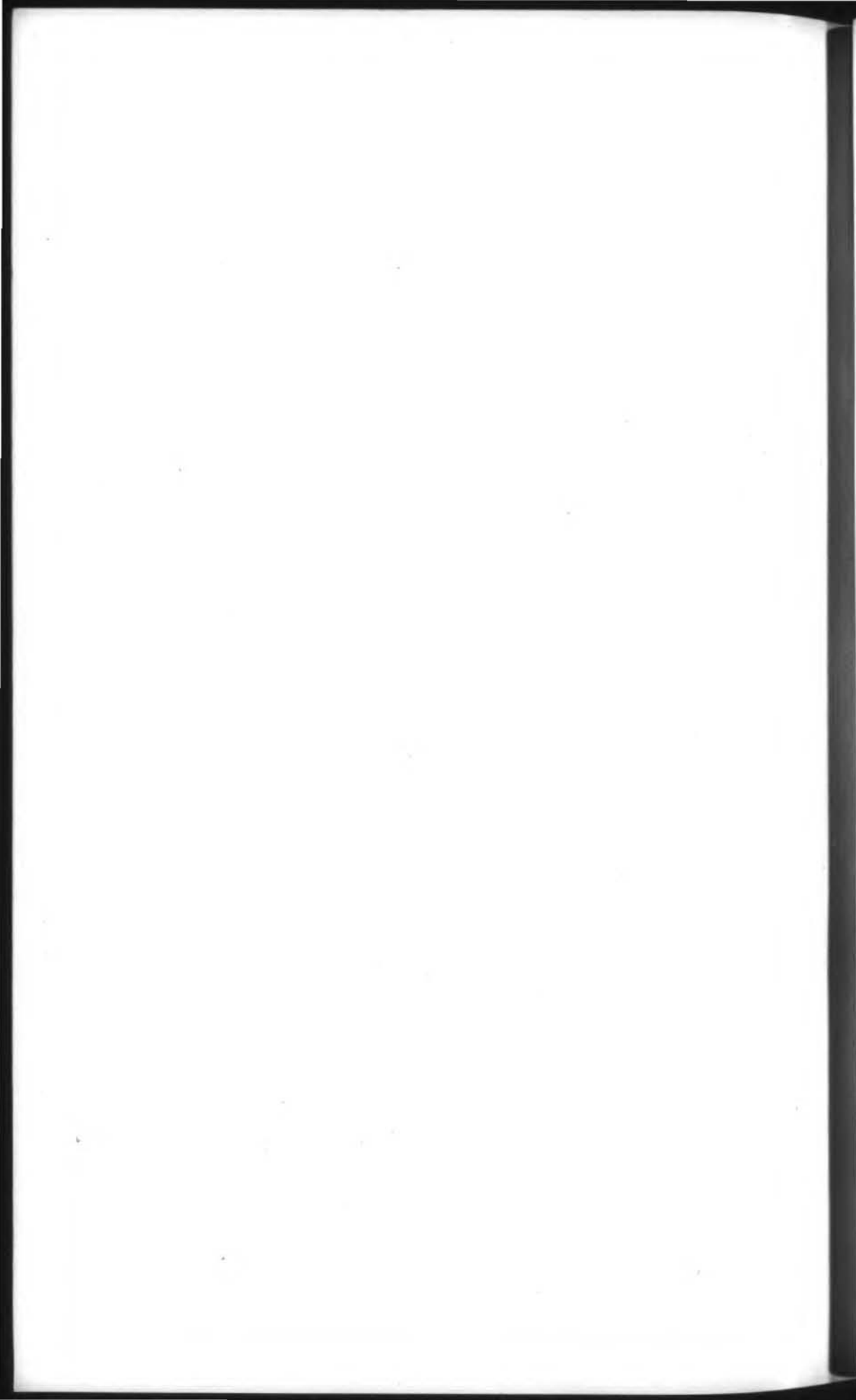
STATE ROAD DEPARTMENT OF FLORIDA
DISBURSEMENTS TO DECEMBER 31st, 1927 (Continued)

	Month of December		Total Twelve Months		Net to Date	
	Debits	Credits	Debits	Credits	Debits	Credits
EQUIPMENT ACCOUNTS:						
Equipment, New.....	\$ 3,335.38		\$ 132,330.98	\$.....	\$.....	\$.....
Equipment, Overhauled.....	10,501.12		159,560.14	383.96		
Equipment, Live Stock.....	4,543.91		14,303.41	1,142.50	1,104,226.18	
Gainesville Shop.....					94,806.85	
Convict Camp Equipment.....					102,720.33	
SUSPENSE ACCOUNTS:						
Shop Operation.....	10,816.79	\$ 9,153.39	131,085.44	128,249.34	45,539.93	
Work in Process.....	8,180.49	7,484.68	108,693.54	96,507.53	25,896.53	
Parts—Junk.....	17.52		541.28	332.30	2,941.24	
Bills Receivable.....	92.49	1,687.67	11,966.20	9,756.73	4,832.86	
Depreciation Reserve.....		27,255.01		328,330.68		328,330.68
Convict Supplies—Raiford.....	498.97	367.50	2,327.17	3,417.55		2,046.82
Pineola Quarry.....					5,048.31	
Yukon Shop.....					25,000.00	
Equipment Donated.....	1,604.30		1,604.30		37,019.62	
Road Signs.....			17,011.21	5.90	33,657.15	
NET DISBURSEMENTS.....	\$ 1,685,239.72		\$23,488,127.83		\$64,981,927.90	
WORKING FUND BALANCES:						
Contingent Fund.....	665.41		69,214.28		192,103.09	
Treasurer.....		222,104.35		4,154,246.25	399,913.59	
GROSS TREASURER'S RECEIPTS.....	\$ 1,463,800.78		\$19,403,095.86		\$65,573,944.58	

STATE ROAD DEPARTMENT OF FLORIDA

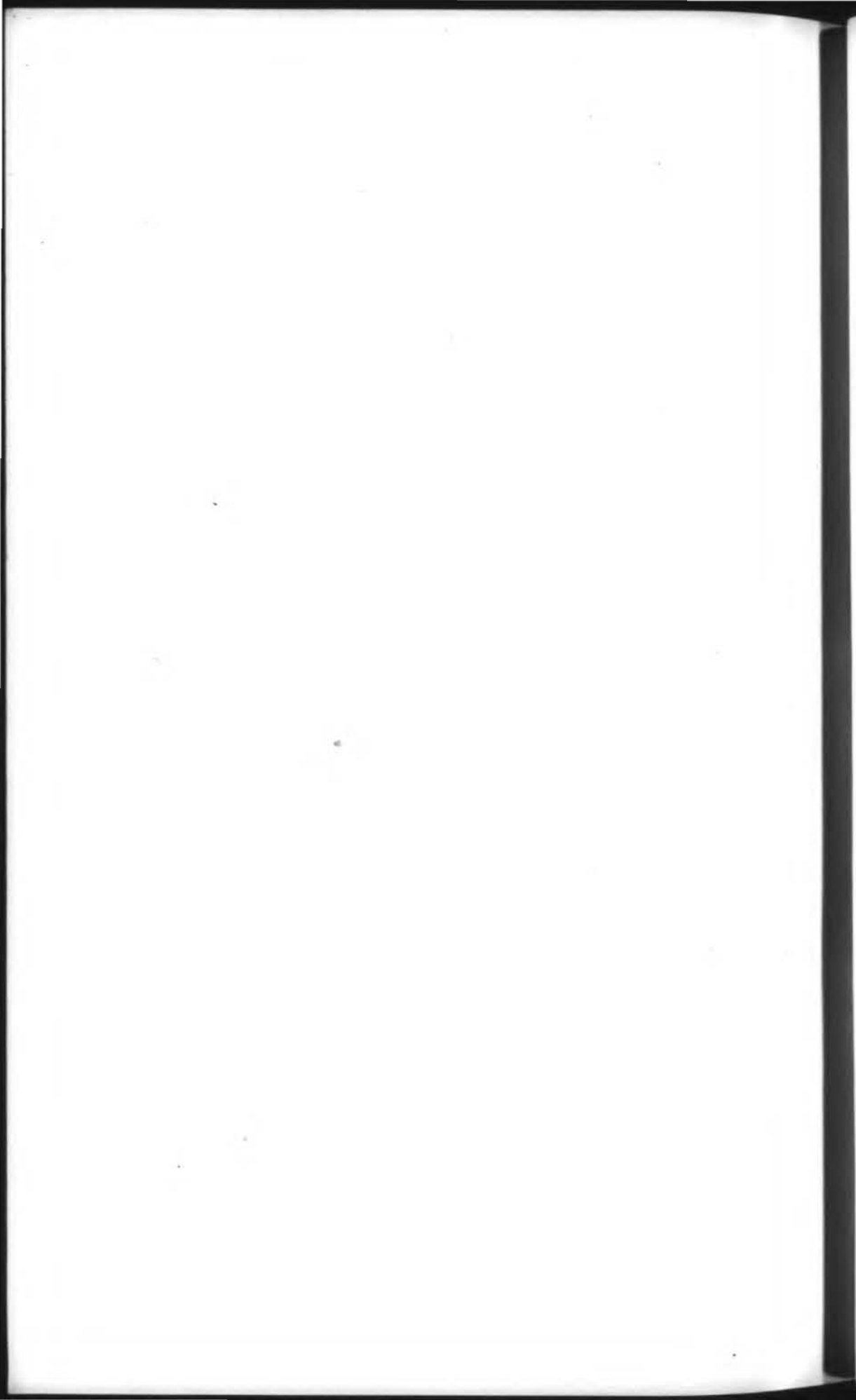
CASH STATEMENT DECEMBER, 1927

	Brought Forward From 1926	Receipts For Dec. 1927	Receipts For Twelve Months 1927	Working Fund Oct. 1915 to Dec. 31, 1927	Disbursements Oct. 1915 to Dec. 31, 1927	Balance to Dec. 31st. 1927
Motor Vehicle License—5% Fund.....	\$ 244,058.91	\$ 5,502.46	\$ 301,710.05	\$ 1,382,693.58	\$ 1,305,165.06	\$ 77,528.52
Motor Vehicle License—85% Fund.....	3,575.54			1,529,511.14	1,525,935.60	3,575.54
State Federal Aid—70% Fund.....	719,091.61	77,034.53	4,221,924.26	14,808,477.79	14,718,294.62	90,183.17
State Road Tax Fund.....	1,021,669.24					
State Road Tax Fund—1 Mill.....		1,136.03	148,838.99			
State Road Tax Fund—Counties.....		475,165.93	4,409,841.01	13,354,076.88	13,351,804.42	2,272.46
Federal Aid Road Fund.....	135,372.12					
Alachua County—Project 37-D.....		11,633.15				
Escambia County—Project 38-A & B.....	13,770.58					
Brevard-Indian River—Project 39-A & B.....		14,506.28				
Dade County—Project 41-B.....		24,830.29				
Flagler County—Project 49-A & B.....		52,524.40				
Putnam County—Project 50-B.....		71,438.62				
Putnam County—Project 50-C.....		67,577.24				
Escambia County—Project 52.....		12,693.08				
Leon County—Project 54-A.....		54,529.11	1,688,401.62	7,231,877.60	7,116,103.38	115,774.22
State Road License Fund Gasoline (3c. Tax) ..	2,395,143.53	581,459.08	2,243,691.18	25,775,778.41	25,665,246.13	110,532.28
Maintenance Fund—Gasoline and Oil Inspection.....	23.00		298,688.75	1,051,527.78	1,051,483.72	44.06
State Road Tax Fund for Maintenance of State Roads.....	35,225.89		90,000.00	440,001.40	439,998.06	3.34
Total—Balance—Comptroller.....	\$ 4,554,159.84	\$ 1,463,800.78	\$19,403,095.86	\$65,573,944.58	\$65,174,030.99	\$ 399,913.59





Sheet Asphalt, Federal Aid, Road 4.



STATE ROAD DEPARTMENT OF FLORIDA
DISBURSEMENTS TO DECEMBER 31st, 1928.

	Month of December		Total Twelve Months		Net to Date	
	Debits	Credits	Debits	Credits	Debits	Credits
ADMINISTRATIVE:						
Salaries.....	\$ 1,225.00		\$ 14,700.00		\$ 99,874.97	
All Others.....	561.63		7,752.07		77,390.27	
Legal.....	5,297.42		12,457.73		33,906.17	
Traffic Expenses.....	250.00		3,000.00		8,947.46	
GENERAL OFFICE:						
Salaries.....	2,085.45		26,126.97		155,779.88	
Stationery.....	300.11		8,547.73		57,756.05	
Furniture and Fixtures.....			1,633.12		10,837.26	
Rents.....					12,636.00	
Telephone and Telegraphs.....	169.58		2,319.25		14,142.08	
ENGINEERING EXPENSES:						
Salaries.....	8,264.17		98,645.86		571,697.49	
All Others.....	696.15		28,136.75		290,621.84	
Testing Division.....	1,854.94		19,882.52		87,022.22	
Maintenance—Pool Engineer.....	2,864.86		38,907.53	\$ 29,190.87	38,907.53	
Maintenance—Pool Maintenance, General.....	7,413.23		55,356.54	6,128.23	55,346.46	
Engineering—Construction Pool.....				1,419.66		
MAINTAINING ROADS:						
Engineering.....	825.42		40,262.78		108,694.16	
Maintenance.....	303,965.77		1,724,008.87		8,834,867.45	
CONSTRUCTION OF ROADS:						
Engineering.....	37,394.87		671,597.88		3,327,035.63	
Construction.....	811,812.19		11,646,643.16		64,203,708.86	
SUPPLY ACCOUNTS:						
Parts.....	9,420.42	\$ 14,493.58	108,328.48	136,722.91	171,561.67	
Convict Supplies, General.....	7.00	13,958.69	31,888.72	42,338.90		\$ 8,442.98
Stationery Stock.....	77.20	515.67	4,791.83	3,291.29	4,221.86	

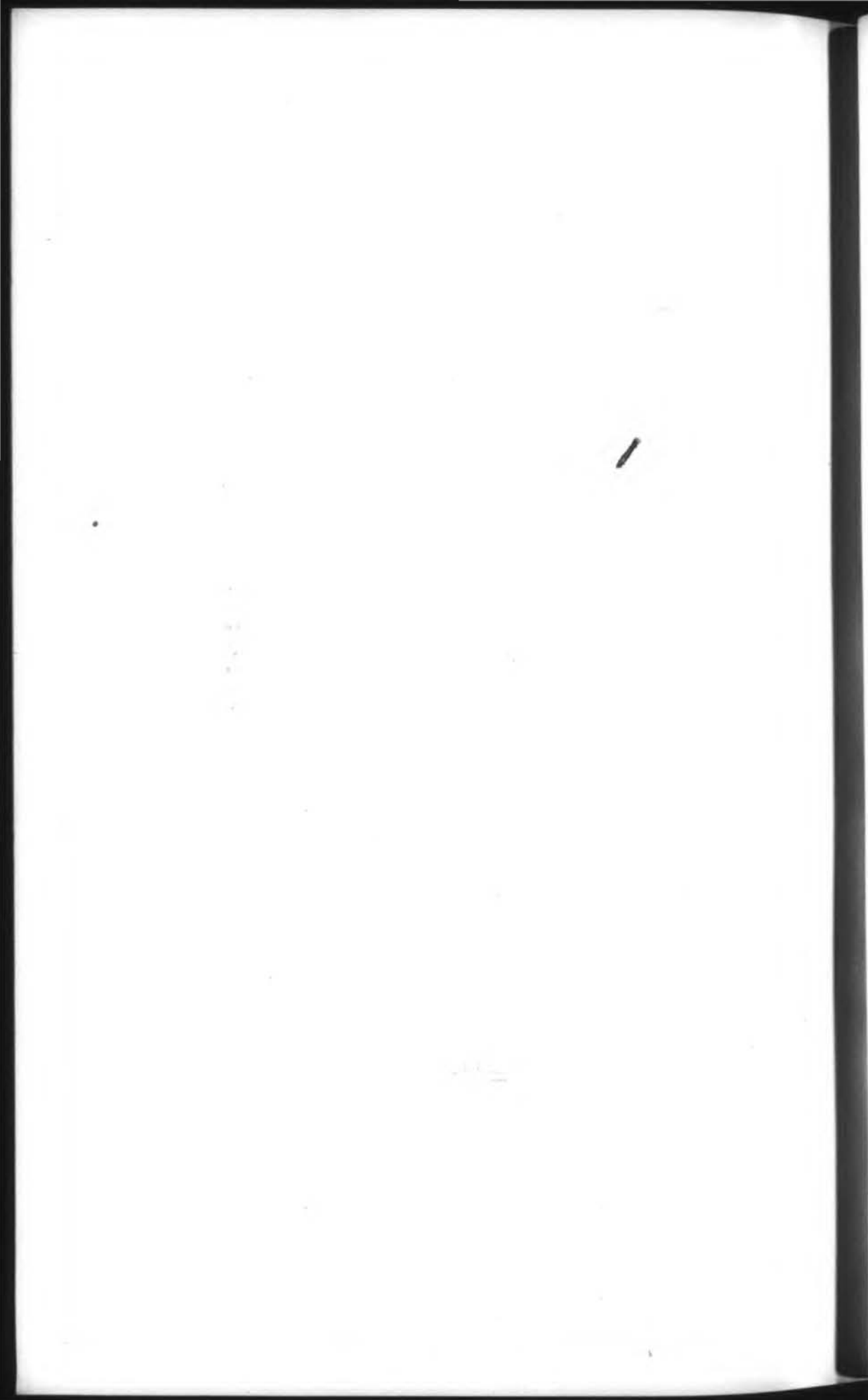
STATE ROAD DEPARTMENT OF FLORIDA
DISBURSEMENTS TO DECEMBER 31st, 1928 (Continued)

	Month of December		Total Twelve Months		Net to Date	
	Debits	Credits	Debits	Credits	Debits	Credits
EQUIPMENT ACCOUNT:						
Equipment, New.....	\$ 8,142.03		\$ 50,418.47	\$ 540.71		
Equipment, Overhauled.....	11,628.52		122,314.47	760.91		
Equipment, Live Stock.....			12,557.56	279.26	\$ 959,605.12	
Gainesville Shop.....					94,806.85	
Convict Camp Equipment.....					102,720.33	
SUSPENSE ACCOUNTS:						
Shop Operation.....	5,508.38	\$ 11,342.10	85,529.00	89,885.25	41,183.68	
Work in Process.....	6,154.35	8,860.04	69,456.38	88,872.79	6,480.12	
Parts—Junk.....	316.21	6.00	475.84	84.90	3,332.18	
Depreciation Reserve.....		52,238.47		301,959.09		\$ 301,959.09
Bills Receivable.....	152.64	10.21	3,481.04	4,957.15	3,356.75	
Convict Supplies, Raiford.....	612.98	707.00	2,108.31	2,372.00		2,310.51
Pineola Quarry.....					5,048.31	
Yukon Shop.....					25,000.00	
Equipment Donated.....					37,019.62	
Road Signs.....		34,931.55	1,269.15	34,931.55		5.25
Grounds and Building Upkeep.....			535.91		535.91	
Funds Diverted.....			150,000.00		150,000.00	
CONTINGENT FUND ACCOUNTS:						
Capital City Bank.....	10,759.36		43,322.81		233,834.41	
Barnett National Bank.....			27.64	200.00	1,419.13	
NET DISBURSEMENTS.....	1,100,606.57		14,342,548.90		79,516,579.89	
WORKING FUND BALANCES:						
Treasurer.....		78,300.52	805,880.10	1,097,549.74	108,243.95	
GROSS TREASURER'S RECEIPTS.....	\$ 1,022,306.05		\$14,050,879.26		\$79,624,823.84	

STATE ROAD DEPARTMENT OF FLORIDA

CASH STATEMENT DECEMBER, 1928

	Brought Forward From 1927	Receipts For Dec., 1928	Receipts For Twelve Months, 1928	Working Fund Oct., 1915, to Dec. 31, 1928	Disbursements Oct., 1915, to Dec. 31, 1928	Balance to Dec. 31st., 1928
Motor Vehicle License—5% Fund	\$ 77,528.52	\$ 724.56	\$ 228,623.58	\$ 1,611,317.16	\$ 1,604,830.10	\$ 6,487.06
Motor Vehicle License—85% Fund	3,575.54			1,529,511.14	1,525,935.60	3,575.54
State Federal Aid 70% Fund	90,183.17	10,143.71	3,200,716.39	18,009,194.18	17,998,856.73	10,337.45
State Road Tax Fund	2,272.46					
State Road Tax Fund—1 Mill			4,649.30			
State Road Tax Fund—Counties		344,389.02	2,973,867.98	16,332,594.16	16,260,640.45	71,953.71
Federal Aid Road Fund	115,774.22					
Alachua County—Project 37-B		9,540.36				
Putnam County—Project 50-C		15,785.82				
Leon County—Project 56		50,858.46				
Osceola County—Project 62-C		42,244.15	872,513.91	8,104,391.51	8,099,240.54	5,150.97
State Road License Fund Gas—(3c. Tax)	110,532.28	548,619.97	6,770,508.10	32,546,286.51	32,535,551.05	10,735.46
Maintenance Fund Gas and Oil Inspection	44.06			1,051,527.78	1,051,527.36	.42
State Road Tax Fund for Maintenance of State Roads	3.34			440,001.40	439,998.06	3.34
Total—Balance—Comptroller	\$ 399,913.59	\$ 1,022,306.05	\$14,050,879.26	\$79,624,823.84	\$79,516,579.89	\$ 108,243.95





Rock Base Before Surface Treating, Road 10, Along the Gulf of Merico.





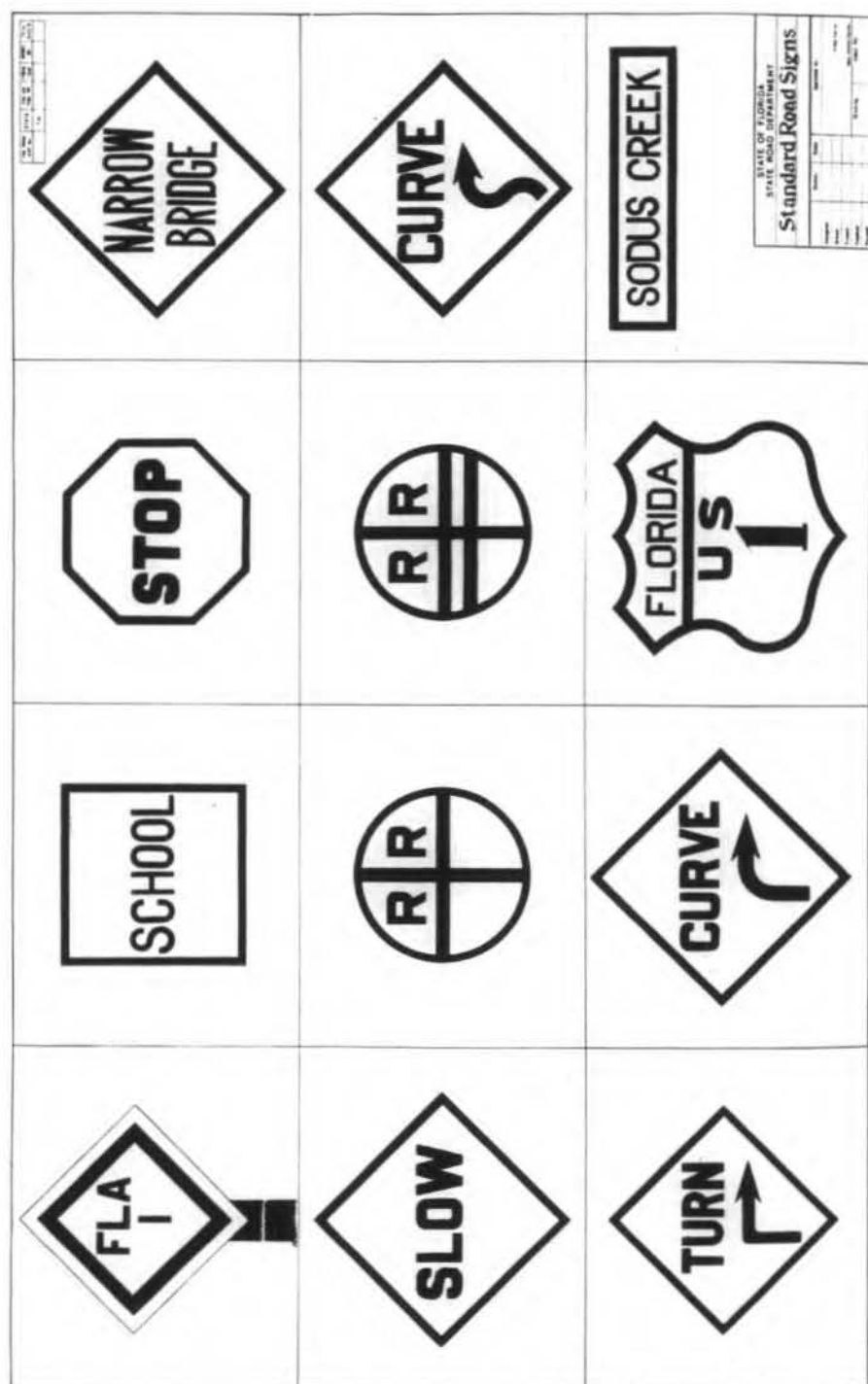
Plain Cement Concrete, Road 4.

HIGHWAY MARKINGS

Some two years ago the American Association of State Highway Officials in conjunction with the United States Bureau of Public Roads, realized the importance of standardizing the highway markers, and at a meeting of the Association of State Highway Officials a Joint Advisory Board was appointed, composed of members of the Association and the Bureau of Public Roads, acting as directing head, to investigate and make an exhaustive study of highway markers and signs.

This Board immediately started functioning and in connection with their investigation and duties they held meetings at various central points over the United States and invited all interested parties to attend. These meetings were in the nature of general discussions with a view to exchanging ideas on the various types of signs. After holding meetings all over the country the Joint Advisory Board then compiled all of the data obtained and drew up a tentative design for road markers and signs as a standard; this was then in turn submitted back to the American Association of State Highway Officials for consideration and further questions and arguments were invited. After carrying this on for a period of two years a standard for road markers and signs was adopted by the Association and that is the standard which we are now following in the erection of signs and markers on our highways.

The system of standardized signs and markers as adopted by the American Association of State Highway Officials at Detroit in November, 1925, is developed in a set of working drawings prepared by the Bureau of Public Roads and adopted by the Association. This set of designs is based on definite principles calculated to produce uniformity of significance in the signs themselves and make familiarity with them easy to acquire on the part of the most casual driver. These principles are a set of shapes, each having its own significance; a set of color combinations, each having its own significance; a few obvious symbols; and uniformity of erection and application as described in their manual.



A circular sign is used for railroad grade crossings only. Octagonal signs are used to indicate "Stop" where for any reason such action is necessary.

A diamond shape sign is used to indicate any condition inherent in the road itself requiring slow speed and caution on the part of the driver.

A square sign is used to indicate any condition requiring caution that is not inherent in the road itself, but which is due to contiguous or adjacent conditions which often are also intermittent.

Rectangular signs of various dimensions are used to carry directions and information of use or benefit to the driver.

All signs of a precautionary character, including the circular railroad signs, octagonal stop signs, the diamond caution signs and square caution signs, have black letters and designs on a yellow background.

All directional signs are black on a white background.

The symbols used are those for railroad grade crossings in single and multiple tracks; for left and right curves and turns and for reverse curves and turns.

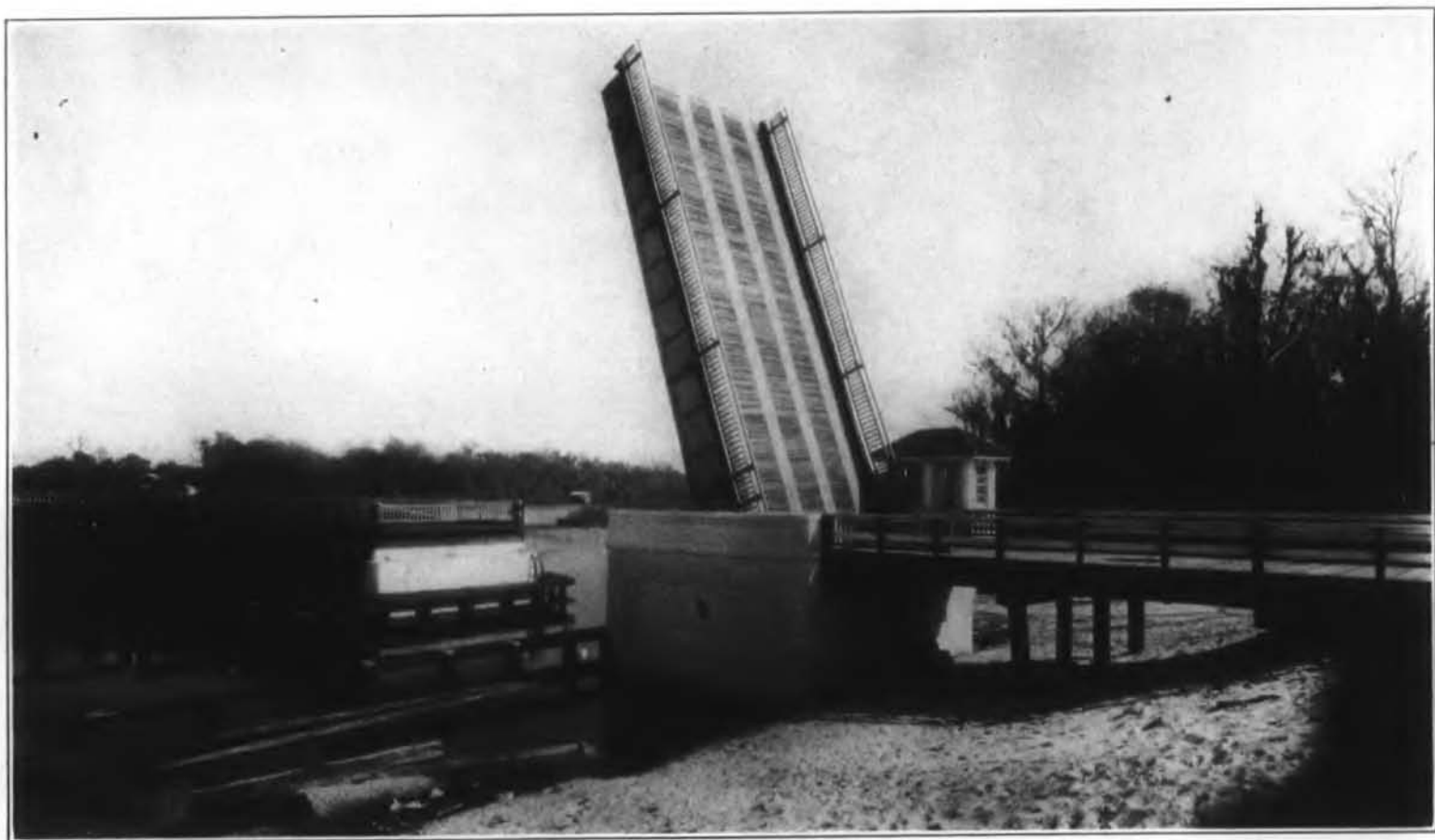
The above is the standard as adopted by the Association and the standard route marker for U. S. Highways is an outline of the official shield of the United States, but each individual State may use whatever appropriate marker they desire to adopt as their standard and place it in conjunction with all other signs. For instance, the official route marker for Nebraska is the Covered Wagon; the Indian Head for North Dakota, and the Keystone in Pennsylvania. This Department adopted as its official route marker, that is the symbol carrying the road number, a twelve-inch diamond shape sign with the letters FLA and the road number. The letters and numbers are white on a blue background, same making a very pleasing and appropriate marker.

Prior to the erection of our signs a complete log was made of the roads that we contemplated marking at this time. This log was made by one person in the same car and was made for the purpose of securing data and showing points of erection for each and every sign, the idea being uniformity throughout, as well as securing accurate distances. All dis-

tances are speedometer measurements corrected for the ratio of error of the speedometer, same having been checked over two hundred miles of measured distance. Our erection crews are following the schedule as outlined from this log of roads.

In addition to the above mentioned warning signs directional signs have been placed at all points wherever needed for the guidance of the traveler, both on the highways and, especially, routes through the towns and cities. In carrying out the directional program the policy adopted was to consider the road, giving the distance to the nearest town at its termini and the nearest local town in either direction.

The roads logged and marked are as follows: Numbers 1, 2, 3, 4, 5, 6, 7, 8, 9, 10 from the Georgia line to Tallahassee, 14 from Newberry to Hastings, 15 from Clearwater to St. Petersburg, 17, 21, 22 from Orlando to Indian River City, 23 and 24, these being the heaviest traveled roads at present and in a more continuous state of pavement than any of the others. It is the plan of the Department to continue to add to the above list of roads others until ultimately we have marked our entire system in accordance with the standard as outlined above and adopted.



Haines Creek Bridge, Road 2.



Sand Clay Surface Treated, Road 6.



Plain Cement Concrete, Federal Aid, Road 1.



Rock Base Surface Treated, Road 2.



Doctor's Lake Bridge, Road 3.



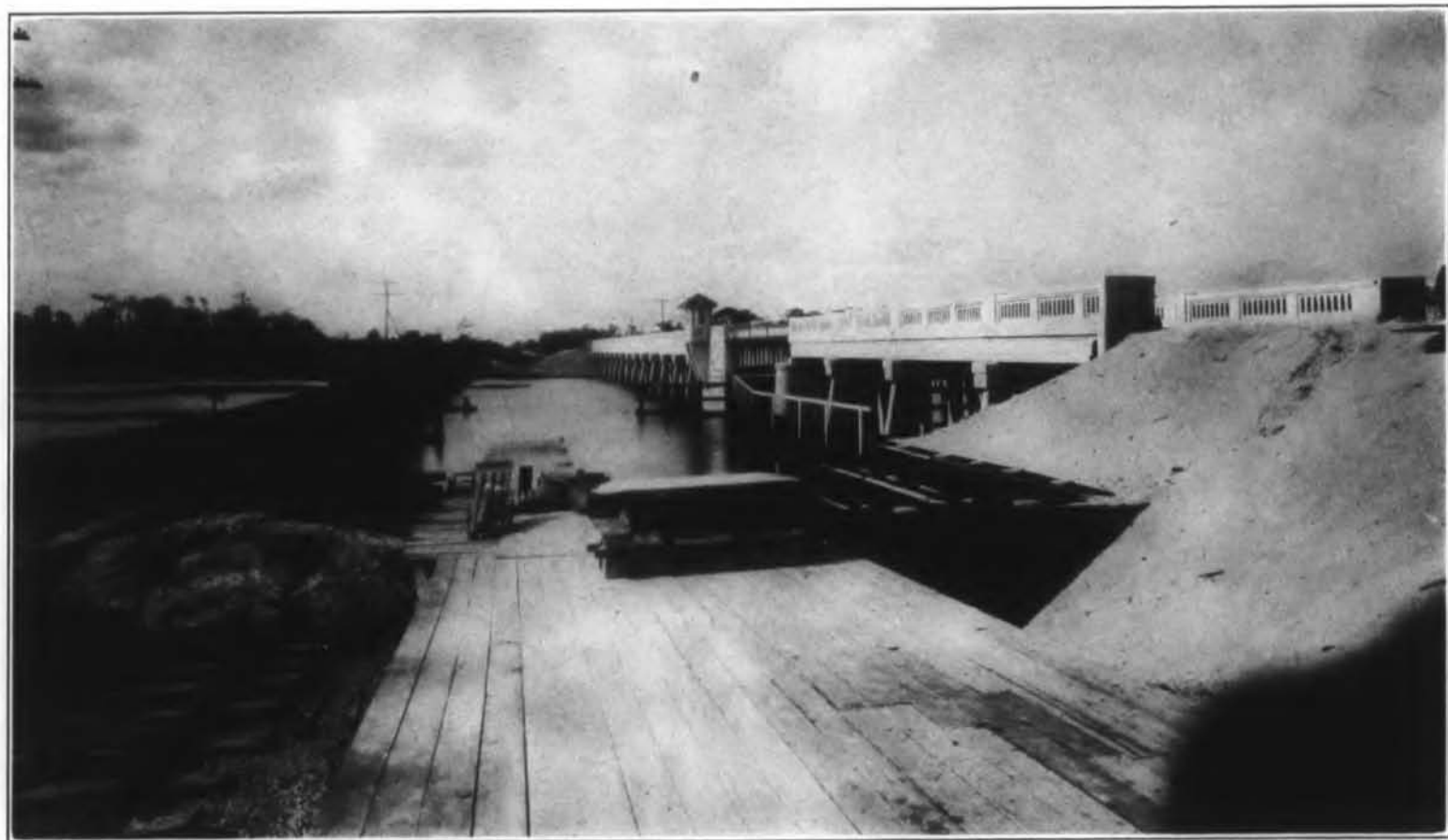
Tamiami Trail, Road 27.



Bituminous Concrete, Road 2.



Rock Base Surface Treated, Road 27.



Dead River Bridge, Road 2.



Tamiami Trail, Road 27.



Scene Showing Live Oak Forest.